



**TOWN OF BLYTHEWOOD
PLANNING COMMISSION AGENDA
VIRTUAL MEETING
JUNE 1, 2020 – 6:00 PM**

Due to the threat of COVID-19, the Planning Commission will be using a virtual meeting platform called Zoom. The meeting will be live streamed to the public using YouTube Live. Please go to the Town of Blythewood website, <http://www.townofblythewoodsc.gov/> and click on the YouTube link to access the meeting. Comments/Questions, before or during the meeting, may be sent via email to cookb@townofblythewoodsc.gov

**RICH MCKENRICK • MALCOLM GORDGE • MARCUS TAYLOR • DERREK PUGH •
ERICA PAGE • ED KESSER • ERNESTINE MIDDLETON**

- I. CALL TO ORDER AND DECLARATION OF A QUORUM**
 - A. NOTIFICATION AND POSTING OF THE AGENDA**
 - B. ADOPTION OF THE AGENDA**
 - C. PLEDGE OF ALLEGIANCE**
 - D. APPROVAL OF MINUTES – (May 4, 2020)**
 - II. CITIZENS TESTIMONY REGARDING ACTION ITEM (AS ITEM IS PRESENTED)**
 - III. ACTION ITEM**
 - A. THE PARK AT BLYTHEWOOD (CREECH ROAD) – Site Plan Review, TMS# 15100-05-06, Devin Blankenship**
 - IV. DISCUSSION ITEM**
 - A. COMPREHENSIVE PLAN**
 - V. OPEN CITIZEN COMMENT**
 - VI. ADJOURNMENT**
-

BLYTHEWOOD PLANNING COMMISSION
VIRTUAL MEETING
MONDAY, MAY 4, 2020
6:00 PM

MINUTES

Members Present

Rich McKenrick, Chair
Malcom Gordge
Derrek Pugh
Ed Kesser
Ernestine Middleton

Staff Present

Brian Cook, Town Admin
Melissa Cowan, Town Clerk
Saralyn Yarbrough, Admin

Members Absent

Marcus Taylor
Erica Page

I. CALL TO ORDER

The meeting was called to order by Co Chair McKenrick at 6:03 PM.

A. DECLARATION OF A QUORUM

Five Commissioners were present, constituting a quorum.

B. NOTIFICATION AND POSTING OF MEETING AGENDA

The Town Clerk confirmed the agenda was properly posted and the media notified.

C. ADOPTION OF THE AGENDA

Malcolm Gordge made a Motion to approve the agenda. The Motion was seconded by Ed Kesser **All in favor; 5-0**

D. PLEDGE OF ALLEGIANCE

E. APPROVAL OF MINUTES

Malcolm Gordge made a Motion to approve the minutes of March 2, 2020. The Motion was seconded by Derrek Pugh. **All in favor; 5-0**

II. CITIZENS TESTIMONY REGARDING ACTION ITEM

No Citizens signed in to speak.

III. ACTION ITEMS

- A. THE PARK AT BLYTHEWOOD (CREECH ROAD) – Site Plan Review, TMS# 15100-05-06, Devin Blankenship

Malcolm Gordge made a Motion to postpone the approval of site plan until more information is received concerning traffic study. The Motion was seconded by Erenstine Middleton. **All in favor; 5-0**

- B. ZONING MAP AMENDMENT- Rezone property located at 10715 Wilson Blvd. from Rural District (RU) to Multi-Neighborhood Office District (MO), TMS# 15100-08-14, Taricka Taylor

Malcolm Gordge made a Motion to approve the rezoning of property located at 10715 Wilson Blvd. The Motion was seconded by Derrek Pugh. **All in favor; 5-0**

- C. COMPREHENSIVE PLAN RE-WRITE – Sub-Committee Appointments

Malcolm Gordge made a Motion to appoint himself as Chair of the comprehensive plan sub- committee. The Motion was seconded by Ed Kesser. **All in favor; 5-0**

IV. OPEN CITIZEN COMMENT

No citizens signed in to speak.

V. ADJOURNMENT

Malcolm Gordge made a Motion to adjourn the meeting at 7:16 PM. The Motion was seconded by Ernestine Middleton. **All in favor; 5-0**

Respectfully submitted,

Melissa Cowan, Town Clerk



To: Planning Commission
From: Staff
Date: June 1, 2020
Subject: The Park at Blythewood, LP (Site Plan Review)
Location: Creech Road (TMS# 15100-05-06) (portion)
Proposed Use: Multi-family Residential
Zoning: Town Center (TC) Architectural Overlay District (AO)
Total Area: +/- 2.86 acres

The subject parcel at Creech Road where the proposed development is to take place (within the Town Center (TC) District) is +/- 2.86 acres in area and is located to the east of I-77. The proposed senior living facility will consist of 48 housing units (24 one-bedroom and 24 two-bedroom) in a three-story building. The development will have a single access point and be accessed exclusively from Creech Road. Multi-family housing is a permitted use in the Town Center (TC) District.

In the review of all group development plans, attention shall be given to internal vehicular and pedestrian circulation and the impact on the existing streets serving the development. The review can include requirements for spacing and location of driveway access, dedication of additional right-of-way on existing or proposed streets and roads to accommodate future widening projects, construction of acceleration and deceleration lanes on existing roads, construction of frontage roads, plant islands and other landscaping requirements, building orientation, screening to protect adjoining properties, character and location of lighting, or other requirements to mitigate traffic congestion and promote traffic and pedestrian safety.

A traffic impact study shall be prepared for review and approval by the Planning Commission as part of the site plan procedure for those group developments designed for 150 or more dwelling units, or a total non-residential gross floor area of 25,000 square feet or more. As such, no traffic impact study is required.

While a traffic study is not generally required for a development of this sort, the applicant has provided one at the request of the Planning Commission due to the specific circumstances of Creech Road being a dead end that intersects with Blythewood Road.

The Town of Blythewood tree preservation/landscaping requirements will be followed. Proper irrigation and maintenance of landscaping shall be required. The applicant shall work with staff to meet all landscaping requirements.

The preserved plus planted tree density shall meet the minimum of 30 units per acre. To determine compliance, a tree inventory will have to be established.

The site plan indicates one ingress/egress lane that will provide access to and from the site. SCDOT and/or Richland County will need to review and approve all intersection and driveway designs.

The total number of parking spaces to be provided for the new development is 71 based on the provided site plan.

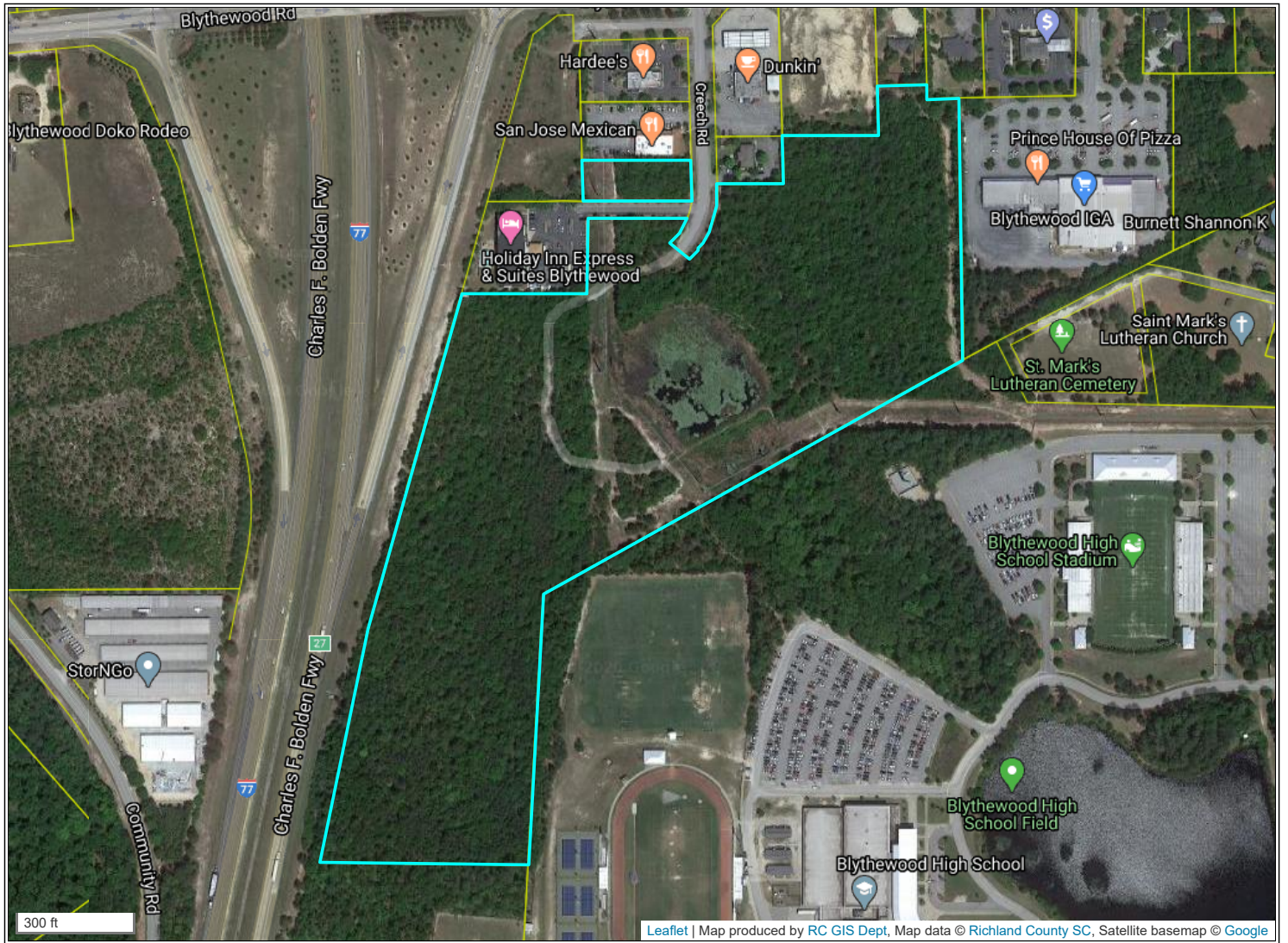
Projects in areas of high pedestrian use that provide improvements for safety, access, connectivity and walkability issues, that also increase walking as an alternative transportation mode are encouraged. As such, staff recommends pedestrian connectivity to the adjacent shopping center.

The applicant shall work with staff on the finalization of street cross sections/driveway design to accommodate the potential for a 95 ft. right of way on Creech (14.5 ft. clear zone from front property line).

Curb cuts and associated stormwater will be reviewed by and designed to the satisfaction of Richland County, SCDHEC, and SCDOT as applicable. All other federal, state, and local design regulations will be applicable.

This project will have to receive design approval from the Board of Architectural Review.

Construction shall be in substantial compliance with submitted plans and testimony given to the Planning Commission.

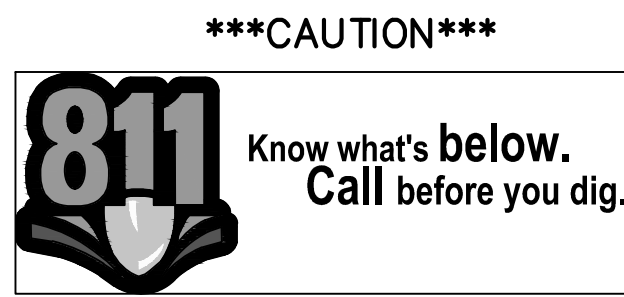


SITE NOTES:

1. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UNDERGROUND UTILITIES BY CALLING UTILITY LOCATION SERVICE AT 811 THREE (3) DAYS PRIOR TO CONSTRUCTION.
2. DIMENSIONS SHOWN ARE TO FACE OF CURB, CENTERLINE OF PARKING STALL OR FACE OF BUILDING UNLESS OTHERWISE NOTED.
3. REFER TO THE ARCHITECTURAL DRAWINGS FOR EXACT BUILDING DIMENSIONS AND ENTRY/EXIT PORCHES.
4. ASPHALT PAVING SECTIONS SHALL BE CONSTRUCTED PER THE DETAILS.
5. CONCRETE SIDEWALKS SHALL BE CONSTRUCTED WITH 3000 PSI CONCRETE 4" THICK WITH EXPANSION JOINTS AND SCORING PER DETAIL.
6. THE CONTRACTOR SHALL CONDUCT ALL WORK IN ACCORDANCE WITH THE LATEST REQUIREMENTS OF THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION.
7. ALL WORK ON STATE, COUNTY, OR CITY RIGHT-OF-WAY INCLUDING DRIVEWAY APRONS, WATER AND SEWER TAPS OR ANY OTHER WORK REQUIRES AN ENCROACHMENT PERMIT FROM THE APPROPRIATE AGENCY.
8. ALL CURBING TO BE 18" FORMED CONCRETE CURBING PER DETAIL, WITH 3000 PSI CONCRETE.
9. ANY MATERIAL DEMOLISHED AS INDICATED ON THIS PLAN IS TO BE HAULED OFF-SITE TO AN APPROVED LANDFILL.
10. TRUNCATED DOMES ARE TYPICAL AT ALL HANDICAP RAMPS. HANDICAP SPACES WILL BE MARKED WITH A FREESTANDING OR WALL MOUNTED SIGN. SEE DETAIL SHEETS FOR PROPER STRIPING.

SITE PLAN KEYED NOTES	
1	18" CONCRETE CURB AND GUTTER
2	ASPHALT PAVING (SEE LEGEND FOR TYPE)
3	CONCRETE PAVING
4	DUMPSTER ENCLOSURE
5	STOP SIGN & 24" STOP BAR
6	PAVEMENT FLUSH WITH SIDEWALK
7	WHEEL STOP
8	ADA PARKING SPACE & ACCESS AISLES 1-1/2% (2% MAX) SLOPE IN ALL DIRECTIONS
9	HANDICAP SIGN
10	DETECTABLE WARNING SURFACE
11	4" PAINTED STRIPE
12	CONCRETE SIDEWALK
13	ADA RAMP
14	CURB TRANSITION
15	STRIPED PATH (4" PAINTED STRIPE)
16	GAZEBO (SEE ARCH PLANS)
17	TRAFFIC DIRECTIONAL ARROWS
18	1' WIDE CURB OPENING
19	CURB FLUME WITH SIDEWALK CROSS DRAIN
20	4' HIGH BLACK VINYL COATED CHAIN LINK FENCE WITH 4' WIDE DOUBLE SWING GATE

PARKING CALCULATIONS					
USE TYPE	USE CALCULATIONS	MINIMUM SPACES REQUIRED	MINIMUM CALCULATED SPACES	MAXIMUM SPACES ALLOWED	MINIMUM CALCULATED SPACES
PROPOSED BUILDING					
Retirement Housing Services	48 units	1.0 spaces per 3 unit	16	1.5 spaces per unit	72
		TOTAL SPACES REQUIRED	16		
		TOTAL REGULAR SPACES PROVIDED	67		
		TOTAL ADA SPACES PROVIDED	4		
		TOTAL PARKING SPACES PROVIDED	71		
Minimum Bicycle Spaces		n/a			



THE UTILITIES SHOWN ARE SHOWN FOR THE CONTRACTOR'S CONVENIENCE ONLY. THERE MAY BE OTHER UTILITIES NOT SHOWN ON THESE PLANS. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THE LOCATIONS SHOWN AND IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATIONS OF ALL UTILITIES WITHIN THE LIMITS OF THE WORK. ALL DAMAGE MADE TO EXISTING UTILITIES BY THE CONTRACTOR SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

CURVE TABLE				
CURVE #	LENGTH	RADIUS	CHORD BEARING	CHORD
C1	53.13	283.00	N03°50'57"E	53.06
C3	157.45	283.00	S25°10'00"W	155.43

TM# R15100-05-33

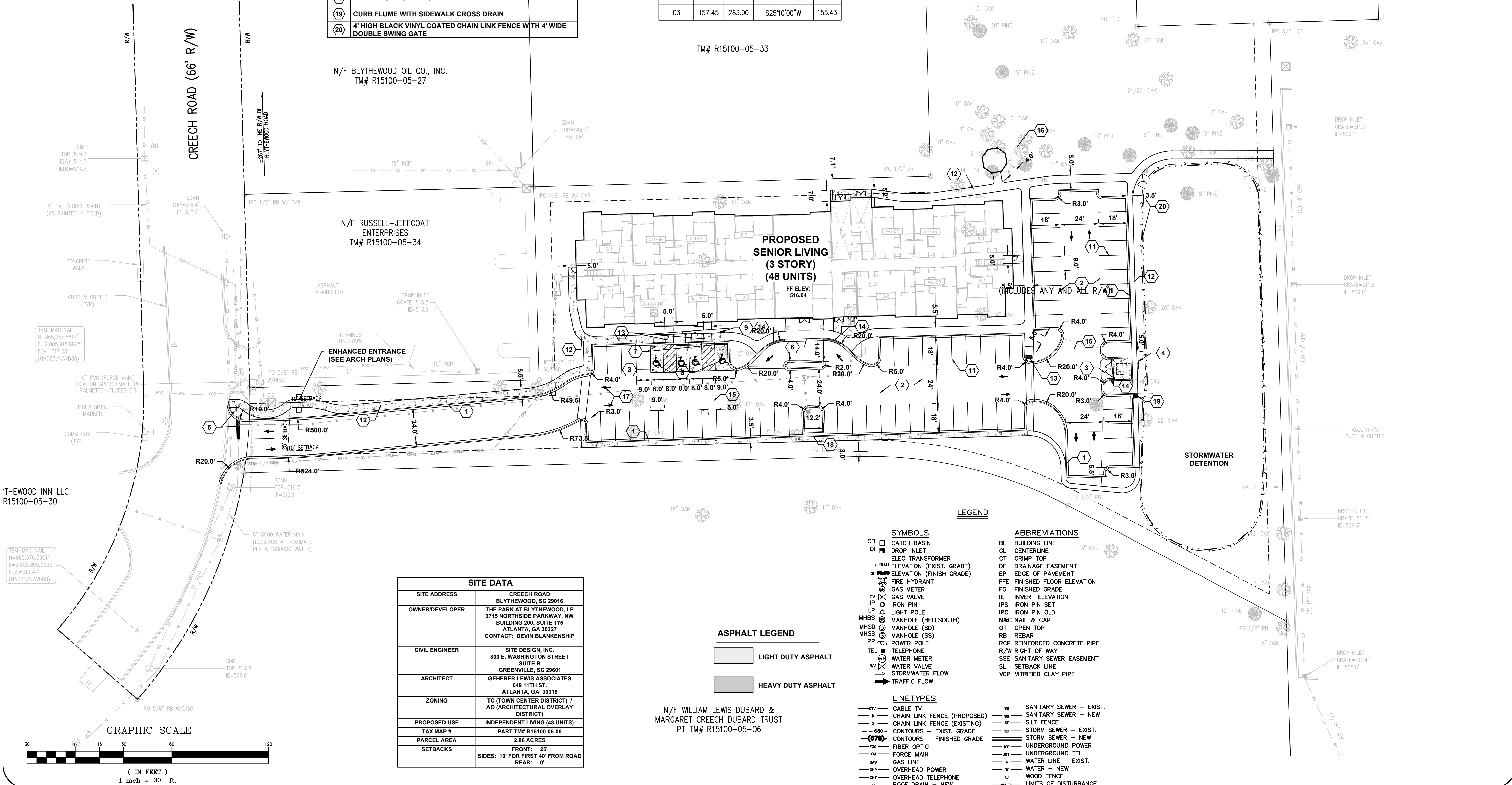
N/F BLYTHEWOOD OIL CO., INC.
TM# R15100-05-27

N/F RUSSELL-JEFFCOAT ENTERPRISES
TM# R15100-05-34

PROPOSED SENIOR LIVING (3 STORY) (48 UNITS)
FF ELEV: 516.04

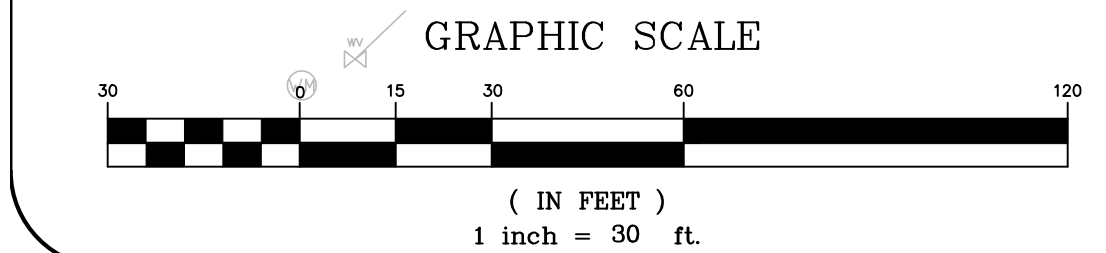
(INCLUDES ANY AND ALL R/W)

STORMWATER DETENTION



THEWOOD INN LLC
R15100-05-30

TBM-MAG NAIL
N=865,754.5817'
E=2,005,918.8822'
ELEV.=517.25'
(NAD83/NAVDS88)

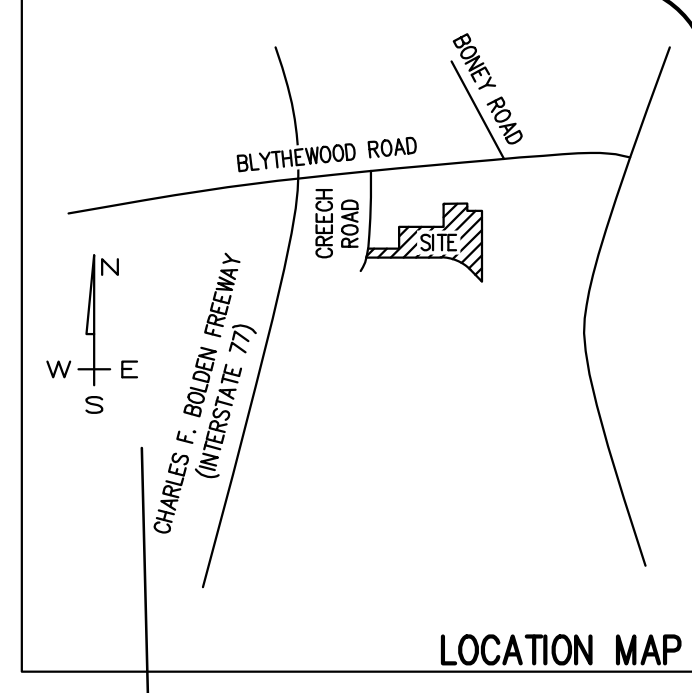


SITE DATA	
SITE ADDRESS	CREECH ROAD BLYTHEWOOD, SC 29016
OWNER/DEVELOPER	THE PARK AT BLYTHEWOOD, LP 3715 NORTHSIDE PARKWAY, NW BUILDING 200, SUITE 175 ATLANTA, GA 30327 CONTACT: DEVIN BLANKENSHIP
CIVIL ENGINEER	SITE DESIGN, INC. 800 E. WASHINGTON STREET SUITE B GREENVILLE, SC 29601
ARCHITECT	GEHEBER LEWIS ASSOCIATES 649 11TH ST. ATLANTA, GA 30318
ZONING	TC (TOWN CENTER DISTRICT) / AO (ARCHITECTURAL OVERLAY DISTRICT)
PROPOSED USE	INDEPENDENT LIVING (48 UNITS)
TAX MAP #	PART TM# R15100-05-06
PARCEL AREA	2.86 ACRES
SETBACKS	FRONT: 25' SIDES: 10' FOR FIRST 40' FROM ROAD REAR: 0'

ASPHALT LEGEND	
[Symbol]	LIGHT DUTY ASPHALT
[Symbol]	HEAVY DUTY ASPHALT

N/F WILLIAM LEWIS DUBARD & MARGARET CREECH DUBARD TRUST
PT TM# R15100-05-06

LEGEND	
SYMBOLS	ABBREVIATIONS
CB [Symbol] CATCH BASIN	BL BUILDING LINE
DI [Symbol] DRAIN INLET	CL CENTERLINE
[Symbol] ELEC TRANSFORMER	CT CRIMP TOP
x 90.0 ELEVATION (EXIST. GRADE)	DE DRAINAGE EASEMENT
x 80.0 ELEVATION (FINISH GRADE)	EP EDGE OF PAVEMENT
[Symbol] FIRE HYDRANT	FFE FINISHED FLOOR ELEVATION
[Symbol] GAS METER	FG FINISHED GRADE
[Symbol] GAS VALVE	IE INVERT ELEVATION
IP [Symbol] IRON PIN	IPS IRON PIN SET
LP [Symbol] LIGHT POLE	IPO IRON PIN OLD
MHS [Symbol] MANHOLE (BELLSOUTH)	N&C NAIL & CAP
MHS [Symbol] MANHOLE (SD)	OT OPEN TOP
MHS [Symbol] MANHOLE (SS)	RB REBAR
PP [Symbol] POWER POLE	RCR REINFORCED CONCRETE PIPE
TEL [Symbol] TELEPHONE	R/W RIGHT OF WAY
[Symbol] WATER METER	SSE SANITARY SEWER EASEMENT
[Symbol] WATER VALVE	SL SETBACK LINE
[Symbol] STORMWATER FLOW	VCP VITRIFIED CLAY PIPE
[Symbol] TRAFFIC FLOW	
LINE TYPES	
--- CABLE TV	--- SANITARY SEWER - EXIST.
-x- CHAIN LINK FENCE (PROPOSED)	--- SANITARY SEWER - NEW
-x- CHAIN LINK FENCE (EXISTING)	--- SILT FENCE
--- CONTOURS - EXIST. GRADE	--- STORM SEWER - EXIST.
--- CONTOURS - FINISHED GRADE	--- STORM SEWER - NEW
--- FIBER OPTIC	--- UNDERGROUND POWER
--- FORCE MAIN	--- UNDERGROUND TEL
--- GAS LINE	--- WATER LINE - EXIST.
--- OVERHEAD POWER	--- WATER - NEW
--- OVERHEAD TELEPHONE	--- WOOD FENCE
--- ROOF DRAIN - NEW	--- LIMITS OF DISTURBANCE



SITE DESIGN, INC.
CIVIL ENGINEERS - SURVEYORS - LANDSCAPE ARCHITECTS

www.sitedesign-inc.com
100 E. WASHINGTON ST. STE B
GREENVILLE, SC 29601
TEL: (864) 271-0902
FAX: (864) 271-0902

NO.	DATE
10.	
9.	
8.	
7.	
6.	
5.	
4.	
3.	
2.	
1.	

THIS DRAWING IS THE PROPERTY OF SITE DESIGN, INC. AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM. WITHOUT THE WRITTEN PERMISSION OF SITE DESIGN, INC. FURTHERMORE, THIS DRAWING IS NOT TO BE USED FOR ANY OTHER PROJECT WITHOUT THE WRITTEN PERMISSION OF SITE DESIGN, INC.

NO. 000122
SITE DESIGN, INC.
SOUTH CAROLINA
SC. REG. NO. 24201

THE PARK AT BLYTHEWOOD SENIOR LIVING
TOWN OF BLYTHEWOOD
RICHLAND COUNTY
SOUTH CAROLINA

THE PARK AT BLYTHEWOOD, LP
3715 NORTHSIDE PWY, NW
BLDG 200, STE 175
ATLANTA, GA 30327

HORIZ. SCALE: 1" = 30'
VERT. SCALE: N/A
DESIGNED BY: WTB
DRAWN BY: WTB
CHECKED BY: CSW
DATE: 2/19/2020

s191265-base.dwg

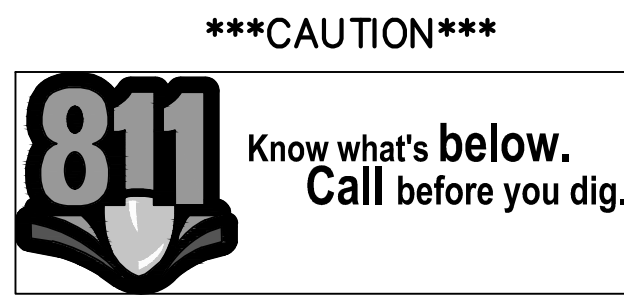
SITE PLAN

SHEET 3 OF XX

C200

GRADING NOTES:

1. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UNDERGROUND UTILITIES BY CALLING UTILITY LOCATION SERVICE AT 811 THREE (3) DAYS PRIOR TO CONSTRUCTION.
2. THE CONTRACTOR SHALL NOTIFY THE ENGINEER FOR A REVIEW SHOULD ANY DISCREPANCIES BE DISCOVERED AT THE SITE OR ON THE DRAWINGS.
3. PROOFROLLING AND COMPACTION TESTS SHALL BE ACCOMPLISHED IN THE FIELD TO TEST FILLED AREAS. INITIAL COMPACTION TESTS WILL BE CONDUCTED AT OWNER'S EXPENSE. RETESTING REQUIRED BECAUSE OF POOR COMPACTION SHALL BE CONDUCTED AT CONTRACTOR'S EXPENSE. CONTRACTOR SHALL SUPPLY RESULTS TO THE ENGINEER BEFORE ANY PRELIMINARY AND/OR FINAL APPROVALS.
4. THE GRADING CONTRACTOR SHALL PROOFROLL THE CONSTRUCTION AREA WITH HEAVY RUBBER-TIRED EQUIPMENT. ALL SOFT SPOTS SHALL BE STABILIZED BY FURTHER COMPACTION EFFORT OR UNDERCUT AND BACKFILLED WITH COMPACTED STRUCTURAL FILL MATERIAL.
5. EARTHWORK SHALL BE TO THE GRADES AND LINES SHOWN. EXISTING AND PROPOSED CONTOUR INTERVALS ARE AT 1'.
6. ALL NEW ELEVATIONS SHOWN ARE FINISH ELEVATIONS. THE GRADING CONTRACTOR SHALL DEDUCT THE APPROPRIATE AMOUNT TO ESTABLISH SUBGRADE ELEVATION.
7. WALL ELEVATIONS WHERE NOTED ARE TOP OF WALL AND FINISH GRADE AT BOTTOM OF WALL. SEE RETAINING WALL PLANS FOR DETAILED CONSTRUCTION INFORMATION.
8. CONTRACTOR SHALL REMOVE TOPSOIL AS NECESSARY (MINIMUM OF 4") TO PROVIDE ADEQUATE SUBGRADE FOR ROADWAYS.
9. ALL EXCAVATION SHALL BE "UNCLASSIFIED EXCAVATION". ALL NEW FILL AND UTILITY TRENCH BACKFILL SHALL BE COMPACTED TO AT LEAST 98% OF MAXIMUM DRY DENSITY AS DETERMINED BY THE STANDARD PROCTOR (ASTM D-998). IN ADDITION, THE UPPER 18" OF ALL FILL MATERIALS BENEATH FLOOR SLABS AND PAVEMENTS SHALL BE COMPACTED TO AT LEAST 98% OF THE MAXIMUM DRY DENSITY (ASTM D-998).
10. THE CONTRACTOR SHALL CONDUCT ALL WORK IN ACCORDANCE WITH THE LATEST REQUIREMENTS OF THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION.
11. ALL REINFORCED CONCRETE PIPE SHALL BE CLASS III, UNLESS OTHERWISE NOTED, AND SHALL CONFORM TO SCDOT SPECIFICATIONS. ALL JOINTS SHALL BE TONGUE AND GROOVE WITH MASTIC MATERIAL.
12. CATCH BASINS SHALL BE PRECAST REINFORCED CONCRETE DRAINAGE BOX WITH SOLID WALL PER SCDOT STANDARD DRAWING 719-17A OR APPROVED EQUAL.
13. ALL WORK ON STATE, COUNTY, OR CITY RIGHT-OF-WAYS, INCLUDING DRIVEWAY APRONS, WATER AND SEWER TAPS OR ANY OTHER WORK REQUIRES AN ENCROACHMENT PERMIT FROM THE APPROPRIATE AGENCY.
14. STANDING GRASS MUST BE EVIDENT IN AREAS THAT WERE SEED.
15. AN AS-BUILT CERTIFICATION OF THE STORMWATER MANAGEMENT SYSTEM WILL BE REQUIRED PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY FOR THE SITE. THE DESIGN ENGINEER WILL NOT BE RESPONSIBLE FOR THE DELAYS RESULTING FROM NON-COMPLIANCE WITH THE DESIGN DRAWINGS.

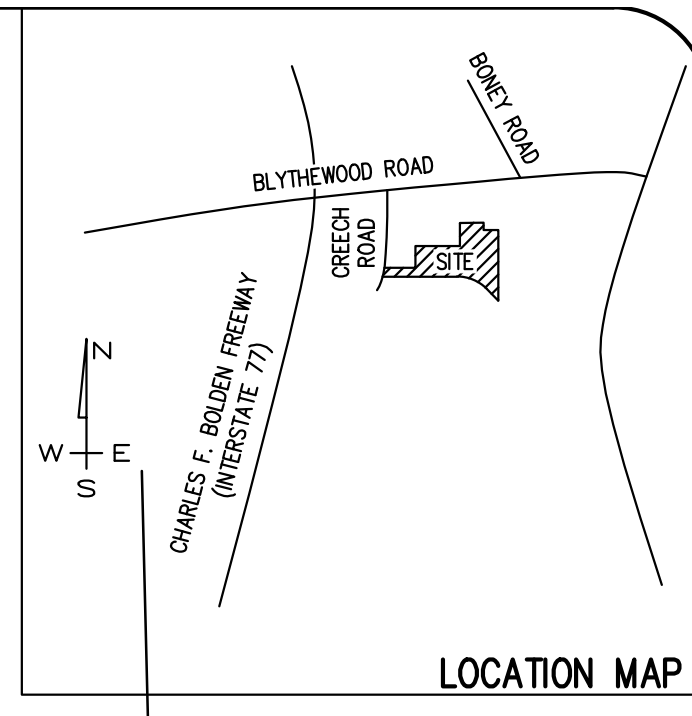


THE UTILITIES SHOWN ARE SHOWN FOR THE CONTRACTOR'S CONVENIENCE ONLY. THERE MAY BE OTHER UTILITIES NOT SHOWN ON THESE PLANS. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THE LOCATIONS SHOWN AND IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATIONS OF ALL UTILITIES WITHIN THE LIMITS OF THE WORK. ALL DAMAGE MADE TO EXISTING UTILITIES BY THE CONTRACTOR SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

CURVE TABLE				
CURVE #	LENGTH	RADIUS	CHORD BEARING	CHORD
C1	53.13	283.00	N03°50'57"E	53.06
C3	157.45	283.00	S25°10'00"W	155.43

NOTES:

1. SLOPES WITHIN ADA PARKING AREAS CANNOT EXCEED 2% IN ANY DIRECTION. ALL ADA PARKING AREAS ARE TO BE CONSTRUCTED WITH CONCRETE.
2. SIDEWALK CROSS SLOPES ARE NOT TO EXCEED 2%.
3. ANY SIDEWALK EXCEEDING 5% RUNNING SLOPE IS CONSIDERED A RAMP. ANY RAMP RISING GREATER THAN 6" MUST HAVE AN ADA APPROVED HANDRAIL ON BOTH SIDES.
4. ANY EXCESS SOIL HAULED OFF-SITE MUST BE TO AN APPROVED PERMITTED SITE.
5. SPOT ELEVATIONS SHOWN DENOTE BOTTOM OF CURB, TOP OF ASPHALT, OR GROUND SURFACE ELEVATION UNLESS OTHERWISE NOTED.
6. TC = TOP OF CURB / BC = BOTTOM OF CURB.
7. TW = TOP OF WALL / BW = BOTTOM OF WALL.
8. BUILDING DOWNSPOUTS WILL DISCHARGE AT SPLASH BLOCKS.



TBM-MAG NAIL
N=865,999.5831'
E=2,005,910.1013'
ELEV.=519.15'
(NAD83/NAVDO88)

TBM-MAG NAIL
N=865,754.5817'
E=2,005,918.8825'
ELEV.=517.25'
(NAD83/NAVDO88)

TBM-MAG NAIL
N=865,576.5901'
E=2,005,995.7925'
ELEV.=513.47'
(NAD83/NAVDO88)

THEWOOD INN LLC
R15100-05-30

N/F BLYTHEWOOD OIL CO., INC.
TM# R15100-05-27

N/F RUSSELL-JEFFCOAT
ENTERPRISES
TM# R15100-05-34

TM# R15100-05-33

N/F BWL HOLDINGS LLC
TM# R15112-03-01

N/F JAMES A. FINKEL
TM# R15112-03-02

PROPOSED SENIOR LIVING (48 UNITS)
124,448 SQ. FT.
2.86 ACRES
(INCLUDES ANY AND ALL R/W)

DETENTION BASIN NOTE:
THE DETENTION BASIN ACTS AS A NATURAL INFILTRATION BASIN UTILIZING THE IN SITU SOIL INFILTRATION RATE OF 10 IN / HR.

SYMBOLS

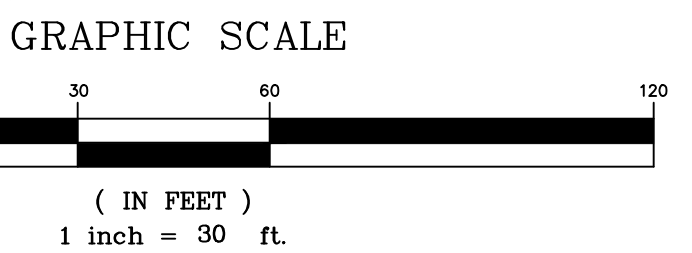
- CB CATCH BASIN
- DI DROP INLET
- ELEC TRANSFORMER
- ELEVATION (EXIST. GRADE)
- ELEVATION (FINISH GRADE)
- FIRE HYDRANT
- GAS METER
- GAS VALVE
- IRON PIN
- LIGHT POLE
- MANHOLE (BELLSOUTH)
- MANHOLE (SD)
- MANHOLE (SS)
- POWER POLE
- TELEPHONE
- WATER METER
- WATER VALVE
- STORMWATER FLOW
- TRAFFIC FLOW

ABBREVIATIONS

- BL BUILDING LINE
- CL CENTERLINE
- CT CRIMP TOP
- DE DRAINAGE EASEMENT
- EP EDGE OF PAVEMENT
- FFE FINISHED FLOOR ELEVATION
- FG FINISHED GRADE
- IE INVERT ELEVATION
- IPS IRON PIN SET
- IPO IRON PIN OLD
- REBAR
- RCP REINFORCED CONCRETE PIPE
- R/W RIGHT OF WAY
- SSE SANITARY SEWER EASEMENT
- SL SETBACK LINE
- VCP VITRIFIED CLAY PIPE

LINETYPES

- C- CABLE TV
- X- CHAIN LINK FENCE (PROPOSED)
- X- CHAIN LINK FENCE (EXISTING)
- 680- CONTOURS - EXIST. GRADE
- (678)- CONTOURS - FINISHED GRADE
- F- FIBER OPTIC
- FM- FORCE MAIN
- G- GAS LINE
- OP- OVERHEAD POWER
- OT- OVERHEAD TELEPHONE
- RD- ROOF DRAIN - NEW
- SS- SANITARY SEWER - EXIST.
- NS- SANITARY SEWER - NEW
- S- SILT FENCE
- SD- STORM SEWER - EXIST.
- SN- STORM SEWER - NEW
- UP- UNDERGROUND POWER
- UT- UNDERGROUND TEL
- W- WATER LINE - EXIST.
- WN- WATER LINE - NEW
- W- WOOD FENCE
- NPES- LIMITS OF DISTURBANCE



N/F WILLIAM LEWIS DUBARD &
MARGARET CREECH DUBARD TRUST
PT TM# R15100-05-06

SITE DESIGN, INC.
CIVIL ENGINEERS - SURVEYORS - LANDSCAPE ARCHITECTS

www.sitedesign-inc.com
100 E. WASHINGTON ST. STE B
GREENVILLE, SC 29601
TEL: (864) 271-0900
FAX: (864) 271-0902

NO.	DATE
10.	
9.	
8.	
7.	
6.	
5.	
4.	
3.	
2.	
1.	

THIS DRAWING IS THE PROPERTY OF SITE DESIGN, INC. AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, WITHOUT THE WRITTEN PERMISSION OF SITE DESIGN, INC. FURTHERMORE, THIS DRAWING IS NOT TO BE USED FOR ANY OTHER PROJECT WITHOUT THE WRITTEN PERMISSION OF SITE DESIGN, INC.

SC. REG. NO. 24201

THE PARK AT BLYTHEWOOD SENIOR LIVING
TOWN OF BLYTHEWOOD
RICHLAND COUNTY
SOUTH CAROLINA

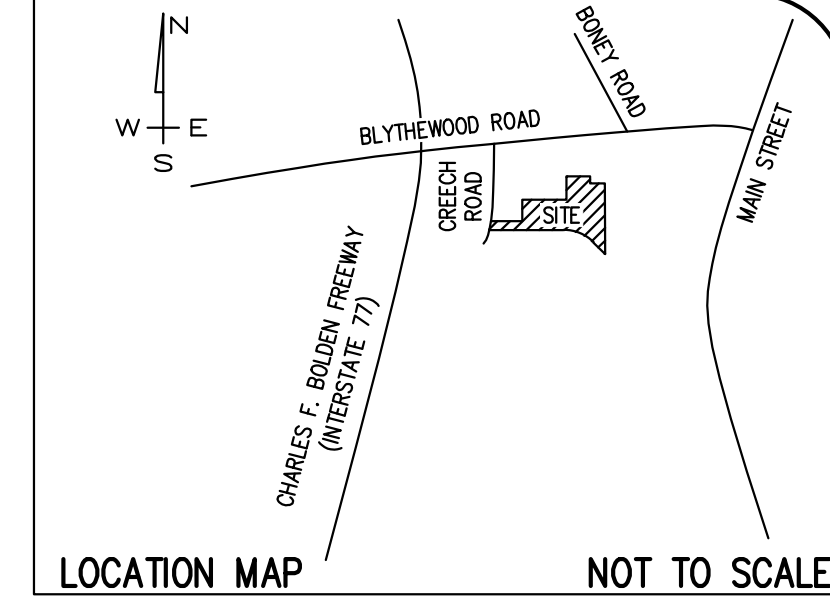
THE PARK AT BLYTHEWOOD, LP
375 NORTHSIDE PWY, NW
BLDG 200, STE 175
ATLANTA, GA 30327

HORIZ. SCALE: 1" = 30'
VERT. SCALE: N/A
DESIGNED BY: WTB
DRAWN BY: WTB
CHECKED BY: CSW
DATE: 2/19/2020

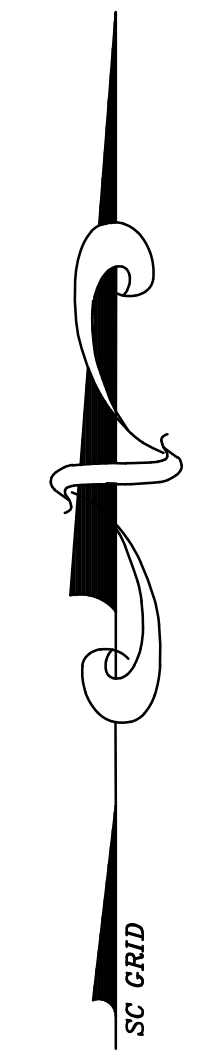
s191265-base.dwg
GRADING / STORMDRAINAGE PLAN

SHEET 11 OF XX
C400

UNRECORDED



N/F KJ INVESTMENT
COMPANY LLC
TM# R15100-05-29



SITE DATA	
SITE ADDRESS	CREECH ROAD BLYTHEWOOD, SC 29016
OWNER/DEVELOPER	THE PARK AT BLYTHEWOOD, LP 3715 NORTHSIDE PARKWAY, NW BUILDING 200, SUITE 175 ATLANTA, GA 30327 CONTACT: DEVIN BLANKENSHIP
CIVIL ENGINEER	SITE DESIGN, INC. 800 E. WASHINGTON STREET SUITE B GREENVILLE, SC 29601
ARCHITECT	GEHEBER LEWIS ASSOCIATES 649 11TH ST. ATLANTA, GA 30318
ZONING	TC (TOWN CENTER DISTRICT) / AO (ARCHITECTURAL OVERLAY DISTRICT)
PROPOSED USE	INDEPENDENT LIVING (48 UNITS)
TAX MAP #	PART TM# R15100-05-06
PARCEL AREA	2.86 ACRES
SETBACKS	FRONT: 25' SIDES: 10' FOR FIRST 40' FROM ROAD REAR: 0'

<p>○ TREE TO BE PROTECTED TYP. (AS LABELED AND ATTACHED CHART)</p> <p>✕ TREE TO BE REMOVED TYP. (AS LABELED)</p>	<p>LEGEND</p> <table border="0"> <tr> <th colspan="2">SYMBOLS</th> <th colspan="2">ABBREVIATIONS</th> </tr> <tr> <td>CB</td><td>CATCH BASIN</td> <td>BL</td><td>BUILDING LINE</td> </tr> <tr> <td>DI</td><td>DROP INLET</td> <td>CL</td><td>CENTERLINE</td> </tr> <tr> <td></td><td>ELEC TRANSFORMER</td> <td>CT</td><td>CRIMP TOP</td> </tr> <tr> <td>x 90.0</td><td>ELEVATION (EXIST. GRADE)</td> <td>DE</td><td>DRAINAGE EASEMENT</td> </tr> <tr> <td>x 100.0</td><td>ELEVATION (FINISH GRADE)</td> <td>EP</td><td>EDGE OF PAVEMENT</td> </tr> <tr> <td>⊕</td><td>FIRE HYDRANT</td> <td>FFE</td><td>FINISHED FLOOR ELEVATION</td> </tr> <tr> <td>⊕</td><td>GAS METER</td> <td>FG</td><td>FINISHED GRADE</td> </tr> <tr> <td>⊕</td><td>GAS VALVE</td> <td>IE</td><td>INVERT ELEVATION</td> </tr> <tr> <td>IP</td><td>IRON PIN</td> <td>IPS</td><td>IRON PIN SET</td> </tr> <tr> <td>LP</td><td>LIGHT POLE</td> <td>IPO</td><td>IRON PIN OLD</td> </tr> <tr> <td>MHBS</td><td>MANHOLE (BELLSOUTH)</td> <td>N&C</td><td>NAIL & CAP</td> </tr> <tr> <td>MHSD</td><td>MANHOLE (SD)</td> <td>OT</td><td>OPEN TOP</td> </tr> <tr> <td>MHSS</td><td>MANHOLE (SS)</td> <td>RB</td><td>REBAR</td> </tr> <tr> <td>PP</td><td>POWER POLE</td> <td>ROP</td><td>ROP REINFORCED CONCRETE PIPE</td> </tr> <tr> <td>TEL</td><td>TELEPHONE</td> <td>R/W</td><td>R/W RIGHT OF WAY</td> </tr> <tr> <td>⊕</td><td>WATER METER</td> <td>SSE</td><td>SANITARY SEWER EASEMENT</td> </tr> <tr> <td>⊕</td><td>WATER VALVE</td> <td>SL</td><td>SETBACK LINE</td> </tr> <tr> <td>→</td><td>STORMWATER FLOW</td> <td>VCP</td><td>VITRIFIED CLAY PIPE</td> </tr> <tr> <td>→</td><td>TRAFFIC FLOW</td> <td></td><td></td> </tr> </table>	SYMBOLS		ABBREVIATIONS		CB	CATCH BASIN	BL	BUILDING LINE	DI	DROP INLET	CL	CENTERLINE		ELEC TRANSFORMER	CT	CRIMP TOP	x 90.0	ELEVATION (EXIST. GRADE)	DE	DRAINAGE EASEMENT	x 100.0	ELEVATION (FINISH GRADE)	EP	EDGE OF PAVEMENT	⊕	FIRE HYDRANT	FFE	FINISHED FLOOR ELEVATION	⊕	GAS METER	FG	FINISHED GRADE	⊕	GAS VALVE	IE	INVERT ELEVATION	IP	IRON PIN	IPS	IRON PIN SET	LP	LIGHT POLE	IPO	IRON PIN OLD	MHBS	MANHOLE (BELLSOUTH)	N&C	NAIL & CAP	MHSD	MANHOLE (SD)	OT	OPEN TOP	MHSS	MANHOLE (SS)	RB	REBAR	PP	POWER POLE	ROP	ROP REINFORCED CONCRETE PIPE	TEL	TELEPHONE	R/W	R/W RIGHT OF WAY	⊕	WATER METER	SSE	SANITARY SEWER EASEMENT	⊕	WATER VALVE	SL	SETBACK LINE	→	STORMWATER FLOW	VCP	VITRIFIED CLAY PIPE	→	TRAFFIC FLOW		
SYMBOLS		ABBREVIATIONS																																																																															
CB	CATCH BASIN	BL	BUILDING LINE																																																																														
DI	DROP INLET	CL	CENTERLINE																																																																														
	ELEC TRANSFORMER	CT	CRIMP TOP																																																																														
x 90.0	ELEVATION (EXIST. GRADE)	DE	DRAINAGE EASEMENT																																																																														
x 100.0	ELEVATION (FINISH GRADE)	EP	EDGE OF PAVEMENT																																																																														
⊕	FIRE HYDRANT	FFE	FINISHED FLOOR ELEVATION																																																																														
⊕	GAS METER	FG	FINISHED GRADE																																																																														
⊕	GAS VALVE	IE	INVERT ELEVATION																																																																														
IP	IRON PIN	IPS	IRON PIN SET																																																																														
LP	LIGHT POLE	IPO	IRON PIN OLD																																																																														
MHBS	MANHOLE (BELLSOUTH)	N&C	NAIL & CAP																																																																														
MHSD	MANHOLE (SD)	OT	OPEN TOP																																																																														
MHSS	MANHOLE (SS)	RB	REBAR																																																																														
PP	POWER POLE	ROP	ROP REINFORCED CONCRETE PIPE																																																																														
TEL	TELEPHONE	R/W	R/W RIGHT OF WAY																																																																														
⊕	WATER METER	SSE	SANITARY SEWER EASEMENT																																																																														
⊕	WATER VALVE	SL	SETBACK LINE																																																																														
→	STORMWATER FLOW	VCP	VITRIFIED CLAY PIPE																																																																														
→	TRAFFIC FLOW																																																																																

TOWNSHIP OF BLYTHEWOOD				
Town of BlytheWood requires 30 TDF per acre minus acreage from stormwater management pond				
TOTAL SITE	2.86 Acres			
STORMWATER MANAGEMENT POND	0.37 Acres			
NET AREA FOR TDF CALCULATION	2.49 Acres			
TDF/ ACRE	30			
REQUIRED TDF	74.70 TDF			

TREES SAVED					
Quantity	Tree Species	Tree Caliper	Credit Units	Credits	
1	OAK	6 in	2.4	2.4	
2	OAK	10 in	4.8	9.6	
1	OAK	13 in	6.4	6.4	
1	OAK	15 in	7.2	7.2	
2	OAK	16 in	7.6	15.2	
1	OAK	24 in	18.6	18.6	
1	PINE	15 in	7.2	7.2	
1	PINE	20 in	9.2	9.2	
1	PINE	24 in	18.6	18.6	
				Existing TDF	94.4
Existing TDF	94.4				
Required TDF	74.70				
Post Development TDF	19.7				

CAUTION

Know what's below.
Call before you dig.

THE UTILITIES SHOWN ARE SHOWN FOR THE CONTRACTOR'S CONVENIENCE ONLY. THERE MAY BE OTHER UTILITIES NOT SHOWN ON THESE PLANS. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THE LOCATIONS SHOWN AND IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATIONS OF ALL UTILITIES WITHIN THE LIMITS OF THE WORK. ALL DAMAGE MADE TO EXISTING UTILITIES BY THE CONTRACTOR SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

GRAPHIC SCALE

(IN FEET)
1 inch = 30 ft.

SITE DESIGN, INC.
CIVIL ENGINEERS - SURVEYORS - LANDSCAPE ARCHITECTS

www.sitedesign-inc.com
100 E. WASHINGTON ST. STE B
GREENVILLE, SC 29601
TEL: (864) 271-0400
FAX: (864) 271-0402

NO.	DATE
10.	
9.	
8.	
7.	
6.	
5.	
4.	
3.	
2.	
1.	

THIS DRAWING IS THE PROPERTY OF SITE DESIGN, INC. AND IS NOT TO BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF SITE DESIGN, INC. FOR ANY OTHER PROJECT. THIS DRAWING IS NOT TO BE USED FOR ANY OTHER PROJECT WITHOUT THE WRITTEN PERMISSION OF SITE DESIGN, INC. FOR ANY OTHER PROJECT.

SITE DESIGN, INC.
No. 000122
SC REG. NO. 522

THE PARK AT BLYTHEWOOD SENIOR LIVING

TOWN OF BLYTHEWOOD
RICHLAND COUNTY
SOUTH CAROLINA

THE PARK AT BLYTHEWOOD
2715 NORTHSIDE PWY, NW
BLDG 200, STE 175
ATLANTA, GA 30327

HORIZ. SCALE:	1" = 30'
VERT. SCALE:	N/A
DESIGNED BY:	JAW
DRAWN BY:	JAW
CHECKED BY:	AGB
DATE:	2/19/2020
S191285-LANDSCAPE.dwg	

TREE PROTECTION PLAN

SHEET 170F 18

L000



GLA-ATL, LLC

649 11TH STREET
ATLANTA, GA 30318
VOICE: 404.228.1958
FAX: 404.228.8350

WWW.GLAATL.COM

THIS DRAWING IS AN INSTRUMENT OF SERVICE
AND SHALL REMAIN THE PROPERTY OF THE
ARCHITECT, AND SHALL NOT BE REPRODUCED,
PUBLISHED OR USED IN ANY WAY WITHOUT THE
CONSENT OF THE ARCHITECT. THIS DRAWING
SHALL NOT BE SCALED. ©COPYRIGHT 2020
GLA-ATL, LLC.

RELEASE DATES:

REV # DATE DESCRIPTION

REV	#	DATE	DESCRIPTION



STAMP:

CLIENT:

PRESTWICK COMPANIES

3715 NORTHSIDE
PARKWAY, NW
BLDG 200, SUITE 175
ATLANTA, GA 30327

PROJECT:

THE PARK AT
BLYTHEWOOD

SENIOR LIVING

DRAWING TITLE:

PROPOSED SITE LAYOUT

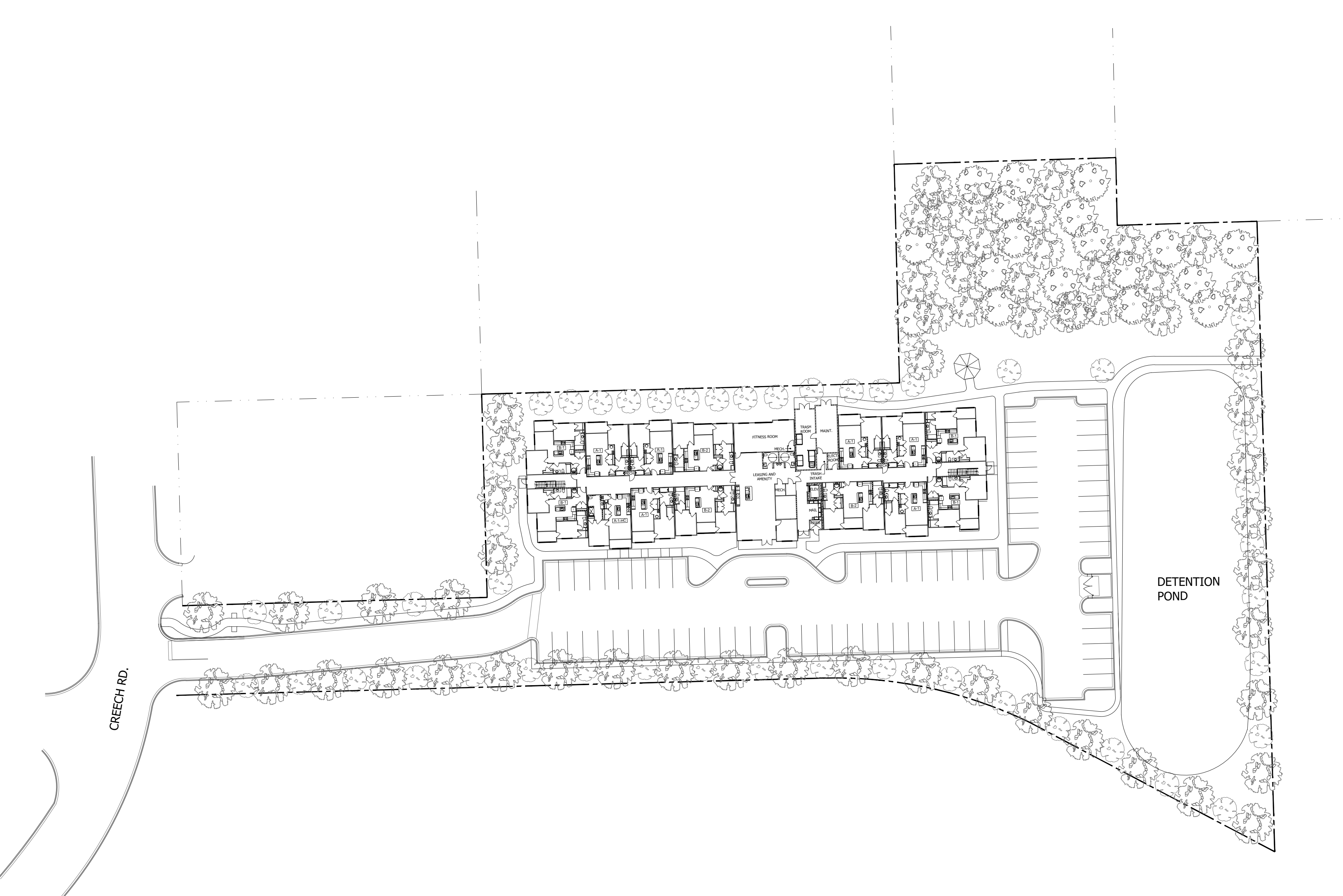
DRAWN BY: CHECKED BY:

SCALE: 1:40 DATE: 04/15/2020

PROJECT NUMBER: 1907

DRAWING NUMBER:

NOT ISSUED FOR CONSTRUCTION



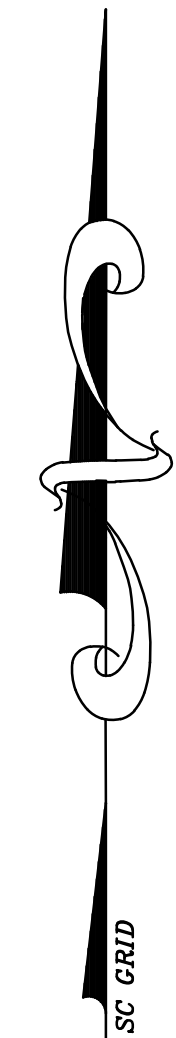
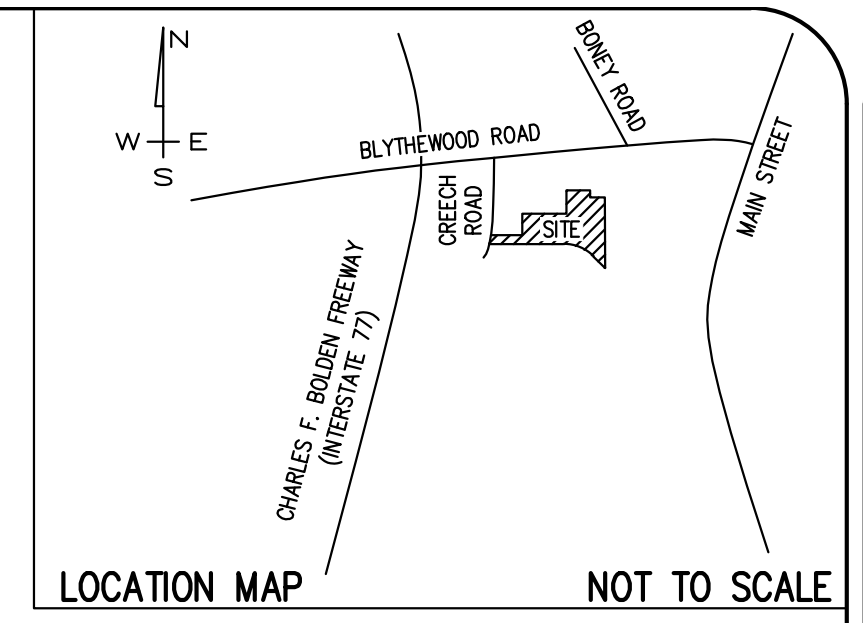
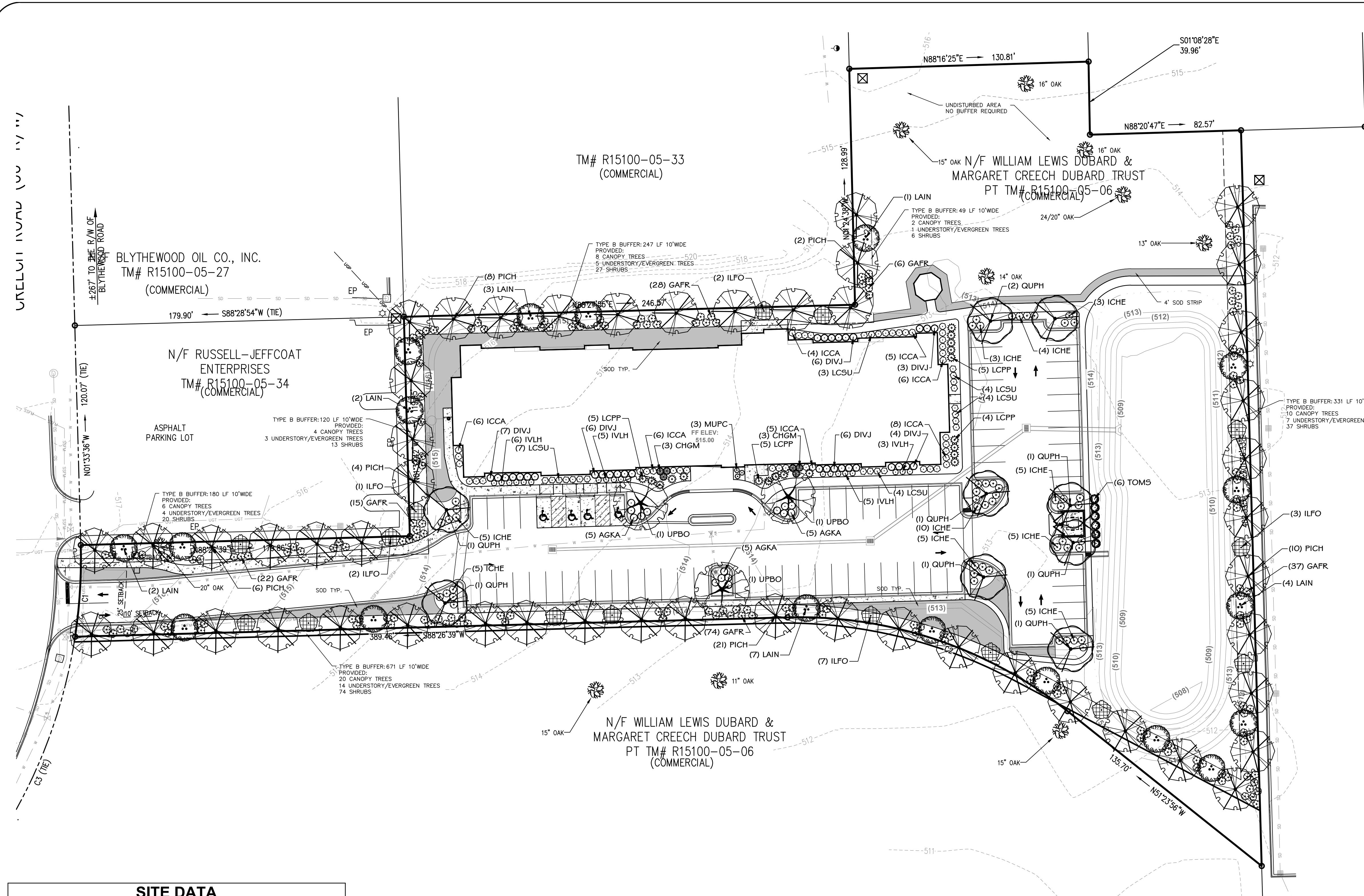
UNIT MIX

DENSITY STUDY- FAMILY
ACREAGE: 2.86 AC

1 BR: 24 UNITS
2 BR: 24 UNITS
TOTAL: 48 UNITS

STORIES: 3

PARKING: 72 SPACES



N/F KJ II
COMP#
TM# R151
(COM)

SITE DATA	
SITE ADDRESS	CREECH ROAD BLYTHEWOOD, SC 29016
OWNER/DEVELOPER	THE PARK AT BLYTHEWOOD, LP 3715 NORTHSIDE PARKWAY, NW BUILDING 200, SUITE 175 ATLANTA, GA 30327 CONTACT: DEVIN BLANKENSHIP
CIVIL ENGINEER	SITE DESIGN, INC. 800 E. WASHINGTON STREET SUITE B GREENVILLE, SC 29601
ARCHITECT	GEHEBER VILLE ASSOCIATES 649 11TH ST. ATLANTA, GA 30318
ZONING	TC (TOWN CENTER DISTRICT) / AO (ARCHITECTURAL OVERLAY DISTRICT)
PROPOSED USE	INDEPENDENT LIVING (48 UNITS)
TAX MAP #	PART TM# R15100-05-06
PARCEL AREA	2.86 ACRES
SETBACKS	FRONT: 25' SIDES: 10' FOR FIRST 40' FROM ROAD REAR: 0'

SYMBOLS		ABBREVIATIONS	
CB	CATCH BASIN	BL	BUILDING LINE
DI	DROP INLET	CL	CENTERLINE
ET	ELEC TRANSFORMER	CT	CRIMP TOP
90.0	ELEVATION (EXIST. GRADE)	DE	DRAINAGE EASEMENT
90.0	ELEVATION (FINISH GRADE)	EP	EDGE OF PAVEMENT
⊕	FIRE HYDRANT	FFE	FINISHED FLOOR ELEVATION
⊕	GAS METER	FG	FINISHED GRADE
⊕	GAS VALVE	IE	INVERT ELEVATION
⊕	IRON PIN	IPS	IRON PIN SET
⊕	IRON PIN OLD	IPO	IRON PIN SET
⊕	LIGHT POLE	N&C	NAIL & CAP
MHSD	MANHOLE (BELLSOUTH)	OT	OPEN TOP
MHSD	MANHOLE (SD)	RB	REBAR
MHSS	MANHOLE (SS)	ROP	REINFORCED CONCRETE PIPE
PP	POWER POLE	R/W	RIGHT OF WAY
TEL	TELEPHONE	SSE	SANITARY SEWER EASEMENT
⊕	WATER METER	SL	SETBACK LINE
⊕	WATER VALVE	VCP	VITRIFIED CLAY PIPE
⊕	STORMWATER FLOW		
→	TRAFFIC FLOW		
LINETYPES		SYMBOLS	
---	CABLE TV	---	SANITARY SEWER - EXIST.
-x-	CHAIN LINK FENCE (PROPOSED)	---	SANITARY SEWER - NEW
-x-	CHAIN LINK FENCE (EXISTING)	---	SILT FENCE
-680-	CONTOURS - EXIST. GRADE	---	STORM SEWER - EXIST.
-(678)-	CONTOURS - FINISHED GRADE	---	STORM SEWER - NEW
-FOC-	FIBER OPTIC	---	UNDERGROUND POWER
-FM-	FORCE MAIN	---	UNDERGROUND TEL
-GAS-	GAS LINE	---	WATER LINE - EXIST.
-OP-	OVERHEAD POWER	---	WATER - NEW
-OT-	OVERHEAD TELEPHONE	---	WOOD FENCE
-RD-	ROOF DRAIN - NEW	---	LIMITS OF DISTURBANCE

LANDSCAPING REQUIREMENTS:

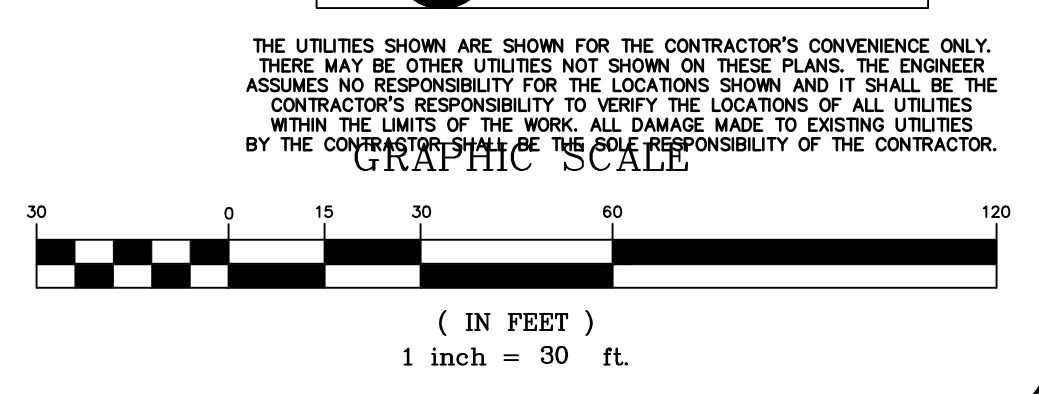
TYPE B BUFFER (AS NOTED ON THIS PLAN)

TYPE C (2) INTERIOR PLANTING
- EACH ROW SHALL TERMINATE IN AN ISLAND OR PLANTER CONTAINING AT LEAST ONE CANOPY SHADE TREE AND A MINIMUM OF 5 FIVE SHRUBS PER TREE. (AS SHOWN ON L100)

TYPE D SERVICE AREAS
- AT LEAST A 75% PERCENT OPAQUE SCREEN FROM THE GROUND TO AT LEAST 1 FOOT HIGHER THAN THE ITEM BEING SCREENED. (AS SHOWN ON L100)

- * COMPLETE PLANT SCHEDULE, DETAILS, AND GENERAL NOTES ON L101
- * ALL DISTURBED AREAS NOT SODDED SHALL BE SEEDED. SEE CIVIL SET FOR GRASSING SCHEDULE.

CAUTION
811 Know what's below.
Call before you dig.



SITE DESIGN, INC.
CIVIL ENGINEERS - SURVEYORS - LANDSCAPE ARCHITECTS

www.sitedesign-inc.com
100 E. WASHINGTON ST., STE B
GREENVILLE, SC 29601
TEL: (864) 271-0400
FAX: (864) 271-0402

NO.	DATE
10.	
9.	
8.	
7.	
6.	
5.	
4.	
3.	
2.	
1.	

SC. REG. NO. 522

THE PARK AT BLYTHEWOOD SENIOR LIVING

CITY OF BLYTHEWOOD
RICHLAND COUNTY
SOUTH CAROLINA

THE PARK AT BLYTHEWOOD
275 NORTHSIDE PWY, NW
BLDG 200, STE 175
ATLANTA, GA 30327

HORIZ. SCALE: 1" = 30'
VERT. SCALE: N/A
DESIGNED BY: JAW
DRAWN BY: JAW
CHECKED BY: AGB
DATE: 2/19/2020
S191285-LANDSCAPE.dwg

LANDSCAPE PLAN

SHEET 170F 18

L100



FRONT ELEVATION



LEFT ELEVATION

RIGHT ELEVATION



GLA-ATL, LLC

649 11TH STREET
ATLANTA, GA 30319
VOICE: 404.228.1958
FAX: 404.228.8350

WWW.GLAATL.COM

THIS DRAWING IS AN INSTRUMENT OF SERVICE AND SHALL REMAIN THE PROPERTY OF THE ARCHITECT. AND SHALL NOT BE REPRODUCED, PUBLISHED OR USED IN ANY WAY WITHOUT THE CONSENT OF THE ARCHITECT. THIS DRAWING SHALL NOT BE SCALED. COPYRIGHT 2019 GLA-ATL, LLC.

RELEASE DATES:

REV # DATE DESCRIPTION

REV #	DATE	DESCRIPTION



STAMP:

CLIENT:
PRESTWICK COMPANIES
3175 NORTHSIDE
PARKWAY NW
BLDG 200, SUITE 175
ATLANTA, GA 30327

PROJECT:
THE PARK AT
BLYTHEWOOD
SENIOR LIVING

CITY OF BLYTHEWOOD
RICHLAND COUNTY
SOUTH CAROLINA

DRAWING TITLE:
RENDERED ELEVATIONS

DRAWN BY: MF CHECKED BY:

SCALE: 3/32"=1'-0" DATE: 2/14/2020

PROJECT NUMBER: 1907

DRAWING NUMBER:

NOT RELEASED FOR CONSTRUCTION





THE PARK AT
BLYTHEWOOD

Traffic Impact Study

Park at Blythewood on Creech Road in Blythewood, SC

For:

Mr. Devin Blankenship
Senior Development Manager
Prestwick Companies
3715 Northside Parkway, NW | Building 200, Suite 175
Atlanta, GA 30327

Prepared by:

Roger D. Dyar, P.E., P.A.
231 Tollison Road
Seneca, SC 29672
(864) 360-7921
dyartraffic@gmail.com

May 14, 2020



Introduction and Background

This traffic impact study was required by the Town of Blythewood, SC for a proposed senior living complex off Creech Road just on the east side of the interchange of I-77 at Blythewood Road. The site location is shown in Figure 1 and will be known as The Park at Blythewood.

Figure 1 Site Location for Proposed Park at Blythewood off Creech Road



Reference is made to a traffic study completed by Roger D. Dyar, P.E., P.A., in 2018 for a motel to be located on the west side of Creech Road. That site is currently under construction. As required and allowed by the Town of Blythewood, the 2018 motel study was used as a basis for this study. The predicted build out traffic volumes with the motel in place and fully operational are taken to be the existing traffic volumes for 2020, including a network-wide increase of 2% from 2019 to 2020 to account for continued background growth in the area. See the appendices for selected information from the 2018 study.

As a result of the review of the motel access plan it was determined that a raised concrete island was to be installed on Creech Road southward from Blythewood Road for a distance of about 125 feet, so as to help force the exits from the motel site to be right turns only due to the close proximity to Blythewood Road. See site plan for the motel site in the appendices.

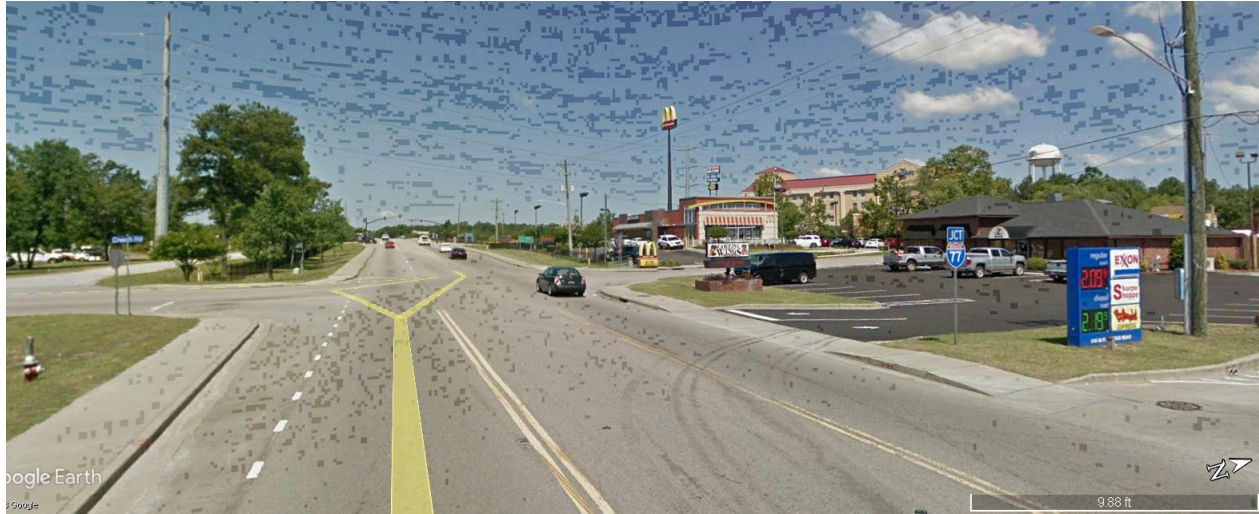
The new Park at Blythewood facility will have 48 rooms in an attached three-story building design. It will have access from Creech Road located about 440 feet south from Blythewood Road. The conceptual site plan for the project is shown in Figure 2.



Aerial of site

This study was done at the request of the Town of Blythewood, using the standard procedures required by SCDOT in their Access and Roadside Management Standards (ARMS) manual. As required by ARMS, we estimated trip generation for the new project using data from the Institute of Transportation Engineers (ITE). The ITE collects, analyzes and assembles traffic generation data from hundreds of land uses all across the U.S.A. The ITE Trip Generation (10th Edition) provides summary data on trip generation based on units of use, typically for daily and morning and afternoon peak hours. These trip generation rates are then used to estimate traffic that will be generated by a site. Traffic is distributed based on an analysis of existing traffic patterns, a market survey and other information that may be available about the project or traffic conditions in the study area. In our case we looked at the daily traffic volumes currently on Blythewood Drive and I-77. Using those numbers it is seen that about 87% of daily traffic is on I-77 or on Blythewood Drive west of I-77, with the remaining 13% on Blythewood Drive east of I-77. To be conservative, we used a figure of 10% of traffic to be oriented to the east on Blythewood Drive, with the remainder to be directed to and from the I-77 interchange.

The study network included the proposed access onto Creech Road and the existing non-signalized intersection of Blythewood Road at Creech Road.



View of Blythewood Road at Creech Road/McNulty Ave looking west

Development of Predicted Future Traffic Volumes

Trip generation for the site was estimated using the Institute of Transportation Engineers (ITE) trip generation manual. The following Table 1 shows the predicted trip generation for the new Park at Blythewood, ITE Land Use Code 252.

Table 1 Trip Generation

Time Period	Trip Generation Rate per unit	Trips In	Trips Out	Trips In	Trips Out	Total Trips
Daily	3.70	50%	50%	89	89	178
A.M. Peak	0.20	35%	65%	3	7	10
P.M. Peak	0.26	55%	45%	7	5	12

As discussed earlier, to be conservative on the high side, we assumed that 90% of the trips generated by the site would be oriented to and from I-77 with the remaining 10% being oriented to and from the east on Blythewood Road. So, 90% of the newly generated trips would make left turns at the stop sign on Creech at Blythewood Road, with the other 10% making right turns onto Blythewood Road. Returning or entering trips would be made in reverse of exiting trips.

See the appendices for detailed information on the development of traffic volumes with the project fully built and occupied in the year 2021, including a 2% background growth from 2020 to 2021. The following Figure 3 shows the predicted traffic volumes added to the study network at full buildout and occupancy of the site in 2021.

Figure 3 Predicted Added Traffic to Study Network

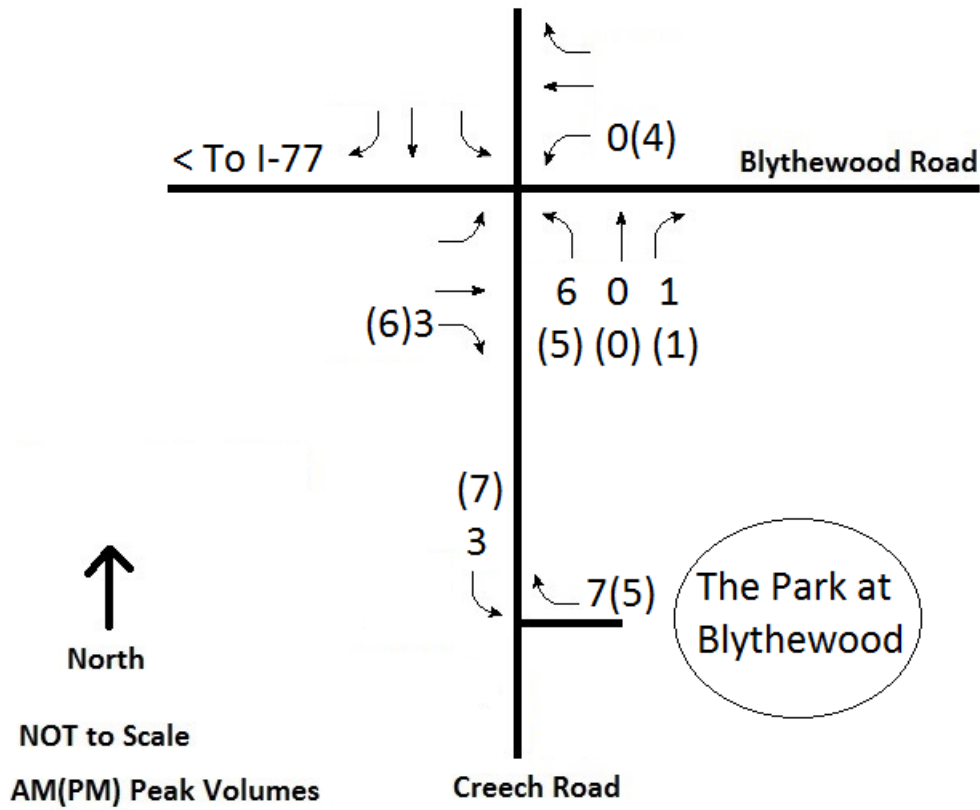
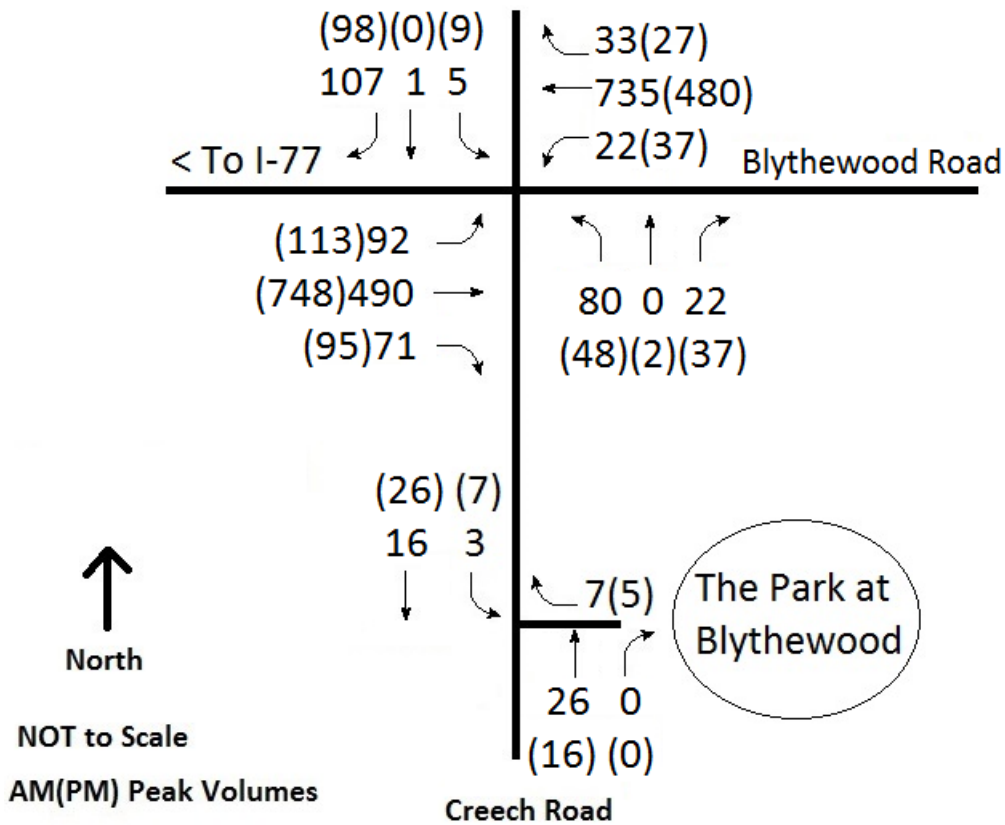


Figure 4 shows the predicted peak hour volumes in 2019 with the new Park at Blythewood in place. This figure includes a 2% growth in background traffic per year from 2019 to 2021.



View to site on Creech Road looking northward

Figure 4 Predicted Peak Hour Volumes in 2021 with New Park at Blythewood in Place



View to Creech Road from McNulty Road

Level of Service

The primary measure of traffic flow quality for the study used the concept of Level of Service (LOS) as defined by the Highway Capacity Manual (HCM). Generally, for intersections the concept of LOS relates the quality of traffic flow to the delay time experienced by drivers. The HCM provides guidance on the use of the concept of level of service for streets and intersections. A tiered system has been established to describe traffic flow and congestion as related to observed and measured or predicted operational values. For intersections the measure is stopped time delay. The following figure provides the HCM criteria for LOS for intersections. As noted, the LOS varies from “A” to “F”, with the quality of traffic service declining as the levels move from “A” towards “F”. With declining LOS, the ability to travel at the desired speed is inhibited by other vehicles either adjacent, opposite or in front of a driver. Generally, in metro area such as Columbia, it is expected that LOS D will be prevalent and be accepted in the morning and afternoon peak hours for most traffic movements. So, any traffic movements with LOS E or worse would need to be reviewed closely to determine if any changes or improvements could be made to move the LOS to an acceptable level. On heavily traveled streets such as Blythewood Road, it is not uncommon for side street left turn movements to have LOS D or E due to the difficulty in making the maneuver. Table 2 below indicates LOS and the thresholds of delay.

Table 2 Levels of Service for Intersections

Level of Service	Signalized Intersections		Non-signalized Intersections	
	Avg. Delay per Vehicle (Seconds)	Description	Avg. Delay per Vehicle (Seconds)	Description
A	< 10	Excellent, most vehicles do not stop	<10	Little or no delay
B	10 – 20	More vehicles must stop, longer delays	10 – 15	Short delays
C	20 – 35	Higher congestion but many do not stop	15 – 25	Average delays
D	35 – 55	Noticeable cycle failures and long delays	25 – 35	Long delays
E	55 – 80	Limit of acceptable operations	35 – 50	Very long delays
F	> 80	Oversaturation, not generally acceptable	> 50	Extreme delays, capacity exceeded

Level of service is graded from A to F to indicate increasing congestion, longer delays and greater limitation in mobility to drivers. Normally, level of service D is considered acceptable at intersections in an urban area. This means that if average delays are less than 55 seconds per vehicle for a particular approach at a traffic signal, then the level of service is considered

acceptable. Similar statements can be made for non-signalized intersections, for example that LOS is C if the average delay is less than 25 seconds. Level of service E is generally considered to be “capacity.” Level of service F indicates a condition where the capacity has been exceeded and extremely long lines of traffic can develop, if there is sufficient demand volume.

Analysis of Traffic with Project in Place

An analysis was made of future traffic conditions with the new Park at Blythewood in place in 2021 using the traffic volumes in Figure 4 and the SimTraffic module of the Synchro software. Ten runs were made for each peak hour studied with a 10-minute network loading period, as normally required by SCDOT. The following Table 3 shows the predicted traffic flow measures in the study network with the new Park at Blythewood in place, using the LOS measures from Table 2.

Table 3 Traffic Flow Results by SimTraffic with New Park at Blythewood in Place in 2021

Intersection	Approach	A.M. Peak Hour			P.M. Peak Hour		
		Delay (sec/veh)	LOS	95 th % Queue (ft)	Delay (sec/veh)	LOS	95 th % Queue (ft)
Blythewood Road at Creech Road	Northbound Lt	45.1	E	101	44.9	E	97
	Northbound Th	0	A	101	17.4	C	97
	Northbound Rt	25.9	D	101	23.6	C	97
	Northbound Tot	41.6	E	101	32.6	D	97
	Southbound Lt	44.7	D	99	27.6	D	81
	Southbound Th	57.5	A	99	0	A	81
	Southbound Rt	18.1	C	99	9.3	A	81
	Eastbound Lt	8.0	A	63	5.4	A	60
	Eastbound Th	1.4	A	2	2.4	A	8
	Eastbound Rt	0.8	A	2	1.7	A	8
	Westbound Lt	6.7	A	33	10.3	B	48
	Westbound Th	3.4	A	6	2.4	A	5
	Westbound Rt	2.7	A	6	2.0	A	5
Creech Road At Access	Westbound Rt	2.8	A	26	2.8	A	20
	Northbound	0.0	A	0	0	A	0
	Southbound	0.2	A	4	0.3	A	4

As seen in Table 3, the new Park at Blythewood access onto Creech Road, the LOS would be A for the right turns that would exit from the site and eventually reach Blythewood Road at both the a.m. and p.m. peak hours. The predicted 95th percentile queue for this movement would be slightly less than one vehicle.

At the intersection of Blythewood Road at Creech Road, the predicted LOS for the side street approach on Creech Road are shown to be at E for the left turns onto Blythewood Road for both the a.m. and p.m. peak hours. This is not unexpected for left turns from a stop sign onto a heavily traveled street in the peak hours in an urban area such as Columbia. The LOS for the right turn movements from Creech Road onto Blythewood Road are shown at D and C for both the a.m. and p.m. peak hours studied. Overall, the LOS for the northbound movement at the stop sign on Creech Road at Blythewood Road is at E in the a.m. peak hour and at D in the p.m. peak hour.

The predicted queue at the 95% level is at about 4 vehicles for both the a.m. and p.m. peak hours. Again, this is not unexpected for a stop sign controlled approach to a heavily traveled major street.

There are no significant queues or delays shown for the traffic movements on Blythewood Road due to the addition of traffic from the new senior living site.

A review was made of the potential benefits of widening Creech Road at its approach to Blythewood Road to provide a separate lane for right turns. An analysis using the SimTraffic module showed that adding a right turn lane would reduce the total delay for the northbound approach to the stop sign by only 5 seconds in both the a.m. and p.m. peak hours, with the 95th percentile queues remaining at about 4 vehicles. So, there would be a very small improvement in traffic conditions at a very large cost by adding a right turn lane on Creech Road at Blythewood Road. In doing so, the LOS would still be at E for the approach for the a.m. peak hour and at D in the p.m. peak hour.

Conclusions

Based on the review of the site and the analysis in this report, the following conclusions are made:

1. The site will produce 178 trips per day which will be newly added to the street system.
2. The overall effects on the peak hour traffic flow are minimal. The overall additions to delays and queues at the intersection of Blythewood Road and Creech Road are not significant. Predicted queues at the 95th percentile level are not excessive that the intersection of Blythewood Road at Creech Road.
3. Traffic flow should be acceptable at the proposed access point onto Creech Road. At this location, all exiting traffic will make a right turn and then have to make a U-turn at some point to the south on Creech Road.
4. Analysis shows that there would be very minimal improvements to traffic flow on Creech Road with the addition of a right turn lane on the stop sign controlled approach. It does not appear the minor accrued benefits to right turn traffic would outweigh the significant cost of adding a lane on this approach.

Recommendations

Based on the review of the data collected and analyzed in this report and reviews of traffic flow at the site, the following recommendations are made:

1. The proposed site access plan should be approved since the site will result in only very minor additions to delay and queues in the study network.
2. The proposed access point onto Creech Road should be designed in conformance to Town of Blythewood guidelines. The details of this design will be handled by the site civil engineer and will need to comply with Town standards as to width, alignment and sight distance.
3. A standard stop sign and painted stop bar should be installed on the new access at its approach to Creech Road in conformance to the standards of the Manual on Uniform Traffic Control Devices.
4. Based on the analysis of the traffic data for conditions with the motel and the senior living complex in place there is not sufficient justification for any mitigation measures.
5. Long-term it is recommended that the Town of Blythewood Road and SCDOT review the possible need for access management on Blythewood Road east of I-77 and possibly the need for another traffic signal somewhere between the northbound I-77 off-ramp and the signal at Blythewood Road at Boney Road (S-40-1367). This should be considered in future plans to improve Blythewood Road and McNulty Road by SCDOT and Richland County.

List of Appendices

1. Information from Traffic Study for Motel on Creech Road in 2018
2. Trip Distribution and Assignment Information
3. SimTraffic Analysis Build AM
4. SimTraffic Analysis Build PM

Appendices

1

Traffic Impact Study

Proposed Motel Creech Road in Blythewood, SC

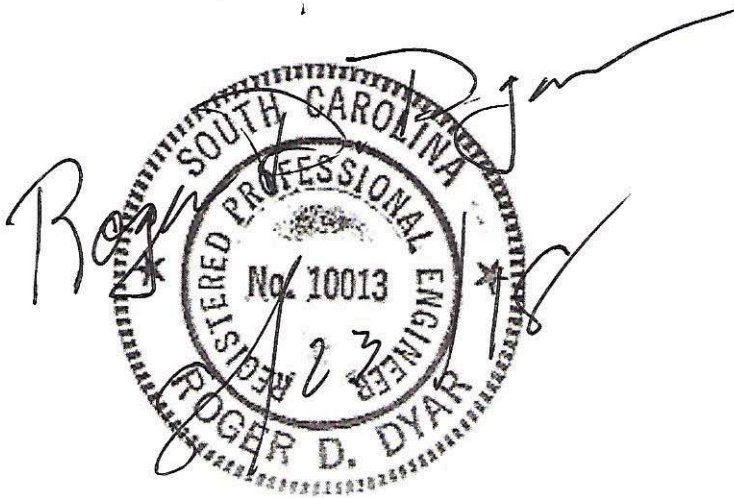
For:

Mr. Jeff Carter
Carter Engineering

Prepared by:

Roger D. Dyar, P.E., P.A.
231 Tollison Road
Seneca, SC 29672
(864) 360-7921
dyartraffic@gmail.com

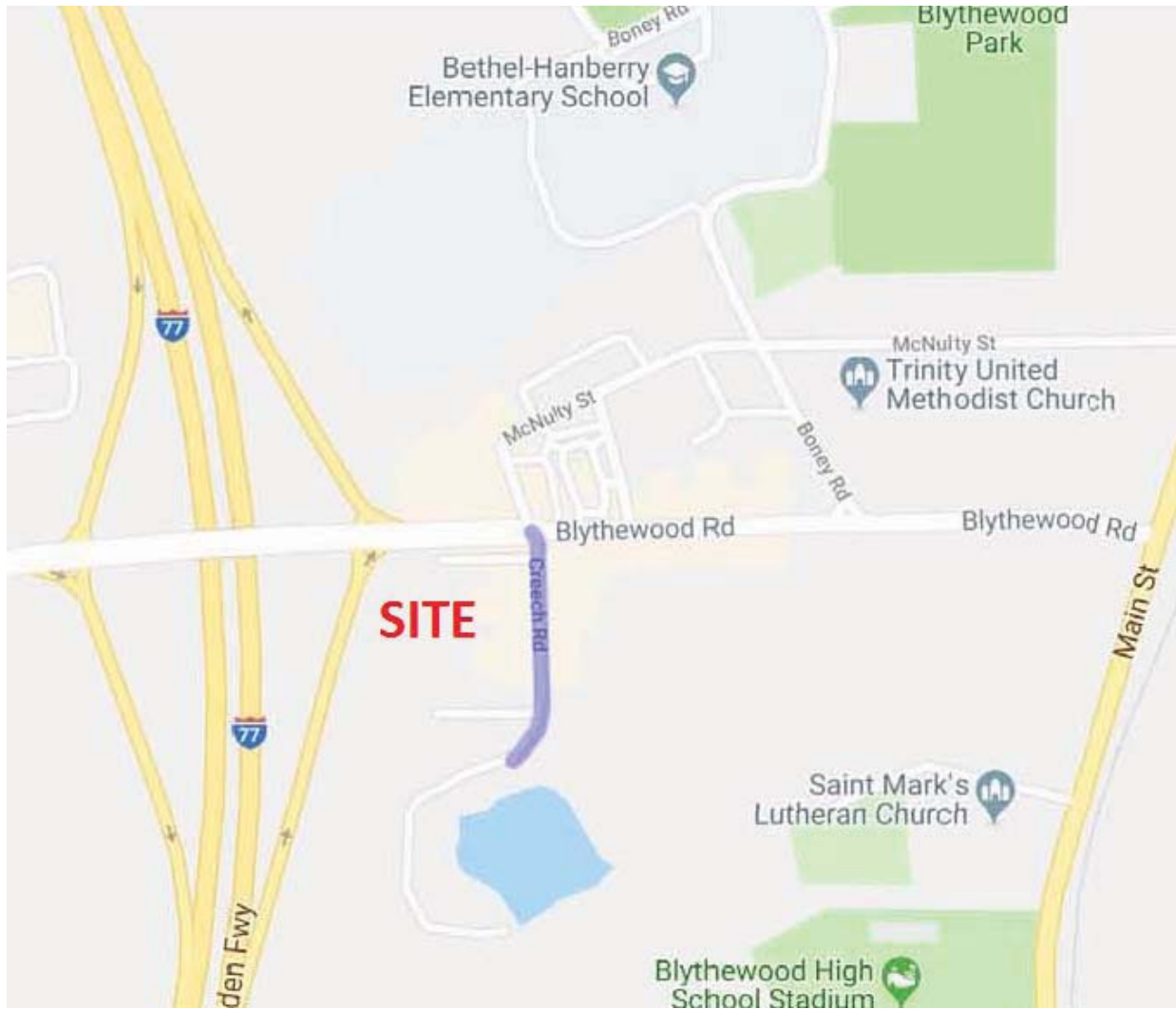
August 23, 2018



Traffic Impact Study for Proposed Motel off Creech Road

This report is an abbreviated traffic impact study required by the Town of Blythewood, SC for a proposed motel to be located off Creech Road just on the east side of the interchange of I-77 at Blythewood Road. The site location is shown in Figure 1.

Figure 1 Site Location for Proposed Motel off Creech Road



The motel will have 88 rooms. It will have access from the existing access to Creech Road which is within SCDOT right-of-way located about 80 feet from Creech Road (measured center to center). It is likely that SCDOT will require some level of improvements to this existing paved roadway. As required by SCDOT, the access will have to be made a right-in/right-out only by means of a raised island to force all exits from the access to be right turns and to prevent any left turn exits or left turn entrances. The conceptual site plan for the project is shown in Figure 2.

Figure 2 Conceptual Site Plan

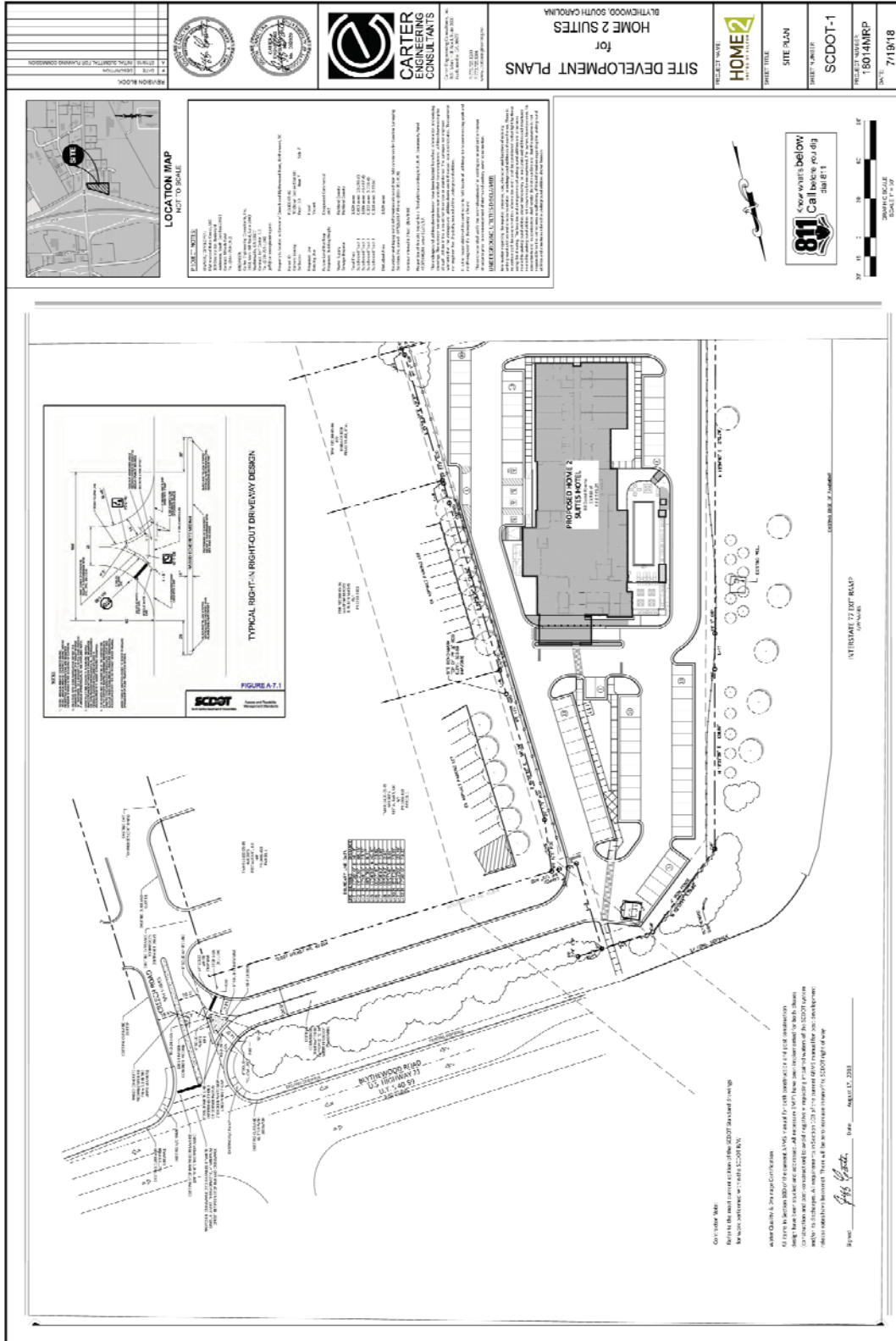


Figure 3 Predicted Added Traffic to Study Network

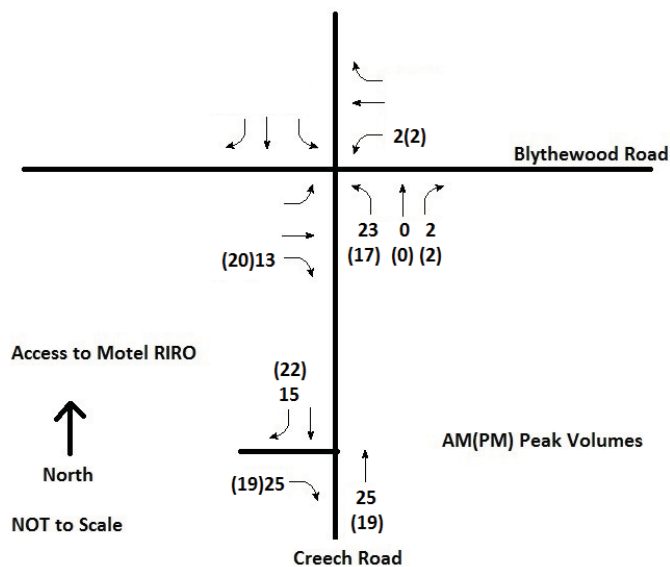
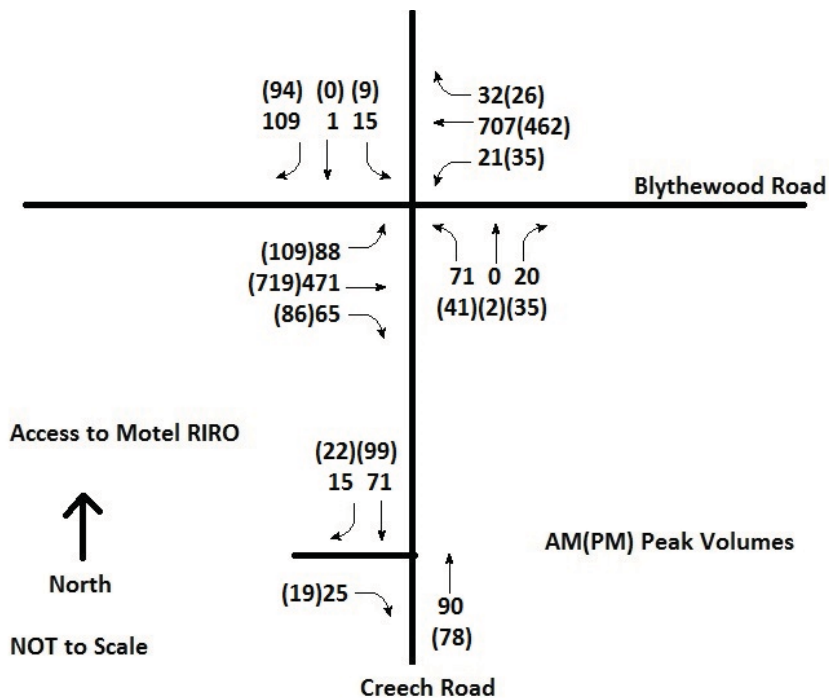


Figure 4 shows the predicted peak hour volumes in 2019 with the new motel in place. This figure includes a 2% growth in background traffic. Historical traffic volume data from SCDOT shows an increase in the average annual daily traffic on Blythewood Road from 10,200 in 2011 to 19,500 in 2017. It is doubtful this rate of increase would continue so a 2% increase was used. Further analysis would be needed to determine a long-term growth rate.

Figure 4 Predicted Peak Hour Volumes in 2019 with New Motel in Place



2

Intersection Turning Movements

Project: Blythewood Senior Living

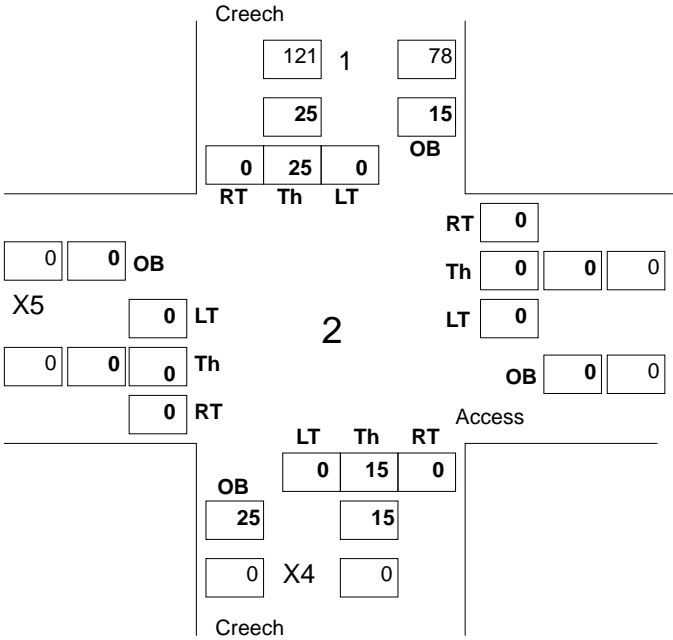
Scenario: Build

Time Period: PM

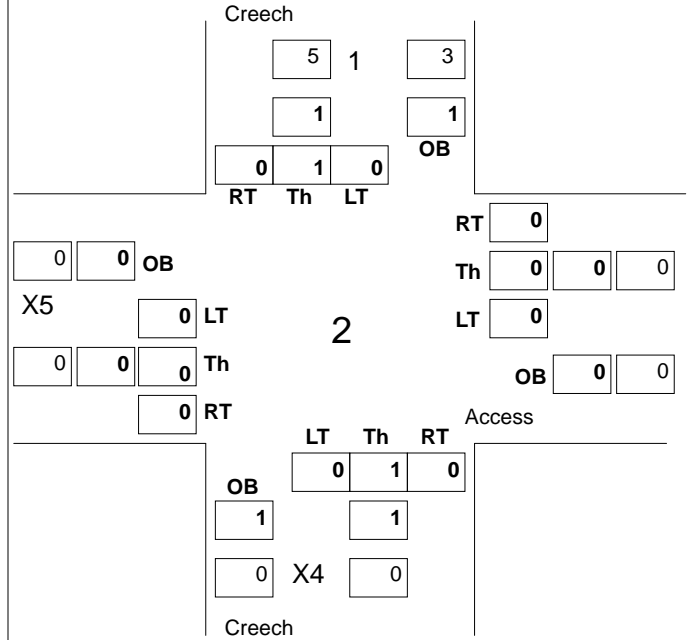
Intersection Name: Creech at Access

Intersection ID: 2

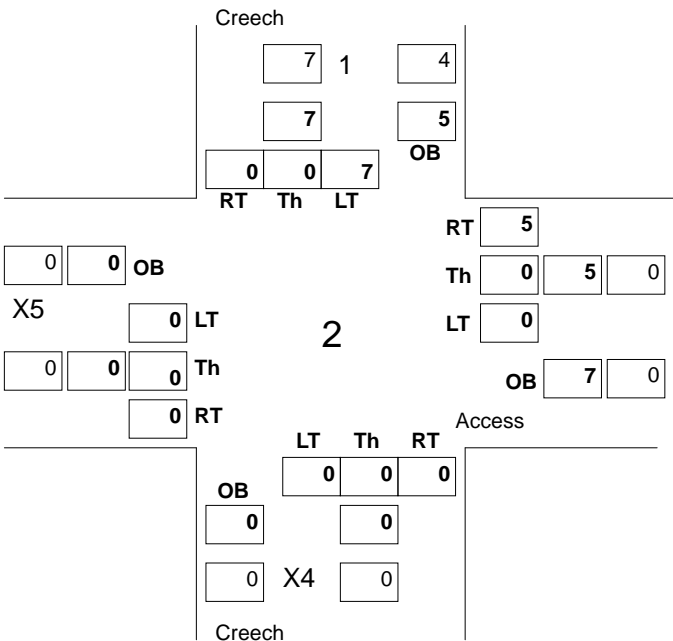
Existing



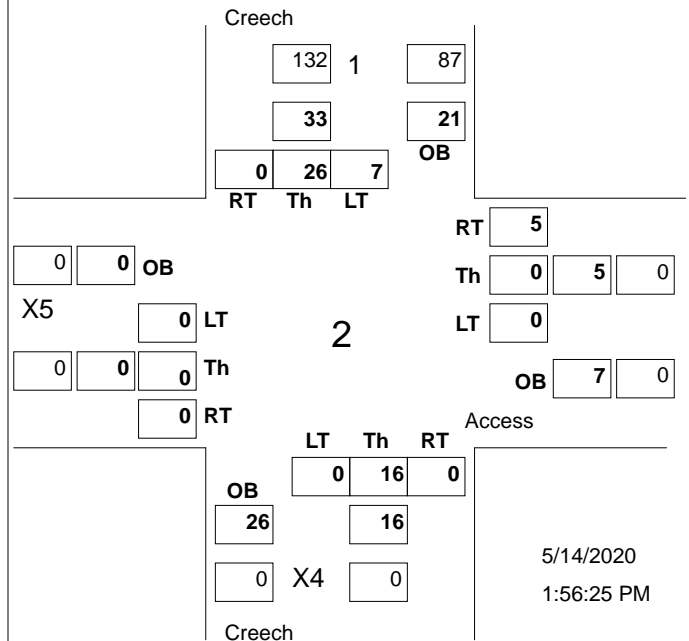
Growth



Site



Future



5/14/2020
1:56:25 PM

Intersection Turning Movements

Project: Blythewood Senior Living

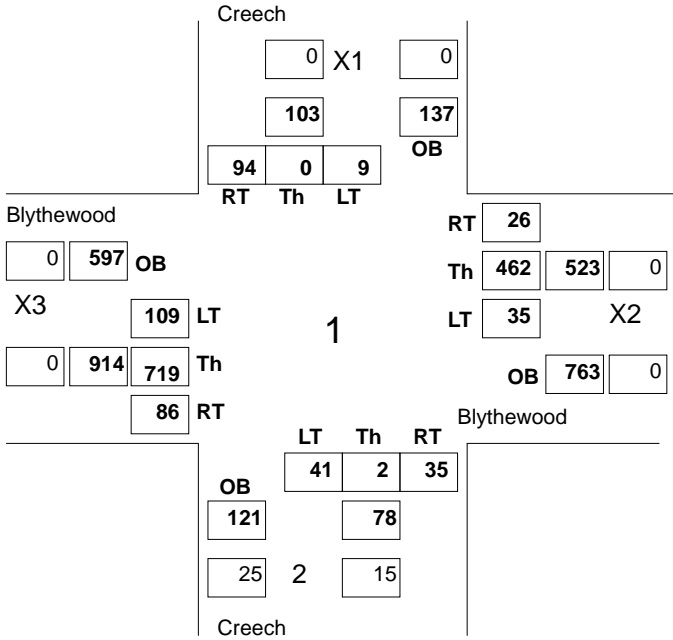
Scenario: Build

Time Period: PM

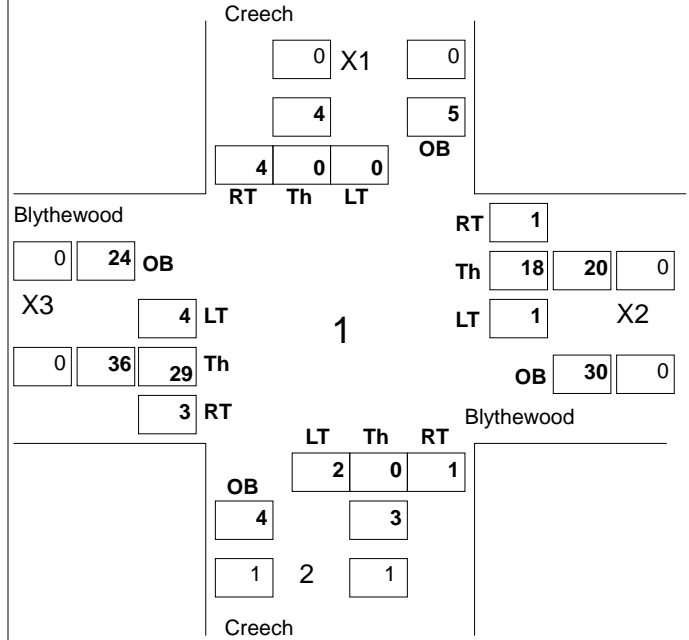
Intersection Name: Blythewood at Creech

Intersection ID: 1

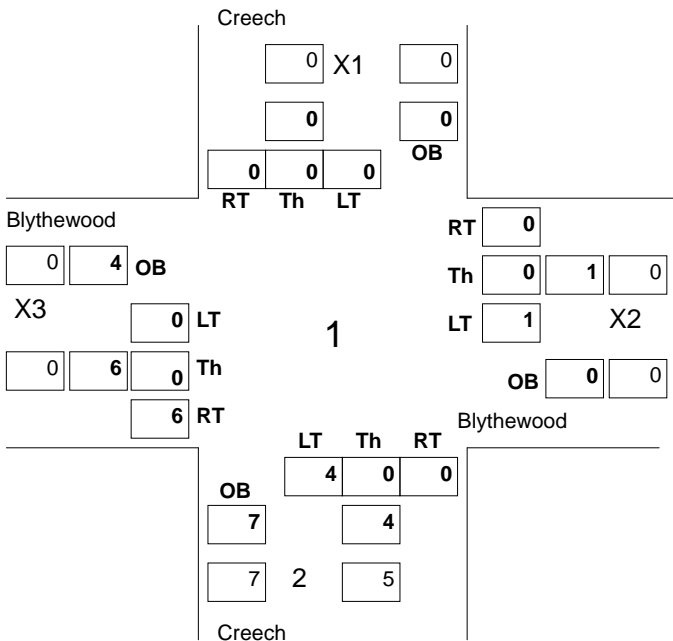
Existing



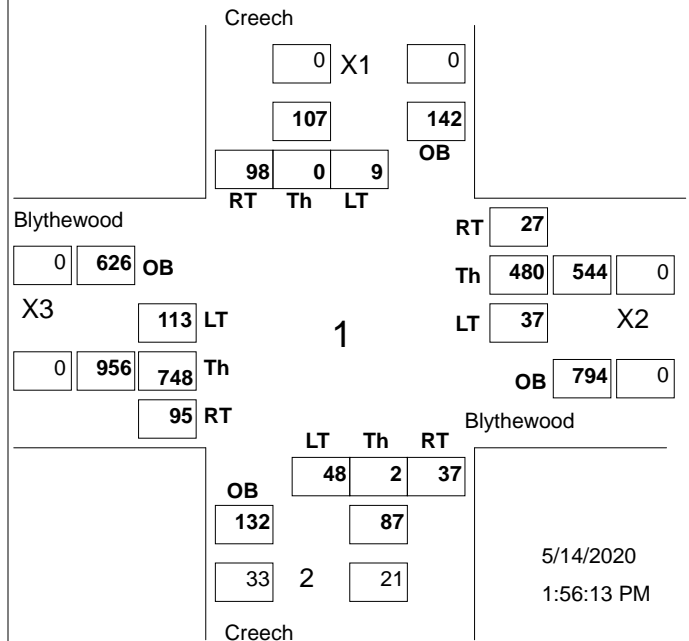
Growth



Site



Future

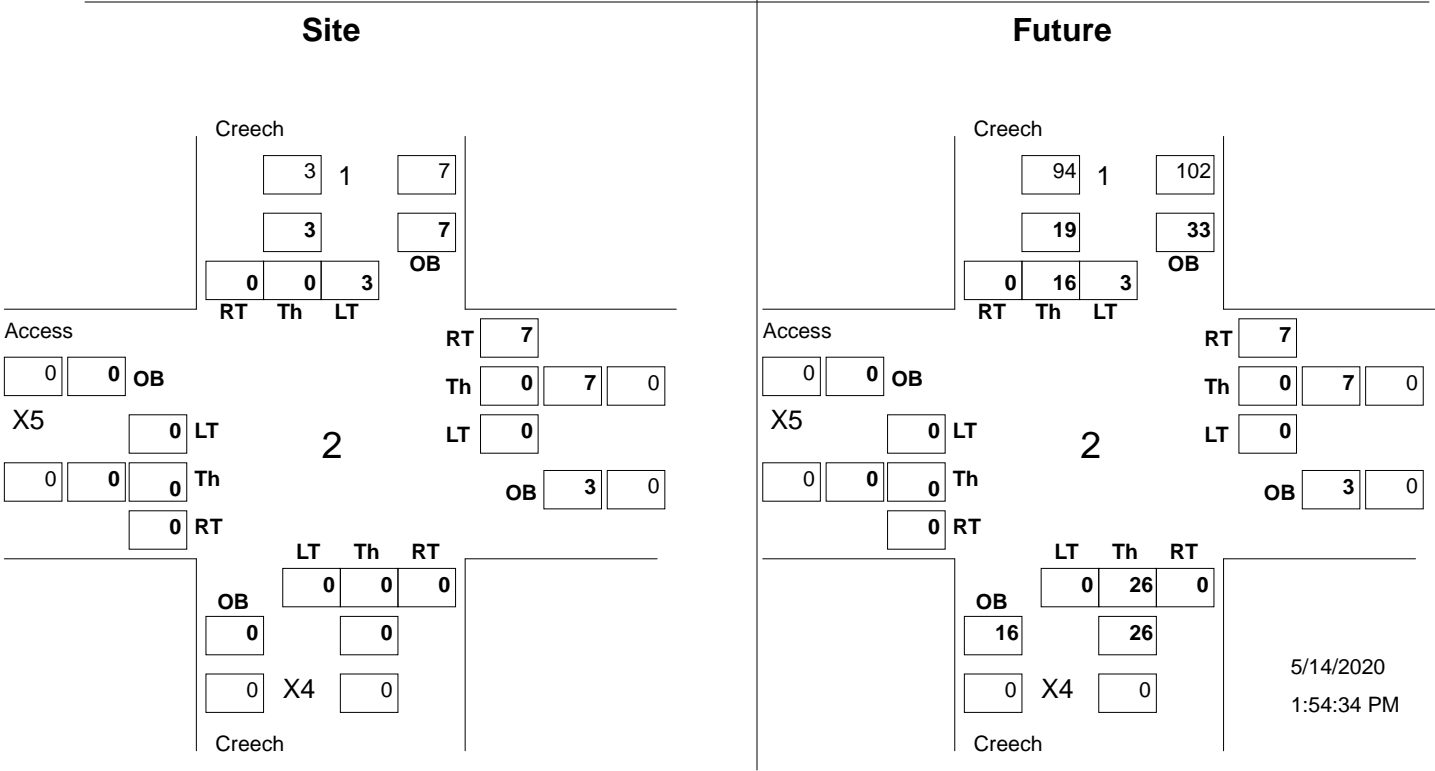
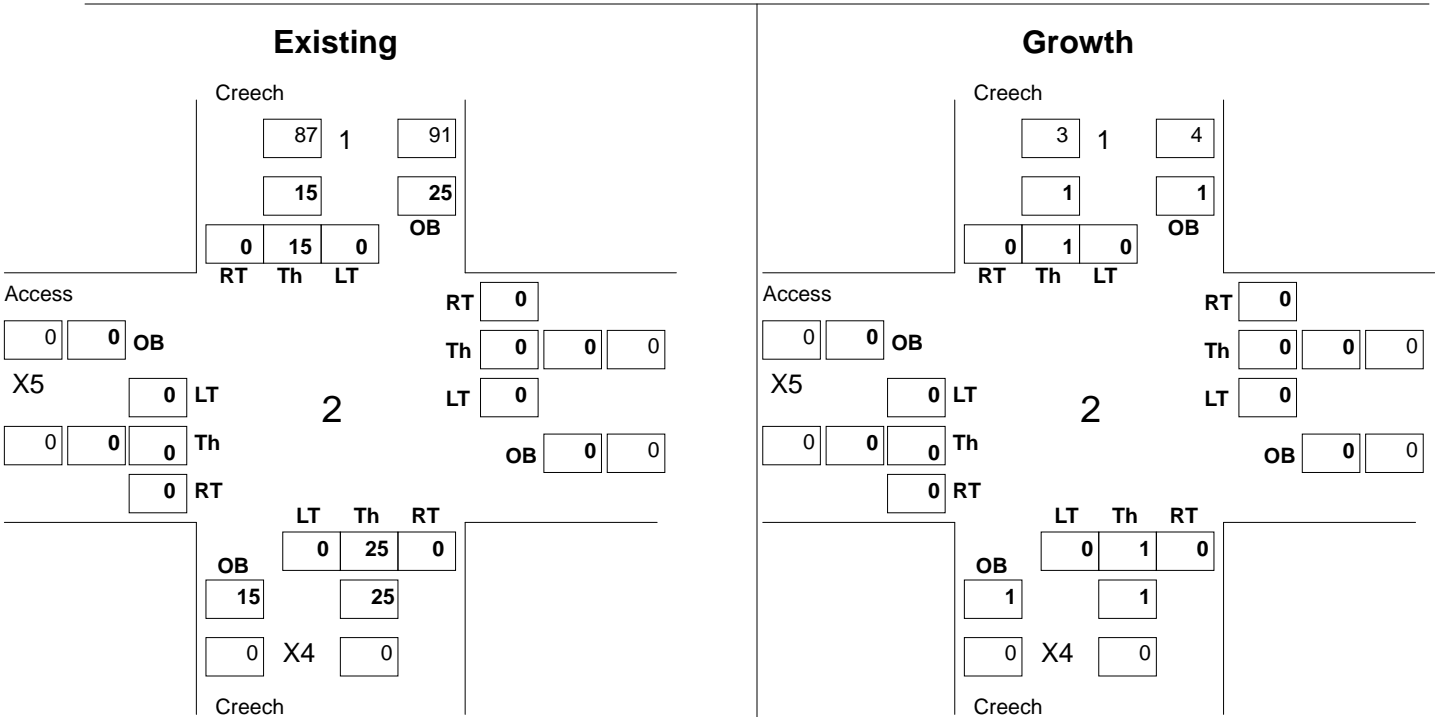


5/14/2020
1:56:13 PM

Intersection Turning Movements

Project: Blythewood Senior Living
Scenario: Build
Time Period: AM

Intersection Name: Creech at Access
Intersection ID: 2



Intersection Turning Movements

Project: Blythewood Senior Living

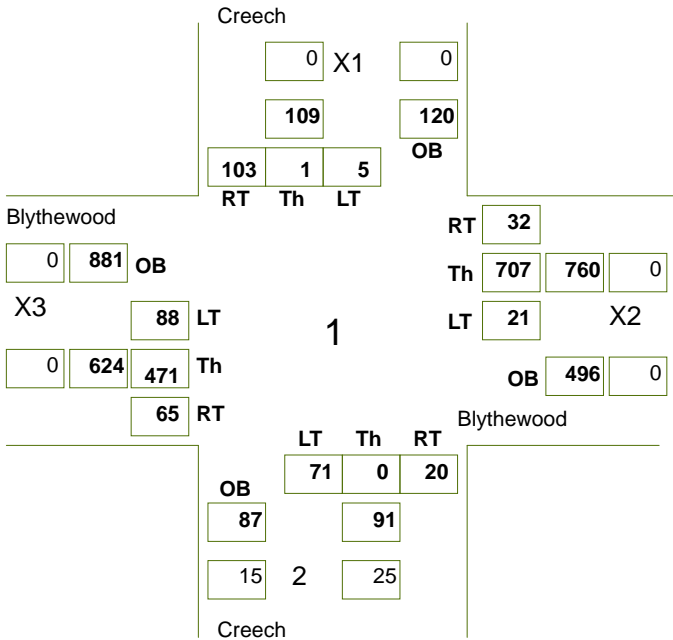
Scenario: Build

Time Period: AM

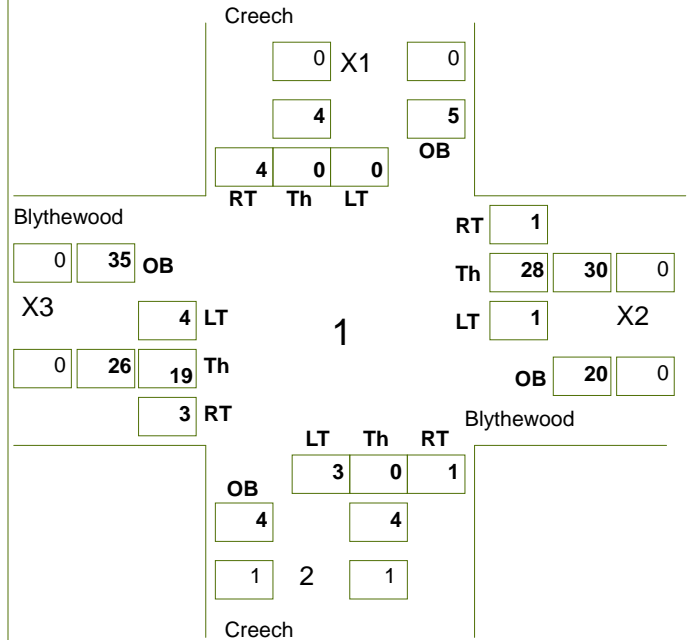
Intersection Name: Blythewood at Creech

Intersection ID: 1

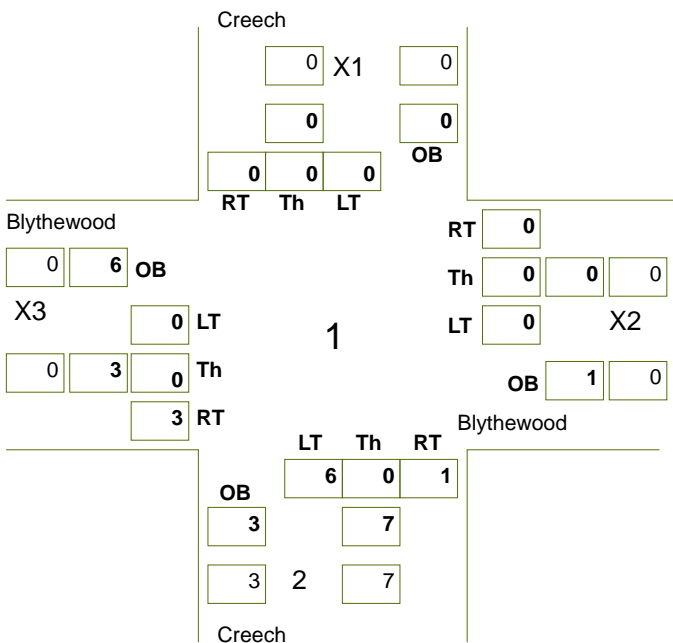
Existing



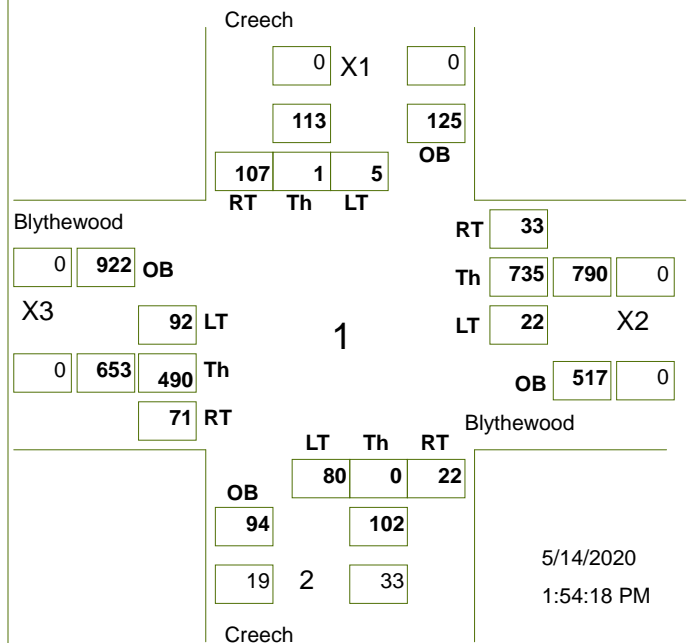
Growth



Site



Future



5/14/2020
1:54:18 PM

3

Summary of All Intervals

Run Number	1	10	2	3	4	5	6
Start Time	7:05	7:05	7:05	7:05	7:05	7:05	7:05
End Time	8:15	8:15	8:15	8:15	8:15	8:15	8:15
Total Time (min)	70	70	70	70	70	70	70
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	5	5	5	5	5	5	5
# of Recorded Intervals	4	4	4	4	4	4	4
Vehs Entered	1766	1650	1708	1645	1690	1690	1703
Vehs Exited	1774	1684	1714	1644	1697	1689	1706
Starting Vehs	28	51	34	27	28	23	25
Ending Vehs	20	17	28	28	21	24	22
Travel Distance (mi)	664	620	638	622	625	635	640
Travel Time (hr)	27.0	27.9	26.8	26.1	25.8	26.2	26.0
Total Delay (hr)	4.0	6.3	4.7	4.7	4.1	4.2	3.8
Total Stops	320	317	329	305	348	305	317
Fuel Used (gal)	21.6	20.9	21.0	20.4	20.5	20.9	20.7

Summary of All Intervals

Run Number	7	8	9	Avg
Start Time	7:05	7:05	7:05	7:05
End Time	8:15	8:15	8:15	8:15
Total Time (min)	70	70	70	70
Time Recorded (min)	60	60	60	60
# of Intervals	5	5	5	5
# of Recorded Intervals	4	4	4	4
Vehs Entered	1762	1671	1753	1704
Vehs Exited	1763	1658	1750	1708
Starting Vehs	23	15	27	26
Ending Vehs	22	28	30	21
Travel Distance (mi)	656	618	657	637
Travel Time (hr)	28.8	24.6	27.1	26.6
Total Delay (hr)	6.0	3.2	4.3	4.5
Total Stops	336	337	316	322
Fuel Used (gal)	21.9	20.1	21.5	20.9

Interval #0 Information Seeding

Start Time 7:05
 End Time 7:15
 Total Time (min) 10
 Volumes adjusted by Growth Factors.
 No data recorded this interval.

Interval #1 Information Recording

Start Time 7:15
 End Time 7:30
 Total Time (min) 15
 Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	435	412	419	417	439	365	400
Vehs Exited	439	442	429	417	446	366	411
Starting Vehs	28	51	34	27	28	23	25
Ending Vehs	24	21	24	27	21	22	14
Travel Distance (mi)	165	163	163	158	164	139	150
Travel Time (hr)	6.7	7.4	6.9	6.4	6.7	5.4	6.0
Total Delay (hr)	1.0	1.7	1.3	0.9	1.0	0.6	0.8
Total Stops	88	71	67	63	89	57	89
Fuel Used (gal)	5.4	5.5	5.3	5.2	5.4	4.4	4.8

Interval #1 Information Recording

Start Time 7:15
 End Time 7:30
 Total Time (min) 15
 Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	413	390	402	409
Vehs Exited	404	375	410	414
Starting Vehs	23	15	27	26
Ending Vehs	32	30	19	22
Travel Distance (mi)	151	140	152	155
Travel Time (hr)	6.2	5.6	6.3	6.4
Total Delay (hr)	1.0	0.7	1.0	1.0
Total Stops	80	84	68	74
Fuel Used (gal)	4.9	4.6	5.0	5.0

Interval #2 Information

Start Time 7:30
 End Time 7:45
 Total Time (min) 15
 Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	426	400	403	378	394	431	410
Vehs Exited	425	405	405	392	387	434	397
Starting Vehs	24	21	24	27	21	22	14
Ending Vehs	25	16	22	13	28	19	27
Travel Distance (mi)	158	149	149	148	148	162	151
Travel Time (hr)	6.5	6.1	5.9	6.3	5.7	6.8	6.2
Total Delay (hr)	1.0	0.9	0.8	1.3	0.6	1.2	1.0
Total Stops	81	77	79	74	64	87	78
Fuel Used (gal)	5.2	4.9	4.9	4.9	4.6	5.4	4.9

Interval #2 Information

Start Time 7:30
 End Time 7:45
 Total Time (min) 15
 Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	445	410	418	412
Vehs Exited	450	410	412	413
Starting Vehs	32	30	19	22
Ending Vehs	27	30	25	21
Travel Distance (mi)	168	150	156	154
Travel Time (hr)	7.1	5.9	6.2	6.3
Total Delay (hr)	1.2	0.7	0.8	1.0
Total Stops	84	85	75	78
Fuel Used (gal)	5.6	4.8	5.0	5.0

Interval #3 Information

Start Time 7:45
End Time 8:00
Total Time (min) 15
Volumes adjusted by PHF, Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	458	434	440	425	467	491	460
Vehs Exited	461	417	432	406	462	477	458
Starting Vehs	25	16	22	13	28	19	27
Ending Vehs	22	33	30	32	33	33	29
Travel Distance (mi)	172	152	162	157	169	181	175
Travel Time (hr)	6.9	7.2	6.9	6.4	7.6	7.8	7.0
Total Delay (hr)	0.9	1.9	1.2	1.0	1.7	1.5	1.0
Total Stops	72	92	86	82	115	92	70
Fuel Used (gal)	5.5	5.3	5.3	5.0	5.7	6.1	5.6

Interval #3 Information

Start Time 7:45
End Time 8:00
Total Time (min) 15
Volumes adjusted by PHF, Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	496	442	486	460
Vehs Exited	488	448	482	453
Starting Vehs	27	30	25	21
Ending Vehs	35	24	29	29
Travel Distance (mi)	182	169	186	171
Travel Time (hr)	8.8	6.8	7.8	7.3
Total Delay (hr)	2.4	1.0	1.4	1.4
Total Stops	95	81	76	85
Fuel Used (gal)	6.2	5.5	6.0	5.6

Interval #4 Information

Start Time 8:00
 End Time 8:15
 Total Time (min) 15
 Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	447	404	446	425	390	403	433
Vehs Exited	449	420	448	429	402	412	440
Starting Vehs	22	33	30	32	33	33	29
Ending Vehs	20	17	28	28	21	24	22
Travel Distance (mi)	169	155	164	158	145	154	164
Travel Time (hr)	7.0	7.1	7.2	6.9	5.8	6.2	6.6
Total Delay (hr)	1.1	1.8	1.4	1.5	0.8	0.9	1.0
Total Stops	79	77	97	86	80	69	80
Fuel Used (gal)	5.6	5.2	5.4	5.3	4.8	5.0	5.3

Interval #4 Information

Start Time 8:00
 End Time 8:15
 Total Time (min) 15
 Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	408	429	447	424
Vehs Exited	421	425	446	430
Starting Vehs	35	24	29	29
Ending Vehs	22	28	30	21
Travel Distance (mi)	155	159	163	159
Travel Time (hr)	6.7	6.3	6.7	6.7
Total Delay (hr)	1.3	0.7	1.1	1.2
Total Stops	77	87	97	83
Fuel Used (gal)	5.1	5.2	5.4	5.2

3: Creech/McNulty & Blythewood Performance by approach

Approach	EB	WB	NB	SB	All
Denied Del/Veh (s)	1.1	0.7	0.0	0.2	0.8
Total Del/Veh (s)	2.3	3.4	41.6	19.4	6.4
Stop/Veh	0.10	0.01	0.73	0.99	0.16

6: Creech & Motel Access Performance by approach

Approach	EB	NB	SB	All
Denied Del/Veh (s)	0.1	1.3	0.0	0.6
Total Del/Veh (s)	2.6	15.0	0.1	7.5
Stop/Veh	1.00	0.24	0.00	0.23

8: Creech & Park at Blythewood Performance by approach

Approach	WB	NB	SB	All
Denied Del/Veh (s)	0.1	0.1	0.0	0.0
Total Del/Veh (s)	2.8	0.0	0.2	0.3
Stop/Veh	1.00	0.00	0.00	0.05

Total Network Performance

Denied Del/Veh (s)	0.8
Total Del/Veh (s)	8.6
Stop/Veh	0.19

Intersection: 3: Creech/McNulty & Blythewood

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	76	2	44	11	99	132
Average Queue (ft)	35	0	9	0	60	53
95th Queue (ft)	63	2	33	6	101	99
Link Distance (ft)		568		1674	16	263
Upstream Blk Time (%)					52	
Queuing Penalty (veh)					55	
Storage Bay Dist (ft)	150		150			
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 6: Creech & Motel Access

Movement	EB	NB
Directions Served	R	T
Maximum Queue (ft)	47	125
Average Queue (ft)	17	24
95th Queue (ft)	44	99
Link Distance (ft)	132	293
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 8: Creech & Park at Blythewood

Movement	WB	SB
Directions Served	LR	LT
Maximum Queue (ft)	30	6
Average Queue (ft)	6	0
95th Queue (ft)	26	4
Link Distance (ft)	230	293
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Network Summary

Network wide Queuing Penalty: 55

Summary of All Intervals

Run Number	1	10	2	3	4	5	6
Start Time	7:05	7:05	7:05	7:05	7:05	7:05	7:05
End Time	8:15	8:15	8:15	8:15	8:15	8:15	8:15
Total Time (min)	70	70	70	70	70	70	70
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	5	5	5	5	5	5	5
# of Recorded Intervals	4	4	4	4	4	4	4
Vehs Entered	1766	1650	1708	1645	1690	1690	1703
Vehs Exited	1774	1684	1714	1644	1697	1689	1706
Starting Vehs	28	51	34	27	28	23	25
Ending Vehs	20	17	28	28	21	24	22
Travel Distance (mi)	664	620	638	622	625	635	640
Travel Time (hr)	27.0	27.9	26.8	26.1	25.8	26.2	26.0
Total Delay (hr)	4.0	6.3	4.7	4.7	4.1	4.2	3.8
Total Stops	320	317	329	305	348	305	317
Fuel Used (gal)	21.6	20.9	21.0	20.4	20.5	20.9	20.7

Summary of All Intervals

Run Number	7	8	9	Avg
Start Time	7:05	7:05	7:05	7:05
End Time	8:15	8:15	8:15	8:15
Total Time (min)	70	70	70	70
Time Recorded (min)	60	60	60	60
# of Intervals	5	5	5	5
# of Recorded Intervals	4	4	4	4
Vehs Entered	1762	1671	1753	1704
Vehs Exited	1763	1658	1750	1708
Starting Vehs	23	15	27	26
Ending Vehs	22	28	30	21
Travel Distance (mi)	656	618	657	637
Travel Time (hr)	28.8	24.6	27.1	26.6
Total Delay (hr)	6.0	3.2	4.3	4.5
Total Stops	336	337	316	322
Fuel Used (gal)	21.9	20.1	21.5	20.9

Interval #0 Information Seeding

Start Time 7:05
 End Time 7:15
 Total Time (min) 10
 Volumes adjusted by Growth Factors.
 No data recorded this interval.

Interval #1 Information Recording

Start Time 7:15
End Time 7:30
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	435	412	419	417	439	365	400
Vehs Exited	439	442	429	417	446	366	411
Starting Vehs	28	51	34	27	28	23	25
Ending Vehs	24	21	24	27	21	22	14
Travel Distance (mi)	165	163	163	158	164	139	150
Travel Time (hr)	6.7	7.4	6.9	6.4	6.7	5.4	6.0
Total Delay (hr)	1.0	1.7	1.3	0.9	1.0	0.6	0.8
Total Stops	88	71	67	63	89	57	89
Fuel Used (gal)	5.4	5.5	5.3	5.2	5.4	4.4	4.8

Interval #1 Information Recording

Start Time 7:15
End Time 7:30
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	413	390	402	409
Vehs Exited	404	375	410	414
Starting Vehs	23	15	27	26
Ending Vehs	32	30	19	22
Travel Distance (mi)	151	140	152	155
Travel Time (hr)	6.2	5.6	6.3	6.4
Total Delay (hr)	1.0	0.7	1.0	1.0
Total Stops	80	84	68	74
Fuel Used (gal)	4.9	4.6	5.0	5.0

Interval #2 Information

Start Time 7:30
End Time 7:45
Total Time (min) 15
Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	426	400	403	378	394	431	410
Vehs Exited	425	405	405	392	387	434	397
Starting Vehs	24	21	24	27	21	22	14
Ending Vehs	25	16	22	13	28	19	27
Travel Distance (mi)	158	149	149	148	148	162	151
Travel Time (hr)	6.5	6.1	5.9	6.3	5.7	6.8	6.2
Total Delay (hr)	1.0	0.9	0.8	1.3	0.6	1.2	1.0
Total Stops	81	77	79	74	64	87	78
Fuel Used (gal)	5.2	4.9	4.9	4.9	4.6	5.4	4.9

Interval #2 Information

Start Time 7:30
End Time 7:45
Total Time (min) 15
Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	445	410	418	412
Vehs Exited	450	410	412	413
Starting Vehs	32	30	19	22
Ending Vehs	27	30	25	21
Travel Distance (mi)	168	150	156	154
Travel Time (hr)	7.1	5.9	6.2	6.3
Total Delay (hr)	1.2	0.7	0.8	1.0
Total Stops	84	85	75	78
Fuel Used (gal)	5.6	4.8	5.0	5.0

Interval #3 Information

Start Time 7:45
End Time 8:00
Total Time (min) 15
Volumes adjusted by PHF, Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	458	434	440	425	467	491	460
Vehs Exited	461	417	432	406	462	477	458
Starting Vehs	25	16	22	13	28	19	27
Ending Vehs	22	33	30	32	33	33	29
Travel Distance (mi)	172	152	162	157	169	181	175
Travel Time (hr)	6.9	7.2	6.9	6.4	7.6	7.8	7.0
Total Delay (hr)	0.9	1.9	1.2	1.0	1.7	1.5	1.0
Total Stops	72	92	86	82	115	92	70
Fuel Used (gal)	5.5	5.3	5.3	5.0	5.7	6.1	5.6

Interval #3 Information

Start Time 7:45
End Time 8:00
Total Time (min) 15
Volumes adjusted by PHF, Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	496	442	486	460
Vehs Exited	488	448	482	453
Starting Vehs	27	30	25	21
Ending Vehs	35	24	29	29
Travel Distance (mi)	182	169	186	171
Travel Time (hr)	8.8	6.8	7.8	7.3
Total Delay (hr)	2.4	1.0	1.4	1.4
Total Stops	95	81	76	85
Fuel Used (gal)	6.2	5.5	6.0	5.6

Interval #4 Information

Start Time 8:00
End Time 8:15
Total Time (min) 15
Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	447	404	446	425	390	403	433
Vehs Exited	449	420	448	429	402	412	440
Starting Vehs	22	33	30	32	33	33	29
Ending Vehs	20	17	28	28	21	24	22
Travel Distance (mi)	169	155	164	158	145	154	164
Travel Time (hr)	7.0	7.1	7.2	6.9	5.8	6.2	6.6
Total Delay (hr)	1.1	1.8	1.4	1.5	0.8	0.9	1.0
Total Stops	79	77	97	86	80	69	80
Fuel Used (gal)	5.6	5.2	5.4	5.3	4.8	5.0	5.3

Interval #4 Information

Start Time 8:00
End Time 8:15
Total Time (min) 15
Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	408	429	447	424
Vehs Exited	421	425	446	430
Starting Vehs	35	24	29	29
Ending Vehs	22	28	30	21
Travel Distance (mi)	155	159	163	159
Travel Time (hr)	6.7	6.3	6.7	6.7
Total Delay (hr)	1.3	0.7	1.1	1.2
Total Stops	77	87	97	83
Fuel Used (gal)	5.1	5.2	5.4	5.2

3: Creech/McNulty & Blythewood Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBR	SBL	SBT	SBR	All
Denied Del/Veh (s)	3.2	0.7	0.7	1.7	0.7	0.6	0.0	0.0	0.2	0.1	0.2	0.8
Total Del/Veh (s)	8.0	1.4	0.8	6.7	3.4	2.7	45.1	25.9	44.7	57.5	18.1	6.4
Stop/Veh	0.70	0.00	0.00	0.52	0.00	0.03	0.72	0.71	1.00	1.00	0.99	0.16

6: Creech & Motel Access Performance by movement

Movement	EBR	NBT	SBT	SBR	All
Denied Del/Veh (s)	0.1	1.3	0.0	0.0	0.6
Total Del/Veh (s)	2.6	15.0	0.1	0.2	7.5
Stop/Veh	1.00	0.24	0.00	0.00	0.23

8: Creech & Park at Blythewood Performance by movement

Movement	WBR	NBT	SBL	SBT	All
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0
Total Del/Veh (s)	2.8	0.0	1.8	0.2	0.3
Stop/Veh	1.00	0.00	0.00	0.00	0.05

Total Network Performance

Denied Del/Veh (s)	0.8
Total Del/Veh (s)	8.6
Stop/Veh	0.19

Intersection: 3: Creech/McNulty & Blythewood

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	76	2	44	11	99	132
Average Queue (ft)	35	0	9	0	60	53
95th Queue (ft)	63	2	33	6	101	99
Link Distance (ft)		568		1674	16	263
Upstream Blk Time (%)					52	
Queuing Penalty (veh)					55	
Storage Bay Dist (ft)	150		150			
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 6: Creech & Motel Access

Movement	EB	NB
Directions Served	R	T
Maximum Queue (ft)	47	125
Average Queue (ft)	17	24
95th Queue (ft)	44	99
Link Distance (ft)	132	293
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 8: Creech & Park at Blythewood

Movement	WB	SB
Directions Served	LR	LT
Maximum Queue (ft)	30	6
Average Queue (ft)	6	0
95th Queue (ft)	26	4
Link Distance (ft)	230	293
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Network Summary

Network wide Queuing Penalty: 55

Summary of All Intervals

Run Number	1	10	2	3	4	5	6
Start Time	7:05	7:05	7:05	7:05	7:05	7:05	7:05
End Time	8:15	8:15	8:15	8:15	8:15	8:15	8:15
Total Time (min)	70	70	70	70	70	70	70
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	5	5	5	5	5	5	5
# of Recorded Intervals	4	4	4	4	4	4	4
Vehs Entered	1749	1647	1741	1663	1716	1684	1723
Vehs Exited	1752	1674	1749	1655	1719	1683	1723
Starting Vehs	25	45	34	19	26	24	27
Ending Vehs	22	18	26	27	23	25	27
Travel Distance (mi)	652	612	642	618	630	621	644
Travel Time (hr)	26.6	27.0	26.8	24.6	25.8	25.9	26.0
Total Delay (hr)	4.0	5.7	4.6	3.3	3.9	4.5	3.7
Total Stops	312	311	315	299	332	320	316
Fuel Used (gal)	21.2	20.4	21.0	20.0	20.6	20.6	20.9

Summary of All Intervals

Run Number	7	8	9	Avg
Start Time	7:05	7:05	7:05	7:05
End Time	8:15	8:15	8:15	8:15
Total Time (min)	70	70	70	70
Time Recorded (min)	60	60	60	60
# of Intervals	5	5	5	5
# of Recorded Intervals	4	4	4	4
Vehs Entered	1759	1714	1738	1713
Vehs Exited	1760	1702	1742	1715
Starting Vehs	23	17	33	25
Ending Vehs	22	29	29	23
Travel Distance (mi)	644	627	651	634
Travel Time (hr)	27.6	24.9	27.3	26.3
Total Delay (hr)	5.2	3.1	4.7	4.3
Total Stops	343	335	326	322
Fuel Used (gal)	21.3	20.4	21.3	20.8

Interval #0 Information Seeding

Start Time 7:05
 End Time 7:15
 Total Time (min) 10
 Volumes adjusted by Growth Factors.
 No data recorded this interval.

Interval #1 Information Recording

Start Time 7:15
End Time 7:30
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	416	406	410	438	439	374	409
Vehs Exited	416	431	419	429	445	376	422
Starting Vehs	25	45	34	19	26	24	27
Ending Vehs	25	20	25	28	20	22	14
Travel Distance (mi)	156	159	156	164	163	140	154
Travel Time (hr)	6.4	6.5	6.3	6.6	6.5	5.4	6.1
Total Delay (hr)	0.9	0.9	1.0	1.0	0.8	0.6	0.8
Total Stops	74	69	67	69	83	63	86
Fuel Used (gal)	5.1	5.2	5.0	5.4	5.2	4.5	5.0

Interval #1 Information Recording

Start Time 7:15
End Time 7:30
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	405	401	421	411
Vehs Exited	401	388	434	415
Starting Vehs	23	17	33	25
Ending Vehs	27	30	20	22
Travel Distance (mi)	146	142	160	154
Travel Time (hr)	5.9	5.7	6.9	6.2
Total Delay (hr)	0.8	0.7	1.3	0.9
Total Stops	81	86	74	75
Fuel Used (gal)	4.8	4.6	5.3	5.0

Interval #2 Information

Start Time 7:30
End Time 7:45
Total Time (min) 15
Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	430	398	410	368	413	420	408
Vehs Exited	429	402	412	380	403	420	401
Starting Vehs	25	20	25	28	20	22	14
Ending Vehs	26	16	23	16	30	22	21
Travel Distance (mi)	159	147	150	142	152	153	151
Travel Time (hr)	6.4	5.9	6.1	5.4	5.9	6.4	5.8
Total Delay (hr)	0.9	0.8	0.9	0.6	0.7	1.1	0.6
Total Stops	77	79	78	65	69	85	77
Fuel Used (gal)	5.2	4.8	5.0	4.5	4.8	5.1	4.8

Interval #2 Information

Start Time 7:30
End Time 7:45
Total Time (min) 15
Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	452	421	404	413
Vehs Exited	452	425	394	412
Starting Vehs	27	30	20	22
Ending Vehs	27	26	30	22
Travel Distance (mi)	168	154	148	152
Travel Time (hr)	6.7	6.1	6.3	6.1
Total Delay (hr)	0.9	0.8	1.1	0.8
Total Stops	85	84	80	77
Fuel Used (gal)	5.4	4.9	4.9	4.9

Interval #3 Information

Start Time 7:45
End Time 8:00
Total Time (min) 15
Volumes adjusted by PHF, Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	461	429	458	427	460	476	468
Vehs Exited	465	415	451	415	459	463	458
Starting Vehs	26	16	23	16	30	22	21
Ending Vehs	22	30	30	28	31	35	31
Travel Distance (mi)	170	152	167	154	168	173	173
Travel Time (hr)	6.9	7.0	6.9	6.2	7.4	7.4	7.3
Total Delay (hr)	1.0	1.7	1.1	0.8	1.6	1.4	1.3
Total Stops	84	85	88	77	97	98	78
Fuel Used (gal)	5.5	5.2	5.5	5.0	5.6	5.8	5.7

Interval #3 Information

Start Time 7:45
End Time 8:00
Total Time (min) 15
Volumes adjusted by PHF, Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	496	441	483	460
Vehs Exited	489	445	484	454
Starting Vehs	27	26	30	22
Ending Vehs	34	22	29	27
Travel Distance (mi)	180	167	185	169
Travel Time (hr)	8.6	6.7	7.7	7.2
Total Delay (hr)	2.3	0.9	1.3	1.3
Total Stops	100	76	78	85
Fuel Used (gal)	6.1	5.5	6.0	5.6

Interval #4 Information

Start Time 8:00
End Time 8:15
Total Time (min) 15
Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	442	414	463	430	404	414	438
Vehs Exited	442	426	467	431	412	424	442
Starting Vehs	22	30	30	28	31	35	31
Ending Vehs	22	18	26	27	23	25	27
Travel Distance (mi)	167	154	169	158	148	154	166
Travel Time (hr)	7.0	7.6	7.5	6.3	6.0	6.8	6.8
Total Delay (hr)	1.2	2.2	1.6	0.9	0.8	1.4	1.1
Total Stops	77	78	82	88	83	74	75
Fuel Used (gal)	5.5	5.3	5.6	5.1	4.9	5.2	5.4

Interval #4 Information

Start Time 8:00
End Time 8:15
Total Time (min) 15
Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	406	451	430	428
Vehs Exited	418	444	430	434
Starting Vehs	34	22	29	27
Ending Vehs	22	29	29	23
Travel Distance (mi)	151	164	157	159
Travel Time (hr)	6.4	6.4	6.4	6.7
Total Delay (hr)	1.2	0.7	1.0	1.2
Total Stops	77	89	94	82
Fuel Used (gal)	5.0	5.4	5.2	5.3

3: Creech/McNulty & Blythewood Performance by approach

Approach	EB	WB	NB	SB	All
Denied Del/Veh (s)	1.1	0.7	0.0	0.2	0.8
Total Del/Veh (s)	2.3	3.4	36.6	18.5	6.0
Stop/Veh	0.10	0.01	0.86	0.99	0.17

6: Creech & Motel Access Performance by approach

Approach	EB	NB	SB	All
Denied Del/Veh (s)	0.1	5.3	0.0	2.5
Total Del/Veh (s)	2.8	7.5	0.1	3.9
Stop/Veh	1.00	0.09	0.00	0.16

8: Creech & Park at Blythewood Performance by approach

Approach	WB	NB	SB	All
Denied Del/Veh (s)	0.1	0.1	0.0	0.1
Total Del/Veh (s)	2.4	0.0	0.3	0.5
Stop/Veh	1.00	0.00	0.00	0.14

10: Creech Performance by approach

Approach	NB	SB	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	0.2	0.1	0.2
Stop/Veh	0.00	0.00	0.00

Total Network Performance

Denied Del/Veh (s)	1.1
Total Del/Veh (s)	7.7
Stop/Veh	0.19

Intersection: 3: Creech/McNulty & Blythewood

Movement	EB	EB	WB	WB	NB	NB	SB
Directions Served	L	TR	L	TR	LT	R	LTR
Maximum Queue (ft)	75	11	31	10	91	40	136
Average Queue (ft)	34	0	8	0	52	17	54
95th Queue (ft)	63	6	28	5	92	43	105
Link Distance (ft)		568		1662	16	16	263
Upstream Blk Time (%)					50	3	
Queuing Penalty (veh)					26	2	
Storage Bay Dist (ft)	150		150				
Storage Blk Time (%)							
Queuing Penalty (veh)							

Intersection: 6: Creech & Motel Access

Movement	EB	NB	SB
Directions Served	R	T	TR
Maximum Queue (ft)	57	72	2
Average Queue (ft)	18	11	0
95th Queue (ft)	48	54	2
Link Distance (ft)	132	106	16
Upstream Blk Time (%)		0	0
Queuing Penalty (veh)		0	0
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 8: Creech & Park at Blythewood

Movement	WB
Directions Served	LR
Maximum Queue (ft)	31
Average Queue (ft)	6
95th Queue (ft)	27
Link Distance (ft)	229
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	
Storage Blk Time (%)	
Queuing Penalty (veh)	

Intersection: 10: Creech

Movement

Directions Served
Maximum Queue (ft)
Average Queue (ft)
95th Queue (ft)
Link Distance (ft)
Upstream Blk Time (%)
Queuing Penalty (veh)
Storage Bay Dist (ft)
Storage Blk Time (%)
Queuing Penalty (veh)

Network Summary

Network wide Queuing Penalty: 28

4

Summary of All Intervals

Run Number	1	10	2	3	4	5	6
Start Time	4:50	4:50	4:50	4:50	4:50	4:50	4:50
End Time	6:00	6:00	6:00	6:00	6:00	6:00	6:00
Total Time (min)	70	70	70	70	70	70	70
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	5	5	5	5	5	5	5
# of Recorded Intervals	4	4	4	4	4	4	4
Vehs Entered	1791	1816	1736	1821	1719	1674	1855
Vehs Exited	1788	1812	1726	1817	1724	1682	1843
Starting Vehs	30	18	19	20	26	25	22
Ending Vehs	33	22	29	24	21	17	34
Travel Distance (mi)	663	676	636	669	642	627	691
Travel Time (hr)	27.3	28.0	25.7	27.7	26.0	24.9	29.8
Total Delay (hr)	4.3	4.6	3.7	4.5	3.7	3.2	5.9
Total Stops	316	323	327	329	282	289	315
Fuel Used (gal)	21.6	22.1	20.7	22.0	20.8	20.3	22.8

Summary of All Intervals

Run Number	7	8	9	Avg
Start Time	4:50	4:50	4:50	4:50
End Time	6:00	6:00	6:00	6:00
Total Time (min)	70	70	70	70
Time Recorded (min)	60	60	60	60
# of Intervals	5	5	5	5
# of Recorded Intervals	4	4	4	4
Vehs Entered	1751	1743	1826	1772
Vehs Exited	1739	1745	1821	1768
Starting Vehs	15	25	22	20
Ending Vehs	27	23	27	25
Travel Distance (mi)	649	640	675	657
Travel Time (hr)	26.7	25.4	27.5	26.9
Total Delay (hr)	4.1	3.2	4.1	4.1
Total Stops	280	293	333	308
Fuel Used (gal)	21.3	20.8	22.2	21.5

Interval #0 Information Seeding

Start Time 4:50
 End Time 5:00
 Total Time (min) 10
 Volumes adjusted by Growth Factors.
 No data recorded this interval.

Interval #1 Information Recording

Start Time 5:00
End Time 5:15
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	430	456	410	459	398	399	421
Vehs Exited	431	447	402	440	400	402	414
Starting Vehs	30	18	19	20	26	25	22
Ending Vehs	29	27	27	39	24	22	29
Travel Distance (mi)	159	168	150	162	148	151	157
Travel Time (hr)	6.3	7.1	5.8	6.7	5.7	5.9	6.1
Total Delay (hr)	0.8	1.3	0.6	1.1	0.6	0.7	0.6
Total Stops	67	74	85	93	58	77	73
Fuel Used (gal)	5.2	5.5	4.8	5.3	4.6	4.8	5.0

Interval #1 Information Recording

Start Time 5:00
End Time 5:15
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	436	438	441	428
Vehs Exited	415	437	441	421
Starting Vehs	15	25	22	20
Ending Vehs	36	26	22	24
Travel Distance (mi)	159	160	164	158
Travel Time (hr)	6.4	6.4	6.7	6.3
Total Delay (hr)	0.9	0.8	1.1	0.8
Total Stops	57	80	81	74
Fuel Used (gal)	5.2	5.2	5.4	5.1

Interval #2 Information

Start Time 5:15
End Time 5:30
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	443	433	410	404	396	436	447
Vehs Exited	449	439	412	421	402	431	451
Starting Vehs	29	27	27	39	24	22	29
Ending Vehs	23	21	25	22	18	27	25
Travel Distance (mi)	162	164	151	155	151	159	162
Travel Time (hr)	6.5	6.5	5.8	6.1	5.9	6.3	6.6
Total Delay (hr)	0.9	0.8	0.7	0.8	0.7	0.7	0.9
Total Stops	91	70	68	63	60	80	101
Fuel Used (gal)	5.2	5.3	4.8	5.0	4.8	5.2	5.4

Interval #2 Information

Start Time 5:15
End Time 5:30
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	456	460	459	436
Vehs Exited	456	465	451	439
Starting Vehs	36	26	22	24
Ending Vehs	36	21	30	24
Travel Distance (mi)	167	169	169	161
Travel Time (hr)	6.9	6.7	6.8	6.4
Total Delay (hr)	1.1	0.8	1.0	0.8
Total Stops	81	87	88	79
Fuel Used (gal)	5.5	5.5	5.5	5.2

Interval #3 Information

Start Time 5:30
End Time 5:45
Total Time (min) 15
Volumes adjusted by PHF, Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	473	497	460	496	465	456	508
Vehs Exited	464	471	458	480	464	456	496
Starting Vehs	23	21	25	22	18	27	25
Ending Vehs	32	47	27	38	19	27	37
Travel Distance (mi)	173	179	168	178	175	170	189
Travel Time (hr)	7.6	7.7	6.7	7.5	7.3	7.1	7.8
Total Delay (hr)	1.6	1.4	0.9	1.3	1.2	1.2	1.3
Total Stops	75	93	81	90	83	80	72
Fuel Used (gal)	5.8	5.9	5.5	5.9	5.8	5.7	6.1

Interval #3 Information

Start Time 5:30
End Time 5:45
Total Time (min) 15
Volumes adjusted by PHF, Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	450	413	473	469
Vehs Exited	463	405	480	464
Starting Vehs	36	21	30	24
Ending Vehs	23	29	23	29
Travel Distance (mi)	172	152	179	174
Travel Time (hr)	7.2	5.9	7.5	7.2
Total Delay (hr)	1.3	0.6	1.3	1.2
Total Stops	82	59	69	78
Fuel Used (gal)	5.7	4.8	5.8	5.7

Interval #4 Information

Start Time 5:45
End Time 6:00
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	445	430	456	462	460	383	479
Vehs Exited	444	455	454	476	458	393	482
Starting Vehs	32	47	27	38	19	27	37
Ending Vehs	33	22	29	24	21	17	34
Travel Distance (mi)	168	164	168	174	169	147	183
Travel Time (hr)	6.9	6.8	7.4	7.4	7.1	5.7	9.3
Total Delay (hr)	1.0	1.1	1.5	1.4	1.2	0.6	3.0
Total Stops	83	86	93	83	81	52	69
Fuel Used (gal)	5.4	5.4	5.6	5.7	5.6	4.7	6.3

Interval #4 Information

Start Time 5:45
End Time 6:00
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	409	432	453	441
Vehs Exited	405	438	449	447
Starting Vehs	23	29	23	29
Ending Vehs	27	23	27	25
Travel Distance (mi)	152	158	164	165
Travel Time (hr)	6.1	6.4	6.5	7.0
Total Delay (hr)	0.9	0.9	0.8	1.2
Total Stops	60	67	95	77
Fuel Used (gal)	4.9	5.2	5.4	5.4

3: Creech/McNulty & Blythewood Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBR	All
Denied Del/Veh (s)	3.1	1.2	1.3	1.7	0.5	0.5	0.0	0.0	0.0	0.2	0.2	1.0
Total Del/Veh (s)	5.4	2.4	1.7	10.3	2.4	2.0	44.9	17.4	23.6	27.6	9.3	5.0
Stop/Veh	0.49	0.00	0.01	0.69	0.00	0.00	0.81	0.35	0.81	0.89	1.00	0.15

6: Creech & Access Rd Performance by movement

Movement	EBR	NBT	SBT	SBR	All
Denied Del/Veh (s)	0.1	0.6	0.0	0.0	0.3
Total Del/Veh (s)	2.7	10.3	0.2	0.2	4.4
Stop/Veh	1.00	0.18	0.00	0.00	0.17

8: Creech & Park Blythewood Acc Performance by movement

Movement	WBR	NBT	SBL	SBT	All
Denied Del/Veh (s)	0.1	0.1	0.0	0.0	0.0
Total Del/Veh (s)	2.8	0.0	2.0	0.2	0.3
Stop/Veh	1.00	0.00	0.00	0.00	0.03

Total Network Performance

Denied Del/Veh (s)	1.0
Total Del/Veh (s)	7.3
Stop/Veh	0.17

Intersection: 3: Creech/McNulty & Blythewood

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	68	24	57	9	96	107
Average Queue (ft)	30	1	20	0	55	45
95th Queue (ft)	60	8	48	5	97	81
Link Distance (ft)		568		1674	16	263
Upstream Blk Time (%)					41	
Queuing Penalty (veh)					43	
Storage Bay Dist (ft)	150		150			
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 6: Creech & Access Rd

Movement	EB	NB
Directions Served	R	T
Maximum Queue (ft)	53	104
Average Queue (ft)	19	15
95th Queue (ft)	46	72
Link Distance (ft)	131	293
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 8: Creech & Park Blythewood Acc

Movement	WB	SB
Directions Served	LR	LT
Maximum Queue (ft)	31	3
Average Queue (ft)	4	0
95th Queue (ft)	20	4
Link Distance (ft)	229	293
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Network Summary

Network wide Queuing Penalty: 43

Summary of All Intervals

Run Number	1	10	2	3	4	5	6
Start Time	4:50	4:50	4:50	4:50	4:50	4:50	4:50
End Time	6:00	6:00	6:00	6:00	6:00	6:00	6:00
Total Time (min)	70	70	70	70	70	70	70
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	5	5	5	5	5	5	5
# of Recorded Intervals	4	4	4	4	4	4	4
Vehs Entered	1791	1816	1736	1821	1719	1674	1855
Vehs Exited	1788	1812	1726	1817	1724	1682	1843
Starting Vehs	30	18	19	20	26	25	22
Ending Vehs	33	22	29	24	21	17	34
Travel Distance (mi)	663	676	636	669	642	627	691
Travel Time (hr)	27.3	28.0	25.7	27.7	26.0	24.9	29.8
Total Delay (hr)	4.3	4.6	3.7	4.5	3.7	3.2	5.9
Total Stops	316	323	327	329	282	289	315
Fuel Used (gal)	21.6	22.1	20.7	22.0	20.8	20.3	22.8

Summary of All Intervals

Run Number	7	8	9	Avg
Start Time	4:50	4:50	4:50	4:50
End Time	6:00	6:00	6:00	6:00
Total Time (min)	70	70	70	70
Time Recorded (min)	60	60	60	60
# of Intervals	5	5	5	5
# of Recorded Intervals	4	4	4	4
Vehs Entered	1751	1743	1826	1772
Vehs Exited	1739	1745	1821	1768
Starting Vehs	15	25	22	20
Ending Vehs	27	23	27	25
Travel Distance (mi)	649	640	675	657
Travel Time (hr)	26.7	25.4	27.5	26.9
Total Delay (hr)	4.1	3.2	4.1	4.1
Total Stops	280	293	333	308
Fuel Used (gal)	21.3	20.8	22.2	21.5

Interval #0 Information Seeding

Start Time 4:50
 End Time 5:00
 Total Time (min) 10
 Volumes adjusted by Growth Factors.
 No data recorded this interval.

Interval #1 Information Recording

Start Time 5:00
End Time 5:15
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	430	456	410	459	398	399	421
Vehs Exited	431	447	402	440	400	402	414
Starting Vehs	30	18	19	20	26	25	22
Ending Vehs	29	27	27	39	24	22	29
Travel Distance (mi)	159	168	150	162	148	151	157
Travel Time (hr)	6.3	7.1	5.8	6.7	5.7	5.9	6.1
Total Delay (hr)	0.8	1.3	0.6	1.1	0.6	0.7	0.6
Total Stops	67	74	85	93	58	77	73
Fuel Used (gal)	5.2	5.5	4.8	5.3	4.6	4.8	5.0

Interval #1 Information Recording

Start Time 5:00
End Time 5:15
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	436	438	441	428
Vehs Exited	415	437	441	421
Starting Vehs	15	25	22	20
Ending Vehs	36	26	22	24
Travel Distance (mi)	159	160	164	158
Travel Time (hr)	6.4	6.4	6.7	6.3
Total Delay (hr)	0.9	0.8	1.1	0.8
Total Stops	57	80	81	74
Fuel Used (gal)	5.2	5.2	5.4	5.1

Interval #2 Information

Start Time 5:15
End Time 5:30
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	443	433	410	404	396	436	447
Vehs Exited	449	439	412	421	402	431	451
Starting Vehs	29	27	27	39	24	22	29
Ending Vehs	23	21	25	22	18	27	25
Travel Distance (mi)	162	164	151	155	151	159	162
Travel Time (hr)	6.5	6.5	5.8	6.1	5.9	6.3	6.6
Total Delay (hr)	0.9	0.8	0.7	0.8	0.7	0.7	0.9
Total Stops	91	70	68	63	60	80	101
Fuel Used (gal)	5.2	5.3	4.8	5.0	4.8	5.2	5.4

Interval #2 Information

Start Time 5:15
End Time 5:30
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	456	460	459	436
Vehs Exited	456	465	451	439
Starting Vehs	36	26	22	24
Ending Vehs	36	21	30	24
Travel Distance (mi)	167	169	169	161
Travel Time (hr)	6.9	6.7	6.8	6.4
Total Delay (hr)	1.1	0.8	1.0	0.8
Total Stops	81	87	88	79
Fuel Used (gal)	5.5	5.5	5.5	5.2

Interval #3 Information

Start Time 5:30
End Time 5:45
Total Time (min) 15
Volumes adjusted by PHF, Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	473	497	460	496	465	456	508
Vehs Exited	464	471	458	480	464	456	496
Starting Vehs	23	21	25	22	18	27	25
Ending Vehs	32	47	27	38	19	27	37
Travel Distance (mi)	173	179	168	178	175	170	189
Travel Time (hr)	7.6	7.7	6.7	7.5	7.3	7.1	7.8
Total Delay (hr)	1.6	1.4	0.9	1.3	1.2	1.2	1.3
Total Stops	75	93	81	90	83	80	72
Fuel Used (gal)	5.8	5.9	5.5	5.9	5.8	5.7	6.1

Interval #3 Information

Start Time 5:30
End Time 5:45
Total Time (min) 15
Volumes adjusted by PHF, Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	450	413	473	469
Vehs Exited	463	405	480	464
Starting Vehs	36	21	30	24
Ending Vehs	23	29	23	29
Travel Distance (mi)	172	152	179	174
Travel Time (hr)	7.2	5.9	7.5	7.2
Total Delay (hr)	1.3	0.6	1.3	1.2
Total Stops	82	59	69	78
Fuel Used (gal)	5.7	4.8	5.8	5.7

Interval #4 Information

Start Time 5:45
End Time 6:00
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	445	430	456	462	460	383	479
Vehs Exited	444	455	454	476	458	393	482
Starting Vehs	32	47	27	38	19	27	37
Ending Vehs	33	22	29	24	21	17	34
Travel Distance (mi)	168	164	168	174	169	147	183
Travel Time (hr)	6.9	6.8	7.4	7.4	7.1	5.7	9.3
Total Delay (hr)	1.0	1.1	1.5	1.4	1.2	0.6	3.0
Total Stops	83	86	93	83	81	52	69
Fuel Used (gal)	5.4	5.4	5.6	5.7	5.6	4.7	6.3

Interval #4 Information

Start Time 5:45
End Time 6:00
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	409	432	453	441
Vehs Exited	405	438	449	447
Starting Vehs	23	29	23	29
Ending Vehs	27	23	27	25
Travel Distance (mi)	152	158	164	165
Travel Time (hr)	6.1	6.4	6.5	7.0
Total Delay (hr)	0.9	0.9	0.8	1.2
Total Stops	60	67	95	77
Fuel Used (gal)	4.9	5.2	5.4	5.4

3: Creech/McNulty & Blythewood Performance by approach

Approach	EB	WB	NB	SB	All
Denied Del/Veh (s)	1.4	0.6	0.0	0.2	1.0
Total Del/Veh (s)	2.7	3.0	32.6	10.9	5.0
Stop/Veh	0.06	0.05	0.74	0.99	0.15

6: Creech & Access Rd Performance by approach

Approach	EB	NB	SB	All
Denied Del/Veh (s)	0.1	0.6	0.0	0.3
Total Del/Veh (s)	2.7	10.3	0.2	4.4
Stop/Veh	1.00	0.18	0.00	0.17

8: Creech & Park Blythewood Acc Performance by approach

Approach	WB	NB	SB	All
Denied Del/Veh (s)	0.1	0.1	0.0	0.0
Total Del/Veh (s)	2.8	0.0	0.3	0.3
Stop/Veh	1.00	0.00	0.00	0.03

Total Network Performance

Denied Del/Veh (s)	1.0
Total Del/Veh (s)	7.3
Stop/Veh	0.17

Intersection: 3: Creech/McNulty & Blythewood

Movement	EB	EB	WB	WB	NB	SB
Directions Served	L	TR	L	TR	LTR	LTR
Maximum Queue (ft)	68	24	57	9	96	107
Average Queue (ft)	30	1	20	0	55	45
95th Queue (ft)	60	8	48	5	97	81
Link Distance (ft)		568		1674	16	263
Upstream Blk Time (%)					41	
Queuing Penalty (veh)					43	
Storage Bay Dist (ft)	150		150			
Storage Blk Time (%)						
Queuing Penalty (veh)						

Intersection: 6: Creech & Access Rd

Movement	EB	NB
Directions Served	R	T
Maximum Queue (ft)	53	104
Average Queue (ft)	19	15
95th Queue (ft)	46	72
Link Distance (ft)	131	293
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 8: Creech & Park Blythewood Acc

Movement	WB	SB
Directions Served	LR	LT
Maximum Queue (ft)	31	3
Average Queue (ft)	4	0
95th Queue (ft)	20	4
Link Distance (ft)	229	293
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Network Summary

Network wide Queuing Penalty: 43

Summary of All Intervals

Run Number	1	10	2	3	4	5	6
Start Time	4:50	4:50	4:50	4:50	4:50	4:50	4:50
End Time	6:00	6:00	6:00	6:00	6:00	6:00	6:00
Total Time (min)	70	70	70	70	70	70	70
Time Recorded (min)	60	60	60	60	60	60	60
# of Intervals	5	5	5	5	5	5	5
# of Recorded Intervals	4	4	4	4	4	4	4
Vehs Entered	1804	1799	1733	1808	1654	1714	1801
Vehs Exited	1813	1797	1729	1804	1653	1735	1788
Starting Vehs	38	21	22	18	19	39	14
Ending Vehs	29	23	26	22	20	18	27
Travel Distance (mi)	656	663	635	661	608	635	658
Travel Time (hr)	26.6	26.8	25.7	26.4	24.3	26.3	26.4
Total Delay (hr)	3.7	3.8	3.7	3.5	3.2	4.3	3.5
Total Stops	340	314	321	306	281	322	336
Fuel Used (gal)	21.4	21.5	20.9	21.4	19.7	20.8	21.3

Summary of All Intervals

Run Number	7	8	9	Avg
Start Time	4:50	4:50	4:50	4:50
End Time	6:00	6:00	6:00	6:00
Total Time (min)	70	70	70	70
Time Recorded (min)	60	60	60	60
# of Intervals	5	5	5	5
# of Recorded Intervals	4	4	4	4
Vehs Entered	1764	1813	1842	1773
Vehs Exited	1754	1818	1834	1773
Starting Vehs	16	29	19	21
Ending Vehs	26	24	27	23
Travel Distance (mi)	641	654	678	649
Travel Time (hr)	25.7	26.7	28.0	26.3
Total Delay (hr)	3.4	4.0	4.4	3.8
Total Stops	312	345	317	320
Fuel Used (gal)	20.9	21.8	22.2	21.2

Interval #0 Information Seeding

Start Time 4:50
 End Time 5:00
 Total Time (min) 10
 Volumes adjusted by Growth Factors.
 No data recorded this interval.

Interval #1 Information Recording

Start Time 5:00
End Time 5:15
Total Time (min) 15
Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	436	444	444	454	405	409	418
Vehs Exited	446	443	440	435	396	416	406
Starting Vehs	38	21	22	18	19	39	14
Ending Vehs	28	22	26	37	28	32	26
Travel Distance (mi)	160	165	163	161	148	156	153
Travel Time (hr)	6.5	6.7	6.6	6.4	5.9	6.3	6.0
Total Delay (hr)	0.9	1.1	0.9	0.8	0.7	0.9	0.7
Total Stops	76	66	85	81	57	74	70
Fuel Used (gal)	5.2	5.3	5.3	5.2	4.7	5.0	4.9

Interval #1 Information Recording

Start Time 5:00
End Time 5:15
Total Time (min) 15
Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	430	443	435	433
Vehs Exited	415	446	434	427
Starting Vehs	16	29	19	21
Ending Vehs	31	26	20	27
Travel Distance (mi)	155	157	158	158
Travel Time (hr)	6.1	6.4	6.9	6.4
Total Delay (hr)	0.7	0.9	1.4	0.9
Total Stops	69	86	78	76
Fuel Used (gal)	5.1	5.3	5.4	5.1

Interval #2 Information

Start Time 5:15
 End Time 5:30
 Total Time (min) 15
 Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	424	426	414	421	368	435	452
Vehs Exited	432	427	415	436	378	440	451
Starting Vehs	28	22	26	37	28	32	26
Ending Vehs	20	21	25	22	18	27	27
Travel Distance (mi)	154	157	150	161	139	155	162
Travel Time (hr)	6.1	6.1	6.0	6.3	5.4	6.5	6.8
Total Delay (hr)	0.8	0.7	0.7	0.7	0.6	1.1	1.1
Total Stops	84	70	73	63	55	99	93
Fuel Used (gal)	5.0	5.0	4.9	5.1	4.4	5.2	5.3

Interval #2 Information

Start Time 5:15
 End Time 5:30
 Total Time (min) 15
 Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	434	466	452	429
Vehs Exited	427	470	447	434
Starting Vehs	31	26	20	27
Ending Vehs	38	22	25	23
Travel Distance (mi)	157	170	165	157
Travel Time (hr)	6.4	6.8	6.8	6.3
Total Delay (hr)	0.9	1.0	1.0	0.9
Total Stops	74	94	83	79
Fuel Used (gal)	5.0	5.6	5.4	5.1

Interval #3 Information

Start Time 5:30
End Time 5:45
Total Time (min) 15
Volumes adjusted by PHF, Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	497	496	432	490	449	483	471
Vehs Exited	499	489	431	480	448	473	474
Starting Vehs	20	21	25	22	18	27	27
Ending Vehs	18	28	26	32	19	37	24
Travel Distance (mi)	180	181	160	176	165	177	171
Travel Time (hr)	7.5	7.6	6.4	7.2	6.8	7.3	6.9
Total Delay (hr)	1.2	1.4	0.8	1.1	1.1	1.2	0.9
Total Stops	87	101	79	91	82	94	101
Fuel Used (gal)	5.9	6.0	5.3	5.7	5.5	5.8	5.6

Interval #3 Information

Start Time 5:30
End Time 5:45
Total Time (min) 15
Volumes adjusted by PHF, Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	469	479	500	476
Vehs Exited	488	488	496	475
Starting Vehs	38	22	25	23
Ending Vehs	19	13	29	25
Travel Distance (mi)	173	175	188	175
Travel Time (hr)	7.1	7.3	7.7	7.2
Total Delay (hr)	1.1	1.3	1.2	1.1
Total Stops	99	89	64	87
Fuel Used (gal)	5.8	6.0	6.0	5.7

Interval #4 Information

Start Time 5:45
End Time 6:00
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	1	10	2	3	4	5	6
Vehs Entered	447	433	443	443	432	387	460
Vehs Exited	436	438	443	453	431	406	457
Starting Vehs	18	28	26	32	19	37	24
Ending Vehs	29	23	26	22	20	18	27
Travel Distance (mi)	163	159	161	163	156	148	172
Travel Time (hr)	6.5	6.3	6.8	6.5	6.2	6.1	6.8
Total Delay (hr)	0.8	0.7	1.2	0.9	0.8	1.0	0.8
Total Stops	93	77	84	71	87	55	72
Fuel Used (gal)	5.3	5.2	5.4	5.3	5.0	4.8	5.5

Interval #4 Information

Start Time 5:45
End Time 6:00
Total Time (min) 15

Volumes adjusted by Growth Factors.

Run Number	7	8	9	Avg
Vehs Entered	431	425	455	435
Vehs Exited	424	414	457	437
Starting Vehs	19	13	29	25
Ending Vehs	26	24	27	23
Travel Distance (mi)	157	152	167	160
Travel Time (hr)	6.1	6.2	6.7	6.4
Total Delay (hr)	0.7	0.9	0.9	0.9
Total Stops	70	76	92	76
Fuel Used (gal)	5.1	5.0	5.5	5.2

3: Creech/McNulty & Blythewood Performance by approach

Approach	EB	WB	NB	SB	All
Denied Del/Veh (s)	1.4	0.6	0.0	0.2	1.0
Total Del/Veh (s)	2.6	2.8	27.1	11.8	4.8
Stop/Veh	0.06	0.05	0.84	0.99	0.16

6: Creech & Access Rd Performance by approach

Approach	EB	NB	SB	All
Denied Del/Veh (s)	0.1	0.1	0.0	0.1
Total Del/Veh (s)	2.7	2.7	0.2	1.4
Stop/Veh	1.00	0.06	0.00	0.12

8: Creech & Park Blythewood Acc Performance by approach

Approach	WB	NB	SB	All
Denied Del/Veh (s)	0.1	0.1	0.0	0.0
Total Del/Veh (s)	2.7	0.0	0.4	0.5
Stop/Veh	1.00	0.00	0.00	0.09

10: Creech Performance by approach

Approach	NB	SB	All
Denied Del/Veh (s)	0.0	0.0	0.0
Total Del/Veh (s)	0.1	0.2	0.2
Stop/Veh	0.00	0.00	0.00

Total Network Performance

Denied Del/Veh (s)	1.0
Total Del/Veh (s)	6.6
Stop/Veh	0.18

Intersection: 3: Creech/McNulty & Blythewood

Movement	EB	EB	WB	WB	NB	NB	SB
Directions Served	L	TR	L	TR	LT	R	LTR
Maximum Queue (ft)	72	26	55	3	91	66	105
Average Queue (ft)	30	2	17	0	44	26	45
95th Queue (ft)	61	11	42	3	87	55	83
Link Distance (ft)		568		1662	16	16	263
Upstream Blk Time (%)					40	8	
Queuing Penalty (veh)					21	4	
Storage Bay Dist (ft)	150		150				
Storage Blk Time (%)							
Queuing Penalty (veh)							

Intersection: 6: Creech & Access Rd

Movement	EB	NB	NB
Directions Served	R	T	T
Maximum Queue (ft)	47	59	3
Average Queue (ft)	19	5	0
95th Queue (ft)	45	32	3
Link Distance (ft)	132	134	134
Upstream Blk Time (%)			
Queuing Penalty (veh)			
Storage Bay Dist (ft)			
Storage Blk Time (%)			
Queuing Penalty (veh)			

Intersection: 8: Creech & Park Blythewood Acc

Movement	WB	SB
Directions Served	LR	LT
Maximum Queue (ft)	30	3
Average Queue (ft)	4	0
95th Queue (ft)	20	3
Link Distance (ft)	229	115
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

Intersection: 10: Creech

Movement

Directions Served
Maximum Queue (ft)
Average Queue (ft)
95th Queue (ft)
Link Distance (ft)
Upstream Blk Time (%)
Queuing Penalty (veh)
Storage Bay Dist (ft)
Storage Blk Time (%)
Queuing Penalty (veh)

Network Summary

Network wide Queuing Penalty: 25

END

FORM FP

Application for Review of Final Plan or Plat

TOWN OF BLYTHEWOOD

Date Filed: _____ Fee Paid: _____ (\$500.00) Application No. _____

(see Article 16, Land Development Regulations)

Instructions

This form must be completed for a review of any proposed development or subdivision of property under the Ordinance and Land Development Regulations. Please print or type. An application for a Zoning Permit (Form ZP) must also be completed and will accompany this document through the review process. If the applicant is not the property owner, the property owner must sign the Designation of Agent on Form ZP. If the application is on behalf of property owner(s), all owners must sign.

- An accurate, legible plat showing current property dimensions and locations of all existing structures, easements, and improvements on the property must be attached to an application for Final Plan or Plat Review and is hereby received as _____ Exhibit A.

THE APPLICANT HEREBY REQUESTS

- Final Review of Subdivision,
Final Review of Site Plan for a Group Development,
Final Review of Site Plan for a Planned Development,

Pursuant to Section 26-103 of the Zoning Ordinance, to develop the property described below in the following manner:

New construction of 48 senior housing units

REF: Form ZP Application #: _____ Filed by: _____
Form SKR Sketch Plan Review Completed? _____
Preliminary Plan Review Completed? _____
Distribution of Final Plan or Plat 30 days

- Planning Office (file copy)
Planning Commission
SC-DHEC
Town Engineering Firm
Winnsboro Water
City of Columbia Water and Sewer
Palmetto Sewer
Central Midlands COG
E-911 Coordinator

Final Plan or Plat approval shall constitute approval to obtain building permits and to record deeds for lots

Final Plats shall meet the minimum of standards of design set forth in these regulations and shall include the following information:

- Prepared by South Carolina Registered Land Surveyor at a convenient scale of not less than one inch equals 100 feet; adjustable depending upon lot sizes and total acreage but in no case shall be less than one inch equals 250 feet.

- Name.

FORM FP

Revised 6/19/03

- Name of subdivision, property, development or property owner.
- Proposed name.
- Name of locality and county in which subdivision is located.
- Ownership.
 - Name and address, including telephone number, of legal owner or agent of property.
 - Name and address, including telephone number, of professional person(s) responsible for the design of the subdivision, development, buildings, improvements, and surveys.
- Location.
 - Vicinity map at a scale of not less than 1" equals 1,000 ft. showing relationship of the proposed development to surrounding development(s). Vicinity map shall include a north arrow and scale.
- Features.
 - Total acreage in the tract.
 - Graphic scale
 - North Arrow identified as magnetic, grid, or true north
 - Date

Boundaries of the tract to be developed or subdivided with all bearings and distances indicated. Boundary survey shall be to such a degree of accuracy that the error of closure is no greater than 1:2,500.

- The following existing conditions shall be stated on the plat:
 - Topography by contours of not less than two (2) feet and extending to at least one hundred (100) feet outside the subdivision.
 - Deed record names of all adjoining property owners.
 - Names of any adjoining developments or subdivisions.
 - Property lines within and adjoining the development.
 - Location and ROWs of all existing or platted streets and other public ways, railroads, easements, water courses and buildings either on or adjacent to the property to be developed. Specify whether utility lines are in easements or ROWs, and show locations of poles, towers, and substations.
 - Location of all political boundaries.
 - Location of streams, lakes, swamps, and lands subject to flood, based on a one hundred-year frequency flood. Lots so affected shall be identified and noted on the plat.
 - Location of existing adjoining property lines.
 - In the case of re-subdivisions, a copy of existing plat with proposed re-subdivisions superimposed thereon.
 - Size and location of existing sewers, water mains, drains, culverts, or other underground facilities within the street or within the right-of-way of streets or roads adjoining the tract. Grades and invert elevations of sewers shall be shown.
 - The acreage of each drainage area affecting the proposed development.
 - All elevations shall refer to MSL datum (if available) where public water and/or public sewers are to be installed. Topography in two (2) foot contour intervals.
 - Grading plan showing proposed finished contours.
 - Title, Name and Address, telephone, and signature of the SC Registered Engineer and Surveyor responsible for the plans, date, and all revision dates.
 - Bearing and distances for all boundaries. Sufficient data to determine readily and reproduce accurately on the ground the location, bearing and length of every street centerline, lot line, easement, and boundary line, whether curved or straight, including flood elevations. This shall include the radius, point of tangent, and other data for curved property lines and curved streets, to an appropriate accuracy and in conformance with good surveying practice.
 - Streets, alleys, rights-of-way, percent of grades, and all street names as approved by Planning Commission and the Richland County E-911 Coordinator. Streets that are as yet unapproved shall be so stated on the plat.
 - All dimensions to the nearest one-tenth (.1) of a foot and angles to the nearest minute or as required by "Minimum Standards for the Practice of Land Surveying in South Carolina" whichever is greater.

FORM FP

Revised 6/19/03

- Accurate description of the location of all monuments and markers.
- Proposed use of all parcels. If the proposed use is the same, this can be placed in a note on the final plat.
- Type, number, and location of all streetlights.
- Certifications.
 - Certification of Accuracy.
 - Certification of Ownership and Dedication for all streets, alleys, walks, parks, and other sites to public or private use as designated.

I (We) certify that the information contained on required documents submitted with this request is correct.

Date: 1/29/2020

(sign)


Applicant

(sign)

Applicant

**Application for Review of Final Plan or Plat
TOWN OF BLYTHEWOOD**