

CHARLESTON COUNTY PLANNING COMMISSION WORKSHOP
Council Chambers, 2nd Floor, Lonnie Hamilton, III Public Services Building
4045 Bridge View Drive, North Charleston, SC

AGENDA
OCTOBER 12, 2020
2:00 P.M.

- I. **CALL TO ORDER**
- II. **CORRESPONDENCE**
- III. **PRESENTATION AND DISCUSSION: PROPOSED AMENDMENTS TO THE CHARLESTON COUNTY COMPREHENSIVE PLAN AND ZONING AND LAND DEVELOPMENT REGULATIONS ORDINANCE (ZLDR) TO INCORPORATE THE JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR OVERLAY ZONING DISTRICT (JO-MHC-O).**
- IV. **PRESENTATION: CONCEPTUAL PLANNED DEVELOPMENT (PD) PLAN, OAK PLANTATION TRACT [TMS 286-00-00-004, -005] (West Ashley/ Johns Island area).**
“The proposed development will be a thoughtfully planned and integrated mix of commercial and residential uses. The proposed concept plan shows general orientation of proposed uses, including (but not limited to): Highway Commercial, Grocery, Retail Village, Hotel, Multifamily and Townhomes planned around the existing natural features of the site. The applicant seeks to create a unique and experiential development on the western boundary of town, which will serve as a “community hub” to serve the existing surrounding residential communities and those farther south and west the currently travel farther into town, creating additional traffic burdens on current infrastructure.”
- V. **ADJOURNMENT**

****The regularly scheduled Planning Commission meeting will begin immediately following this workshop. Please see the regular meeting agenda on the back of this page.****

CHARLESTON COUNTY PLANNING COMMISSION MEETING
Council Chambers, 2nd Floor, Lonnie Hamilton, III Public Services Building
4045 Bridge View Drive, North Charleston, SC

AGENDA
OCTOBER 12, 2020
Immediately following the Planning Commission Workshop

- I. CALL TO ORDER & INTRODUCTIONS**
- II. COMPLIANCE WITH THE FREEDOM OF INFORMATION ACT**
- III. APPROVAL OF THE SEPTEMBER 14, 2020 WORKSHOP AND MEETING MINUTES**
- IV. CORRESPONDENCE**
- V. COMPREHENSIVE PLAN AND ZLDR TEXT AMENDMENT:**
 - a. Proposed amendments to the Charleston County Comprehensive Plan and Zoning and Land Development Regulations Ordinance (ZLDR) to adopt the Johns Island Maybank Highway Corridor Overlay Zoning District (JO-MHC-O).**
- VI. ZLDR TEXT AMENDMENT:**
 - a. ZLDR-08-20-00124: Request to amend the Zoning and Land Development Regulations Ordinance (ZLDR), Table 5.13-1, DuPont-Wappoo Area Overlay Zoning District Use Table, to conditionally allow “Beverage or Related Products Manufacturing, including Alcoholic Beverages and Excluding Microbreweries and Brewpubs, and Food or Related Products Manufacturing” in the Community Commercial District (Du-Wap-O only), subject to conditions, Sec. 6.4.57, Manufacturing and Production.**
- VII. DIRECTOR’S REMARKS**
 - a. Resilience Committee**
 - b. Director’s Report**
- VIII. CHAIR’S REMARKS**
- IX. NEXT MEETING DATE: NOVEMBER 9, 2020**
- X. ADJOURNMENT**

**CHARLESTON COUNTY
PLANNING COMMISSION
WORKSHOP MINUTES
SEPTEMBER 14, 2020**

ATTENDEES

Planning Commission: Kip Bowman, Sussan Chavis, Vice Chair Cindy Floyd, Warwick Jones, David Kent, Adam MacConnell, Chair Eric Meyer, Bruce Miller, and Pete Paulatos.

County staff: Joel Evans, Director of Planning/Zoning; Andrea Melocik, Deputy Director of Planning/Zoning; Austin Bruner, County Attorney; Niki Grimball, Planner II; and Anna Kimelblatt, Administrative Support Coordinator I.

Members of the public: Chris Carrara, Jonathan Yates, Aden Ferillo, Brian Hellman, Kelsey Gregory, Richard Bailey, Barry Whalen, James Tucker, Ronald and Janelle Davis, Yvonne Wigfall, Jake Gordon, John Zlogar, Susan Heyward, Christine Lyles, Samy Guzman, Ed and Nancy Harold, C.D. Liollo, Joseph Fleming, Pam Skinner, Martha Brody, Viola Heyward, Stacy Heyward, Johnny Heyward, Elizabeth Coaxum, Gracie White, Gwendolyn Smith, Elaine Towns, Ashley Towns, Todd Gerhardt, Michelle Strobel, Bill Britton, Jimmie Gianokos, Rich Thomas, Joshua Caleb Costa, Craig Weaver, Taylor Charpon, Ben Stein, and Louise Bennett, Pam Skinner.

CALL TO ORDER

Chair Meyer called to order the workshop of the Charleston County Planning Commission in Council Chambers at the Lonnie Hamilton, III, Public Services Building, 4045 Bridge View Drive at 2:04 pm.

COMPLIANCE WITH THE FREEDOM OF INFORMATION ACT & INTRODUCTIONS

Chair Meyer announced that the meeting was noticed in compliance with the South Carolina Freedom of Information Act. He stated the purpose of the Planning Commission and introduced its members. Mr. Evans introduced County staff.

PRESENTATION AND DISCUSSION: PROPOSED AMENDMENTS TO THE CHARLESTON COUNTY COMPREHENSIVE PLAN AND ZONING AND LAND DEVELOPMENT REGULATIONS ORDINANCE (ZLDR) TO INCORPORATE THE MAIN ROAD CORRIDOR OVERLAY ZONING DISTRICT (MRC-O).

Andrea Melocik presented the changes made to the proposed overlay zoning district from the version the Commission heard at their August 10, 2020 meeting to address the concerns of the owners of properties located in the Kitford Community Industrial District, the Johns Island Task Force, and the SC Coastal Conservation League, pursuant to the direction of the Commission. She stated the changes include:

- Expanding the uses allowed in the Kitford Community Industrial District to include the Rural Commercial (RC) District uses plus limited manufacturing and production uses;
- Prohibiting Short-term Rental, Commercial Guest House uses and allowing Limited Home Rentals and Extended Home Rentals instead;
- Prohibiting Heavy Duty Truck Dealers; Manufactured Home Dealers;
- Prohibiting Impound Yards and Towing Facilities;
- Requiring all canopy trees to be Live Oaks; and
- Increasing the planted right-of-way buffer to 50 feet in depth per the S4 buffer requirements (6 canopy trees, 9 understory trees, 50 shrubs, and 2 street trees per 100 linear feet).

Chair Meyer asked if there were any questions or comments from the Planning Commission. Hearing no questions from the Commissioners, Chair Meyer asked for public comment.

John Zlogar, representing the Johns Island Task Force, spoke in favor of the proposed overlay zoning district. He stated that the Task Force had been working with staff for over two years on the amendments, and that the Task Force fully endorsed the new amendments.

Barry Whalen also spoke regarding the overlay zoning district. He stated his interest in two of the properties, which were located at 636 and 607 Main Road. He stated that he felt the more intense uses permitted in the Belvedere area would be appropriate on these tracts and that he wanted to go on record and state that the owners of these properties may come to the Commission with a Planned Development request in the future to permit some of these more intensive uses.

Ashley Towns also spoke in regards to 636 Main Road. She stated her family has owned this parcel for over 60 years and that the lack of flexibility in terms of permitted uses in the overlay zoning district would impact the marketability of the property. She requested that the uses allowed in the Belvedere-Main Commercial District be allowed on her family's property.

PRESENTATION AND DISCUSSION: CONCEPTUAL PLANNED DEVELOPMENT (PD) PLAN, SEA ISLAND GOLF COURSE PD [TMS 204-00-00-025, 2.68 ACRES].

Jonathan Yates provided a summary of the project on behalf of the applicant, Sea Island Golf LLC. He stated the proposal includes a 36-hole miniature golf course, two bocce ball courts, and the restoration of the Walnut Hill School House, circa 1868, a concessions facility, and 75 parking spaces.

The engineer for the project, Richard Bailey of Hussey, Gay & Bell, provided an explanation of the site plan and reviewed the plans for parking, buffers, setbacks, grand tree preservation, and landscaping. He also addressed concerns from the community regarding light pollution, stating they would abide by the "dark sky initiative." Additionally, he stated that the golf course would not be visible from the road, and that buffers would be used to obstruct the view of the parking area from the road to the extent possible.

Mr. Yates reviewed the history of the Walnut Hill School, which is located on the property. Todd Gerhardt, the applicant, stated that it is the intention of Sea Island Golf LLC to restore the schoolhouse to its original state to be used for educational purposes. Mr. Gerhardt also stated that 50% of the gross revenue from weekends during the months between November and February would be donated to one of 12 different charities.

Mr. Yates clarified that this was intended to be a high quality miniature golf course that would be in context with the surrounding sea islands. He stated that there would not be any "gaudy" features similar to other miniature golf courses.

Chair Meyer asked for comments and questions from the Commissioners. Commissioner Floyd asked for clarification regarding the proceeds that would be given to charity. Mr. Yates clarified that each charity would be given a designated weekend from which they would collect a portion of the gross revenue.

Commissioner Jones stated that while such generosity was noble, it was not appropriate for the Commission to consider such a gesture when assessing the appropriateness of the rezoning request. He emphasized that this was not relevant to their decision-making, and that their decision should be based on environmental, traffic, and land use impacts. He also

expressed concerns with regards to the applicant's provided traffic study, stating that he was not convinced that this would have a minimal traffic impact or be less impactful than the uses allowed in the current Single Family Residential 4 Zoning District. He stated that he did not agree with the turnover rate as provided by the applicant. Mr. Yates stated that a miniature golf course used to be located on Ben Sawyer Boulevard in Mt. Pleasant and that the traffic impacts from that site had been minimal.

Commissioner Chavis asked Mr. Yates if the applicant had met with the surrounding community regarding this project. Mr. Yates stated that they hosted an on-site community meeting on August 25, 2020 and there were approximately 26 attendees, three of which expressed negative opinions regarding the project. He stated that he felt the response at the community meeting was mostly positive.

Commissioner Chavis asked if this amenity would be affordable to the greater Johns Island community. Mr. Yates responded that the intent is for it to be affordable to everyone in the tri-county area.

Commissioner Chavis asked the applicant to provide clarity regarding the criteria that were used to select the twelve charities proposed to receive a portion of the profits. Mr. Gerhardt stated that most were charities he had worked with in the past. Commissioner Chavis asked if other local community groups or churches would be permitted to apply for such fundraising opportunities, or if these opportunities would be limited to the 12 charities identified by Mr. Gerhardt. Mr. Gerhardt replied that other groups were welcome to request fundraising opportunities and that he had already been approached by groups outside the 12 he originally identified. Commissioner Chavis stated her agreement with Commissioner Jones that the charitable contributions should not be considered in the Commission's decision-making process, but that she was in support of the restoration of the Walnut Hill Schoolhouse.

Commissioner Jones asked staff if commitments to charities could or should be written into Planned Developments. Commission Attorney Austin Bruner stated that this was not recommended and the applicant concurred.

Commissioner MacConnell clarified the current and future zoning of the parcel, being Residential (R-4) and Urban/Suburban Mixed Use, respectively. He asked staff for guidance on what the recommendation would be if this was presented as a straight rezoning rather than a Planned Development (PD). Mr. Yates stated that outdoor recreation was an allowed use in the Urban/Suburban Mixed Use Zoning District.

Mr. Evans clarified the reasoning behind the PD. The applicant originally presented the project as a downzoning to the Agricultural Preservation (AG-8) Zoning District, which allows miniature golf. He stated there was concern from the community that this downzoning would not allow for control or restriction on uses in the future, and therefore, staff recommended a PD to allow for more specific use conditions and limitations.

Chair Meyer clarified for attendees that no vote would be taken on this request today. He then asked for public comment, beginning with those in support of the PD.

Michelle Strobel spoke in favor, representing the local chapter of the Boy Scouts of America. She stated that she had worked with Mr. Gerhardt for a number of years, and that she was in support of this as it was a family- friendly amenity. Bill Britton, the co-founder of the Water Wellness Mission, a charity to be included in the reception of profits, also spoke in support of the PD.

Chair Meyer asked for those who wished to speak in opposition to the proposal. Nancy Harold, 4454 Betsy Kerrison Parkway, stated she lives directly across from the parcel in question and cited concerns about traffic and safety. Additionally, she stated that she felt the community meeting held by the applicant was more oppositional than the applicant stated.

Gwendolyn Smith, 3215 Resurrection Road, also spoke in opposition, expressing concerns that the applicant did not adequately notify the surrounding community for their public meeting. She also stated that while charitable contributions are nice, the charities chosen by the applicant do not benefit the people who live nearby that will be the most impacted by this development. Elizabeth Coaxum, 3788 Betsy Kerrison Parkway, echoed the concerns regarding lack of communication from the applicant with the surrounding community, especially those who reside on Resurrection Road. She also emphasized that traffic would be a major concern.

Rich Thomas, 4360 Betsy Kerrison Parkway, stated his concerns regarding preservation of the character of the Sea Islands and the greater Johns Island community. He also stated that he didn't feel these charities were relevant to the surrounding community.

Pam Skinner, a Wadmalaw Island resident, stated that she did not feel this was an acceptable use of a PD, and expressed concern that the applicant could amend the PD in the future to allow for additional uses.

ADJOURNMENT

Chair Meyer adjourned the workshop at 3:02 p.m.

Anna Kimelblatt
Recording for the Planning Department

Ratified by the Charleston County Planning Commission this
12th day of October, 2020.

Eric Meyer
Chair

Attest:

Joel Evans, PLA, AICP, Director
Zoning & Planning Department

PLANNING COMMISSION DISCLAIMER:

UNRATIFIED WORKSHOP MINUTES

This document is a draft of the minutes of the most recent workshop of the Charleston County Planning Commission.

The members of the Planning Commission have neither reviewed nor approved this document; therefore, these minutes shall only become official meeting minutes after adoption by the Planning Commission at a public meeting of the Commission.

**CHARLESTON COUNTY
PLANNING COMMISSION
MEETING MINUTES
SEPTEMBER 14, 2020**

ATTENDEES

Planning Commission: Kip Bowman, Sussan Chavis, Vice Chair Cindy Floyd, Warwick Jones, David Kent, Adam MacConnell, Chair Eric Meyer, Bruce Miller, and Pete Paulatos.

County staff: Joel Evans, Director of Planning/Zoning; Andrea Melocik, Deputy Director of Planning/Zoning; Austin Bruner, County Attorney; Niki Grimball, Planner II; and Anna Kimelblatt, Administrative Support Coordinator I.

Members of the public: Chris Carrara, Jonathan Yates, Aden Ferillo, Brian Hellman, Kelsey Gregory, Richard Bailey, Barry Whalen, James Tucker, Ronald and Janelle Davis, Yvonne Wigfall, Jake Gordon, John Zlogar, Susan Heyward, Christine Lyles, Samy Guzman, Ed and Nancy Harold, C.D. Liollo, Joseph Fleming, Pam Skinner, Martha Brody, Viola Heyward, Stacy Heyward, Johnny Heyward, Elizabeth Coaxum, Gracie White, Gwendolyn Smith, Elaine Towns, Ashley Towns, Todd Gerhardt, Michelle Strobel, Bill Britton, Jimmie Gianokos, Rich Thomas, Joshua Caleb Costa, Craig Weaver, Taylor Charpon, Ben Stein, Louise Bennett, and Pam Skinner.

CALL TO ORDER

Chair Meyer called to order the meeting of the Charleston County Planning Commission in Council Chambers at the Lonnie Hamilton, III, Public Services Building, 4045 Bridge View Drive at 3:03 pm.

COMPLIANCE WITH THE FREEDOM OF INFORMATION ACT & INTRODUCTIONS

Chair Meyer announced that the meeting was noticed in compliance with the South Carolina Freedom of Information Act. He stated the purpose of the Planning Commission.

APPROVAL OF MINUTES

On the motion of Vice Chair Floyd, seconded by Commissioner Chavis, the Planning Commission voted unanimously (9-0) to approve the August 10, 2020 workshop and meeting minutes as amended. Commissioner Miller had been mistakenly identified as a dissenting party for an agenda item on which he voted for approval.

COMPREHENSIVE PLAN AND ZONING AND LAND DEVELOPMENT REGULATIONS (ZLDR) TEXT AMENDMENTS REQUEST: MAIN ROAD CORRIDOR OVERLAY ZONING DISTRICT (MRC-O).

Chair Meyer introduced the first item on the agenda, proposed amendments to the Comprehensive Plan and ZLDR to incorporate the Main Road Corridor Overlay Zoning District (MRC-O). Having seen staff's presentation and discussed the proposed amendments at the workshop immediately preceding the meeting, Chair Meyer asked the Commission if they had any additional questions or comments for staff.

Commissioner Chavis asked for clarification regarding stormwater requirements as they relate to the Dutch Dialogues. Andrea Melocik stated that the Public Works Department reviews all stormwater plans prior to implementation, but that maintenance and evaluation was a shared responsibility among other entities such as SCDOT, municipalities, and the County. She added that a larger collaborative effort among the Public Works, Building Inspection Services, and Zoning and Planning Departments regarding implementation of the Dutch Dialogues was forthcoming.

Commissioner MacConnell noted that there had been oppositional public input regarding the largest parcel of land on Main Road (636 Main Road) during the workshop preceding the meeting and asked for staff's recommendation. Ms. Melocik stated that staff had previously spoken with the representative for the property regarding the requested rezoning and would not recommend editing the overlay to accommodate the requested change at this time. Mr. Evans clarified that additional increases to the zoning would not be appropriate for the area or consistent with the intent of the overlay zoning district.

Chair Meyer noted the extensive planning and community outreach staff has completed to develop the overlay zoning district as presented. He asked for any additional public comment regarding the proposed amendments.

Barry Whalen, speaking in reference to 636 and 607 Main Road, clarified that he was not requesting a rezoning at the present moment. Rather, he stated he wished to have his comments on the public record so that if the Commission is ever presented with a proposed Planned Development on the parcels in question, they may consider additional or more intense uses for these properties.

Ashley Towns, 636 Main Road, also spoke in regards to the overlay zoning district, echoing the sentiments of Mr. Whalen. Additionally, she provided a letter for the Commissioners identifying other uses she wished the Commission to consider permitting on this parcel.

On a motion from Commissioner Jones, seconded by Commissioner Chavis, the Commission voted unanimously (9-0) to approve the amendments as recommended by staff.

REZONING REQUEST: ZREZ-07-20-00117

Chair Meyer introduced the next item on the agenda, request to rezone TMS 424-10-00-014 from the Single Family Residential 4 (R-4) Zoning District to the Mixed-Style Residential 12 (M-12) Zoning District at 515 Joseph Street in the James Island area. Niki Grimball delivered the staff presentation, stating the staff recommendation was approval of the request.

Chair Meyer asked for any input or questions from the Commissioners. Seeing none, he asked for any public comment. The applicant, Caleb Costa, spoke in favor of the application. No one spoke in opposition of the request.

On a motion from Commissioner Kent, seconded by Commissioner Bowman, the Commission voted unanimously (9-0) to approve the request.

DIRECTOR'S REPORT

Mr. Evans delivered the Director's Report for the month of August, stating that there had been no impact to the activities of the Department due to the COVID-19 pandemic. At the request of Chair Meyer, Mr. Evans acknowledged the departure of Dan Frazier, Planner II, for a career opportunity with the City of Beaufort.

CHAIR'S REMARKS

Chair Meyer stated that the next meeting of the Planning Commission would be October 12, 2020 at 2:00 p.m. He stated the regular meeting would begin immediately following the close of the Planning Commission Workshop, and stated that this would likely be the format for the Planning Commission meetings for the rest of the year.

Commissioner Chavis then expressed her concerns to Mr. Evans that many of the individuals on Resurrection Road did not receive notice of the proposed Sea Island Golf PD discussed at

the Planning Commission Workshop preceding the meeting, nor were they notified by the applicant regarding the community outreach meeting. She asked for clarification regarding the Department's procedures in requiring that notifications be sent by the applicant.

Mr. Evans assured Commissioner Chavis that staff had notified the public of the Planning Commission Workshop as required by state and county ordinance. He clarified that staff only notices County sponsored meetings and that notifications for community outreach meetings for Planned Development applications is the responsibility of the applicant.

Commissioner Paulatos echoed the concerns of Commissioner Chavis. He proposed that the Commission consider holding applicants to the same standard of public notice as the County so that opportunity for attendance at community meetings regarding Planned Developments is more equitable. He asked that this requirement be incorporated into the ZLDR for Planned Development applications.

Mr. Evans agreed that this was a possibility and stated that staff would follow up on implementing this requirement in the Ordinance.

ADJOURNMENT

Chair Meyer adjourned the meeting at 3:25 p.m.

Anna Kimelblatt
Recording for the Planning Department

Ratified by the Charleston County Planning Commission this
12th day of October, 2020.

Eric Meyer
Chair

Attest:

Joel Evans, PLA, AICP, Director
Zoning & Planning Department

PLANNING COMMISSION DISCLAIMER:

UNRATIFIED MEETING MINUTES

This document is a draft of the minutes of the most recent meeting of the Charleston County Planning Commission.

The members of the Planning Commission have neither reviewed nor approved this document; therefore, these minutes shall only become official meeting minutes after adoption by the Planning Commission at a public meeting of the Commission.

Proposed Amendments to the Charleston County Comprehensive Plan and ZLDR to incorporate the Johns Island Maybank Hwy Corridor Overlay Zoning District (JO-MHC-O)

- Case History**
- Presentation**
- Proposed Amendments**
- Public Input**

**JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR
OVERLAY ZONING DISTRICT
CASE HISTORY**

**Planning Commission Workshop: January 13, 2020
Planning Commission Workshop and Meeting: October 12, 2020**

Background

The MHC-O, Maybank Highway Corridor Overlay Zoning District, was first established as part of the Charleston County Zoning and Land Development Regulations Ordinance (ZLDR) when it was adopted on November 20, 2001. The MHC-O included only unincorporated properties along Maybank Highway located on Johns Island. In 2006, the MHC-O district was amended to implement the recommendations of the 2003 *Charleston County Comprehensive Plan Five-Year Review*. In 2018, the *10-Year Comprehensive Plan Update* was adopted, and includes the priority recommendation to coordinate with the City of Charleston to review the MHC-O District and extend the overlay boundary onto James Island. The JO-MHC-O, Johns Island Maybank Highway Corridor Overlay Zoning District, implements that recommendation.

The JO-MHC-O project began in July 2018 with a focus on community input. Public outreach included a community survey resulting in over 1,700 responses, four focused stakeholder meetings, and four rounds of on-location public workshops attended by hundreds of Johns Island residents. The JO-MHC-O divides the corridor into two districts: (1) the Mixed Use District, intended for higher intensity commercial development at the three major nodes along the corridor; and (2) the Limited Commercial District, intended for service and neighborhood commercial uses. It also addresses vehicle access issues, enhances right-of-way buffer and signage requirements, and creates more consistent land use and density/intensity and dimensional standards.

The JO-MHC-O is the culmination of a two-year City of Charleston and Charleston County collaborative planning process including input from hundreds of residents and property owners, business owners, and stakeholders. Below is a summary of the project timeline to date:

- January 6, 2015: County Council adopted the Comprehensive Plan Five-Year Review, including the priority recommendation to review the MHC-O District and extend the overlay boundary onto James Island.
- July 31, 2018: 219 community members attended the first Kick-Off Community Workshop at the Berkeley Electric Cooperative Conference Room
- August 23, 2018: 141 community members attended the second Kick-Off Community Workshop at St. Johns High School.
- August-November 2018: Received 1,725 responses to the JO-MHC-O Community Survey.
- November-December 2018: Held three individual stakeholder meetings:
 - November 28, 2018 Transportation and Drainage Stakeholder Meeting: Included representatives from SCDOT District 6, Town of Kiawah Island, Town of Seabrook Island, and the respective City of Charleston and Charleston County Zoning and Planning, Public Works, and Transportation Departments.
 - November 29, 2018 Community Stakeholder Meeting: Included representatives from various community organizations, neighborhood associations, James Island Public Service District, EMS, CCSD, and city and county zoning and planning staff.
 - December 4, 2018 Business Owner Stakeholder Meeting: Included representatives from the business community along the corridors of Maybank Highway on James Island, and Maybank Highway and Main Road on Johns Island.

- February 29, 2019: 103 community members attended the third workshop.
- June 17, 2019: 72 community members attended the fourth workshop.
- October 24, 2019: 70 community members attended the fifth workshop.
- January 13, 2020: The first Planning Commission workshop was held.
- March 2020: City and County Staff facilitated Planning Commission member site visits to view and discuss the Johns Island Maybank Highway corridor and elements of the draft JO-MHC-O.
- March 2020 – Present: City and County staff held multiple meetings to finalize the JO-MHC-O.
- To date, City and County staff have met 19 times to discuss, review, and revise the JO-MHC-O.

The proposed amendments to the Comprehensive Plan and the ZLDR to incorporate the JO-MHC-O are attached.

COMPREHENSIVE PLAN AMENDMENTS

In summary, the proposed Comprehensive Plan amendments:

- Amend Chapter 3.1 as follows:
 - Update Map 3.1.4: Future Land Use to amend the JO-MHC-O;
 - Update Map 3.1.5: Urban/Suburban Future Land Use Detail to amend the JO-MHC-O;
 - Incorporate Map 3.1.18: Johns Island Maybank Highway Corridor Overlay Zoning District.
- Amend Chapter 3.9 as follows:
 - In Table 3.9.1, update the Project Status column to show “Completion in 2020”;
 - Remove the Maybank Highway Corridor Overlay Zoning District project from Table 3.9.2.

APPROVAL CRITERIA

Pursuant to Article 3.2.6 of the Charleston County Zoning and Land Development Regulations Ordinance, *Comprehensive Plan* Amendments may be approved by the County Council only if they determine that the proposed amendment is consistent with the overall purpose and intent of the Comprehensive Plan and that any one of the following criteria has been met:

- A. There was a significant error in the original Comprehensive Plan adoption;
Response: N/A
- B. In adopting the Comprehensive Plan, the County Council failed to take into account facts, projections or trends that were reasonably foreseeable to exist in the future;
Response: N/A
- C. Events, trends, or facts after adoption of the Comprehensive Plan have changed the County Council's original findings made upon plan adoption;
Response: N/A
- D. Events, trends, or facts after adoption of the Comprehensive Plan have changed the character or condition of an area, making the proposed amendment necessary;
Response: The Maybank Highway Corridor Overlay Zoning District was developed in coordination with the residents of Johns Island and the City of Charleston in the late 1980s/early 1990s. Since that time, many changes affecting this corridor have taken place, including:
 - *The City of Charleston revised their Maybank Highway Corridor Overlay Zoning District, adopting land use, density, and site design standards that differ from those adopted by Charleston County; and*

- *Charleston County Council approved the construction of significant transportation improvements in the area, including widening Maybank Highway and incorporating bicycle and pedestrian ways, creating a pitchfork road design network north and south of Maybank Highway on the eastern side of River Road to manage traffic coming onto the island from James Island, and the final leg of Interstate 526 from West Ashley, across Johns Island, and onto James Island.*

These amendments implement the Comprehensive Plan strategy to review this Overlay Zoning District in light of the changes described above and work with the public and the City of Charleston to make revisions as appropriate.

- E. The proposed Comprehensive Plan Amendment is requested pursuant to and complies with Art.3.17, Developments of County Significance; or

Response: N/A

- F. The proposed Comprehensive Plan Amendment is consistent with the comprehensive plan future land use recommendations of adjacent municipalities that have adopted extra-territorial jurisdiction for the subject parcel(s).

Response: N/A

Staff Recommendation – Comprehensive Plan Amendments: Approval

The proposed amendments meet the criteria of ZLDR Art. 3.2.6.

ZLDR AMENDMENTS

In summary, the proposed ZLDR amendments:

- Amend Article 5.4, MHC-O, Maybank Highway Corridor Overlay Zoning District as follows:
 - Creates two new districts: The Mixed Use District and the Limited Commercial District;
 - Improves pedestrian safety by requiring a 10-foot separated sidewalk the Mixed Use District and a 10-foot separated multi-use path in the Limited Commercial District;
 - Includes incentives for dedication of property for a connector road and drainage system;
 - Limits individual building footprints to 7,500 square feet and building height to 35 feet or 2.5 stories in the Limited Commercial District and 15,000 square feet and 55 feet/5 stories in the Mixed Use District;
 - Requires freestanding signs;
 - Improves vehicle access management; and
 - Limits density and lot area calculations to highland areas only (does not include freshwater wetland or OCRM Critical Line Area).
- Amend other sections of the ZLDR as follows:
 - ZLDR Sec. 8.4.2.A.4.i, Preliminary Plat Application Requirements, and ZLDR Sec. 8.5.2.B.9, Final Plat Application Requirements: To clarify that a United States Army Corps of Engineers (USACE) jurisdictional determination is required for approval of a subdivision plat application as required for the Johns Island Maybank Highway Corridor Overlay Zoning District.
 - ZLDR Sec. 9.5.4, Table 9.5.4.A.3, Buffer Types by Roadway: Amend the table to change “Maybank Highway Corridor Overlay Zoning District” to “Maybank Highway Corridor Overlay Zoning District (Johns Island), and change footnote 2 to state: “Buffer type shall be as described in the Johns Island Maybank Highway Corridor Overlay Zoning District.”

APPROVAL CRITERIA

Pursuant to Article 3.3.6 of the Charleston County Zoning and Land Development Regulations Ordinance, text amendments to the ordinance may be approved if the following approval criteria have been met:

- A. The proposed amendment corrects an error or inconsistency or meets the challenge of a changing condition.

Response: The proposed overlay zoning district meets the challenge of the changing conditions of the Johns Island Maybank Highway corridor by improving pedestrian mobility, requiring standards for safer and more efficient vehicle access, enhancing right-of-way buffer and signage requirements, and adding more restrictive density/intensity and dimensional standards.

- B. The proposed amendment is consistent with the adopted Charleston County Comprehensive Plan and goals as stated in Article 1.5;

Response: The proposed overlay zoning district is consistent with the adopted Charleston County Comprehensive Plan and goals as stated in Article 1.5 as it implements the Comprehensive Plan's priority recommendation to review and revise the Johns Island Maybank Highway Corridor Overlay Zoning District.

- C. The proposed amendment is to further the public welfare in any other regard specified by County Council;

Response: The proposed overlay zoning district furthers the public welfare by: improving the safety and efficiency of vehicle, pedestrian, and bicycle movement in the corridor; enhancing right-of-way buffer and signage requirements; and addressing drainage issues through Special Protection Area stormwater design criteria.

Public Input: All public input received to date is included in this packet.

Staff Recommendation – ZLDR Amendments: Approval

The proposed amendments meet the criteria of ZLDR Art. 3.2.6 and 3.3.6.

Planning Commission Workshop and Meeting: October 12, 2020

Notifications: On September 25, 2020, 1,434 Planning Commission meeting notifications were sent to individuals on the Johns Island, Johns Island Maybank Highway and ZLDR/Comprehensive Plan Interested Parties' Lists, as well as owners of properties within the JO-MHC-O boundary and within 300 feet of it. Additionally, the Planning Commission meeting was noticed in the Post & Courier on September 25, 2020.



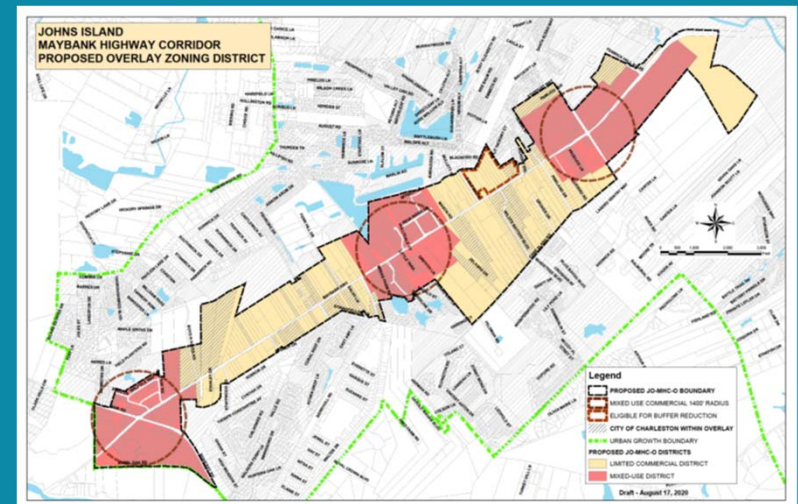
JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR OVERLAY ZONING DISTRICT (JO-MHC-O)

Planning Commission Workshop: January 13, 2020

Planning Commission Workshop and Meeting: October 12, 2020

PROJECT HISTORY AND OVERVIEW

- The JO-MHC-O District was first established as part of the 2001 ZLDR.
 - 2006: The MHC-O was amended to implement the 2003 Comprehensive Plan Review.
 - 2018: The 10-Year Comprehensive Plan Update was adopted, including the priority recommendation to coordinate with the City of Charleston and residents to review and update the MHC-O.
- The MHC-O creates two nodes (the Mixed Use District and the Limited Commercial District) and:
 - Implements consistent and coordinated land use, design, and development standards;
 - Addresses stormwater runoff, drainage, and flooding issues; and
 - Supports transportation and pedestrian safety improvements.



PROJECT OVERVIEW AND HISTORY

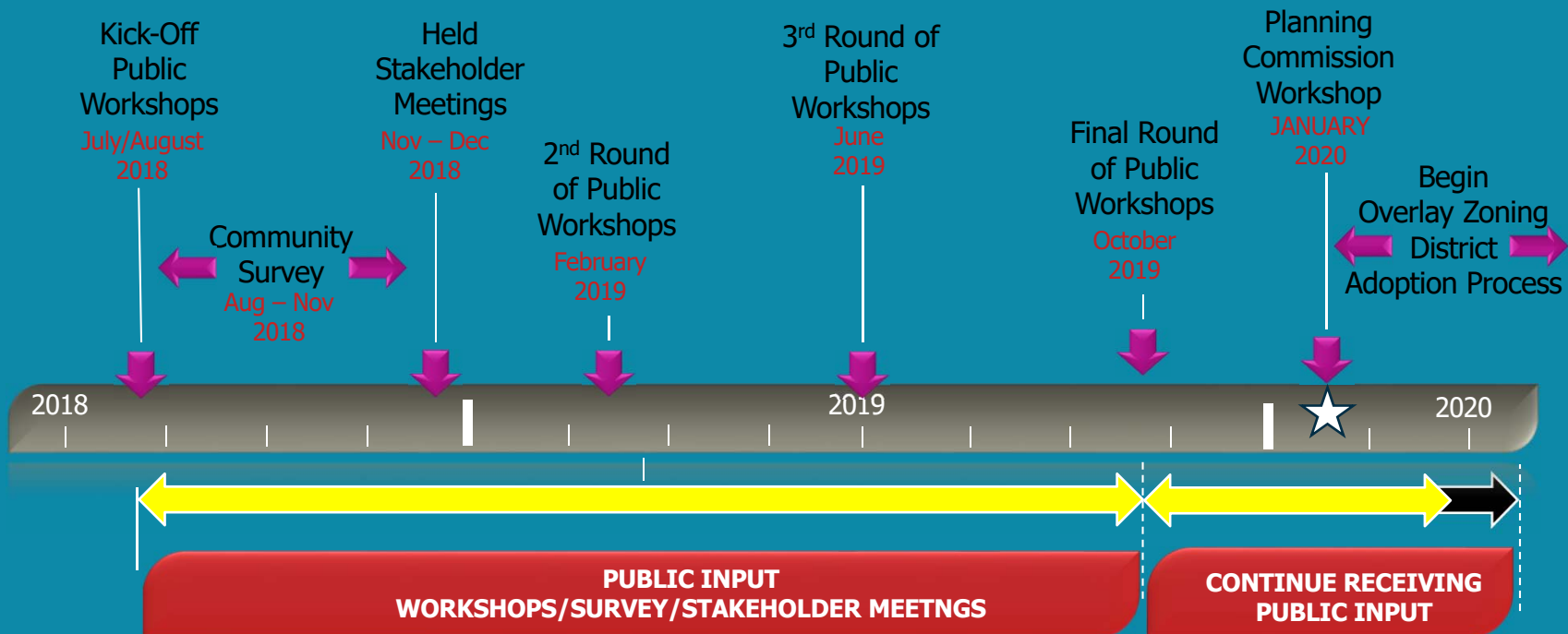
- **January 6, 2015**: County Council adopted the Comprehensive Plan Five-Year Review, including the priority recommendation to review the MHC-O District and extend the overlay boundary onto James Island.
- **July 31, 2018**: 219 community members attended the first Kick-Off Community Workshop at BEC.
- **August 23, 2018**: 141 community members attended the second Kick-Off Community Workshop at St. Johns High School.
- **August-November 2018**: Received 1,725 responses to the JO-MHC-O Community Survey.
- **November-December 2018**: Held three individual stakeholder meetings:
 - **November 28, 2018 Transportation and Drainage Stakeholder Meeting**: Included representatives from SCDOT District 6, Town of Kiawah Island, Town of Seabrook Island, and the respective City of Charleston and Charleston County Zoning and Planning, Public Works and Transportation Departments
 - **November 29, 2018 Community Stakeholder Meeting**: Included representatives from various community organizations, neighborhood associations, St. Johns Water Company, St. Johns Fire District, EMS, CCSD, and city and county zoning and planning staff.
 - **December 4, 2018 Business Owner Stakeholder Meeting**: Included representatives from the business community along the corridors of Maybank Highway on James Island, and Maybank Highway and Main Road on Johns Island.

PROJECT OVERVIEW AND HISTORY

- **February 29, 2019**: 103 community members attended the third workshop.
- **June 17, 2019**: 72 community members attended the fourth workshop.
- **October 24, 2019**: 70 community members attended the fifth workshop.
- **January 13, 2020**: The first Planning Commission workshop was held.
- **March 2020**: City and County Staff facilitated Planning Commission member site visits to view and discuss the Johns Island Maybank Highway corridor and elements of the draft overlay district.
- **March 2020 – Present**: City and County staff held multiple meetings to finalize the JO-MHC-O.
- **To date, City and County staff have met 19 times to discuss, review and revise the JO-MHC-O.**

PROJECT OVERVIEW AND HISTORY

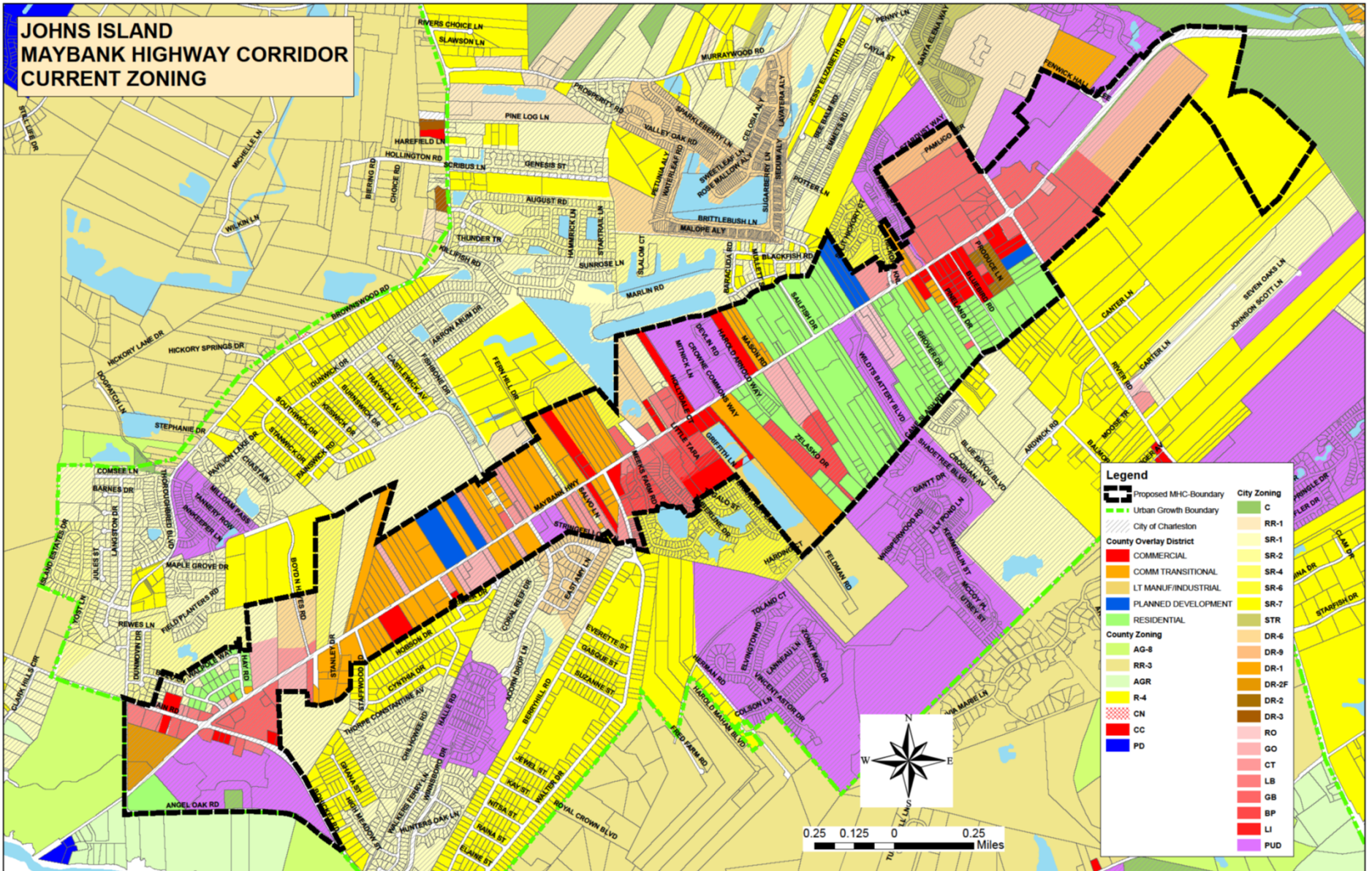
- Four Rounds of Public Workshops
- Two Community Surveys
- Three Stakeholder Meetings
- Numerous City-County Staff-level Meetings



CITY OF CHARLESTON ADOPTION SCHEDULE

- November 18, 2020: City Planning Commission
- December 15, 2020: City Council Public Hearing and first reading
- January 26, 2020: City Council second and third readings

JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR CURRENT ZONING



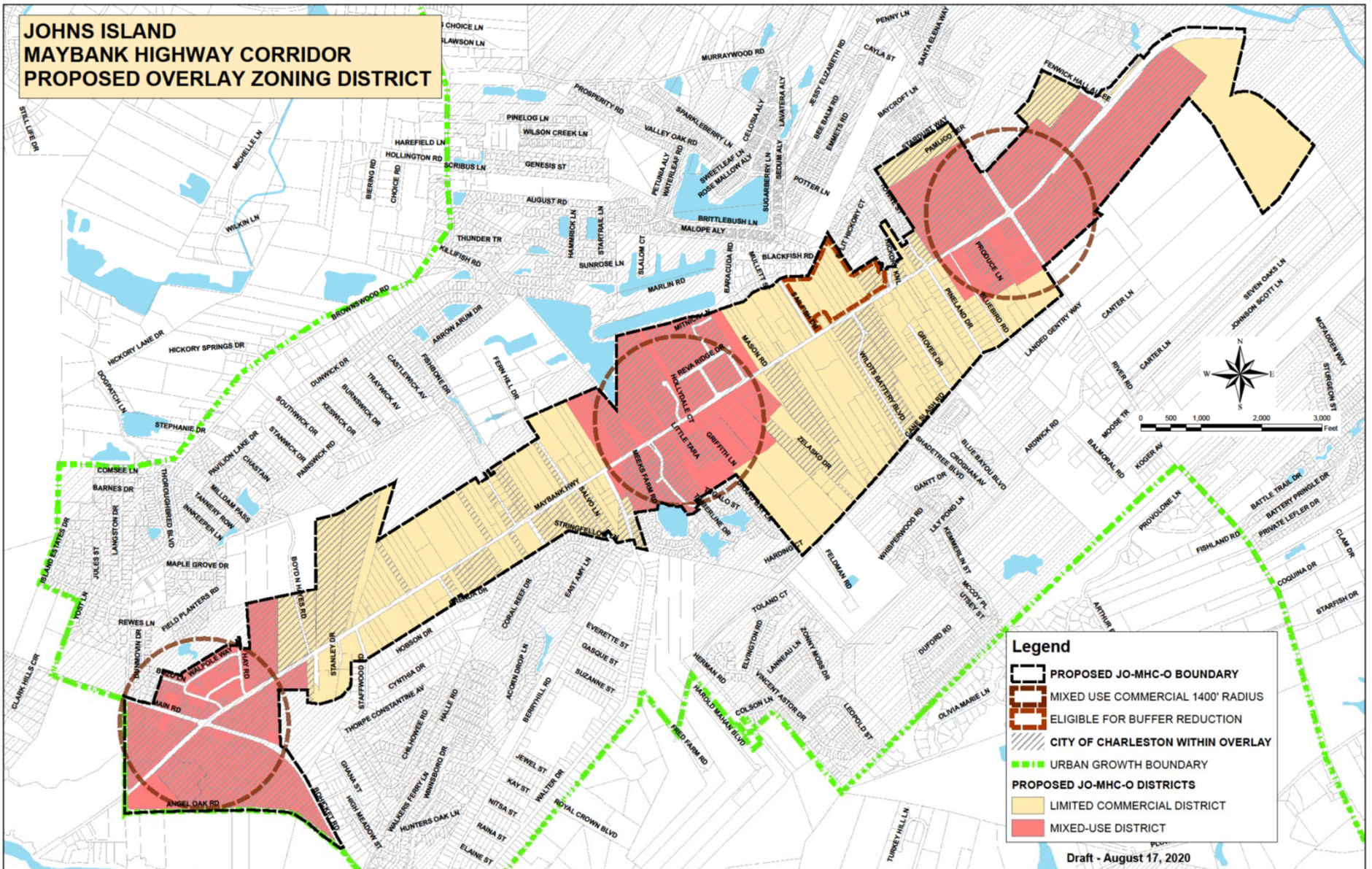
Legend

- Proposed MHC-Boundary
- Urban Growth Boundary
- City of Charleston
- County Overlay District
- COMMERCIAL
- COMM TRANSITIONAL
- LT MANUF/INDUSTRIAL
- PLANNED DEVELOPMENT
- RESIDENTIAL
- AG-8
- RR-3
- AGR
- R-4
- CN
- CC
- PD

City Zoning

- C
- RR-1
- SR-1
- SR-2
- SR-4
- SR-6
- SR-7
- STR
- DR-6
- DR-9
- DR-1
- DR-2F
- DR-2
- DR-3
- RO
- GO
- CT
- LB
- GB
- BP
- LI
- PUD

**JOHNS ISLAND
MAYBANK HIGHWAY CORRIDOR
PROPOSED OVERLAY ZONING DISTRICT**



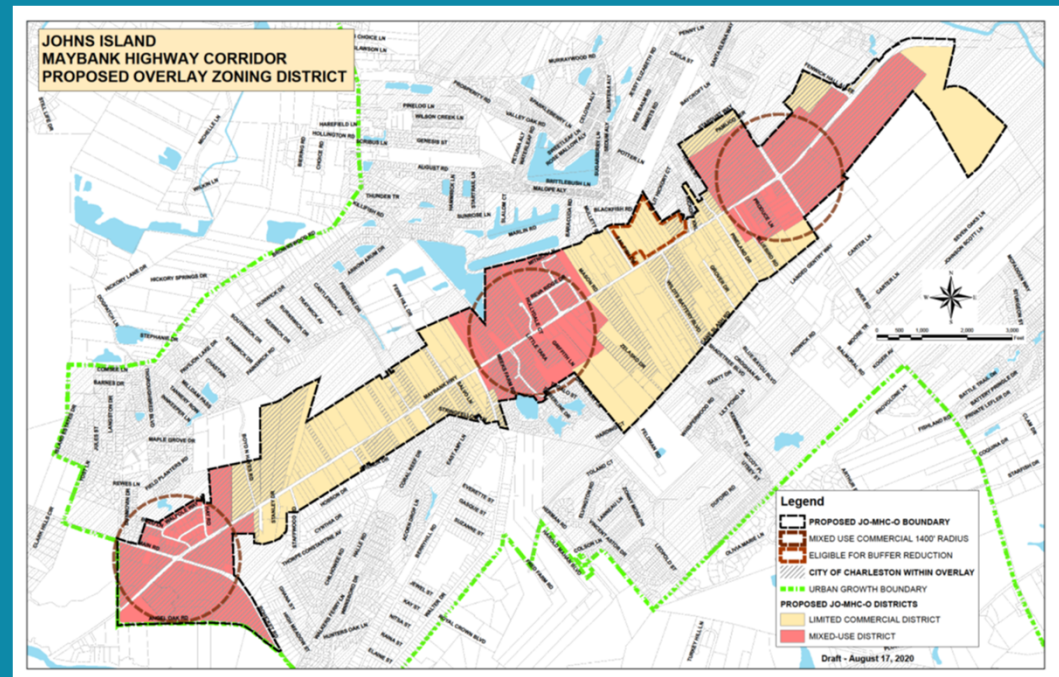
COMPREHENSIVE PLAN AMENDMENTS

- Amend Chapter 3.1 as follows:

- ✓ Update Map 3.1.4: Future Land Use to amend the JO-MHC-O;
- ✓ Update Map 3.1.5: Urban/Suburban Future Land Use Detail to amend the JO-MHC-O;
- ✓ Update Map 3.1.18: Johns Island Maybank Highway Corridor Overlay Zoning District.

- Amend Chapter 3.9 as follows:

- ✓ In Table 3.9.1, update the Project Status column to show "Completed in 2020";
- ✓ Remove the Maybank Highway Corridor Overlay Zoning District project from Table 3.9.2.



APPROVAL CRITERIA – SECTION 3.2.6

Pursuant to Article 3.2.6 of the Charleston County Zoning and Land Development Regulations Ordinance, Comprehensive Plan Amendments may be approved by the County Council only if they determine that the proposed amendment is consistent with the overall purpose and intent of the Comprehensive Plan and that any one of the following criteria has been met:

- A. There was a significant error in the original Comprehensive Plan adoption; Response: N/A
- B. In adopting the Comprehensive Plan, the County Council failed to take into account facts, projections or trends that were reasonably foreseeable to exist in the future; Response: N/A
- C. Events, trends, or facts after adoption of the Comprehensive Plan have changed the County Council's original findings made upon plan adoption; Response: N/A

APPROVAL CRITERIA – SECTION 3.2.6

D. Events, trends, or facts after adoption of the Comprehensive Plan have changed the character or condition of an area, making the proposed amendment necessary;

Response: The Maybank Highway Corridor Overlay Zoning District was developed in coordination with the residents of Johns Island and the City of Charleston in the late 1980s/early 1990s. Since that time, many changes affecting this corridor have taken place, including:

- *The City of Charleston revised their Maybank Highway Corridor Overlay Zoning District, adopting land use, density, and site design standards that differ from those adopted by Charleston County; and*
- *Charleston County Council approved the construction of significant transportation improvements in the area, including widening Maybank Highway and incorporating bicycle and pedestrian ways, creating a pitchfork road design network north and south of Maybank Highway on the eastern side of River Road to manage traffic coming onto the island from James Island, and the final leg of Interstate 526 from West Ashley, across Johns Island, and onto James Island.*

These amendments implement the Comprehensive Plan strategy to review this Overlay Zoning District in light of the changes described above and work with the public and the City of Charleston to make revisions as appropriate.

E. The proposed Comprehensive Plan Amendment is requested pursuant to and complies with Art. 3.17, Developments of County Significance; Response: N/A
or

F. The proposed Comprehensive Plan Amendment is consistent with the comprehensive plan future land use recommendations of adjacent municipalities that have adopted extra-territorial jurisdiction for the subject parcel(s). Response: N/A

RECOMMENDATION

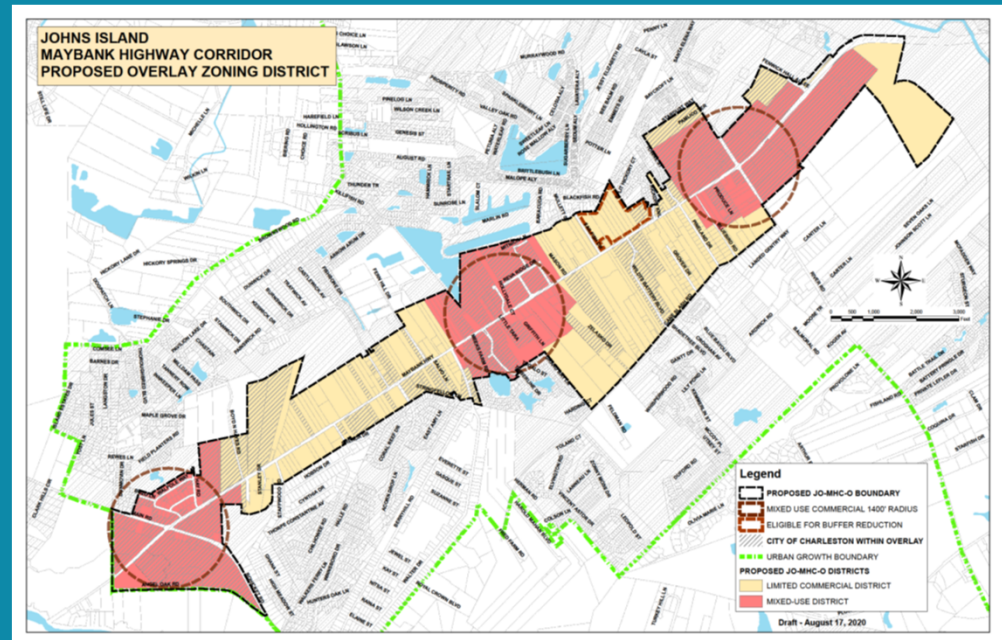
The Comprehensive Plan amendments
meet the approval criteria.

STAFF RECOMMENDATION:

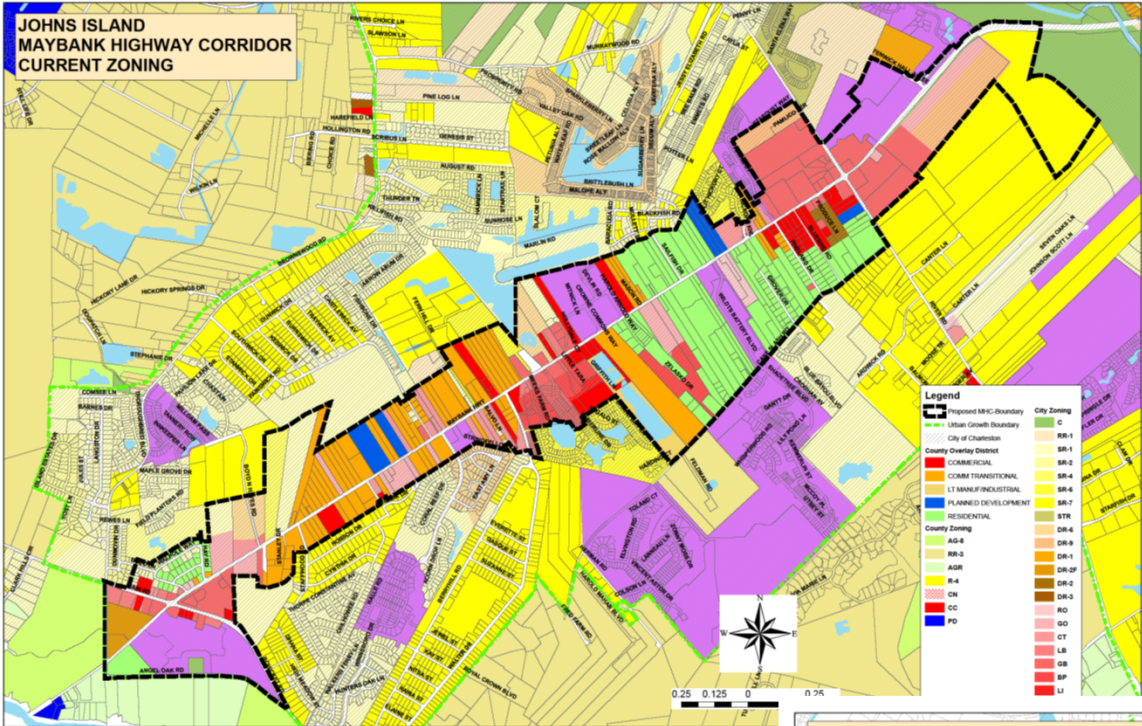
**APPROVAL OF THE
COMPREHENSIVE PLAN AMENDMENTS**

JO-MHC-O SUMMARY

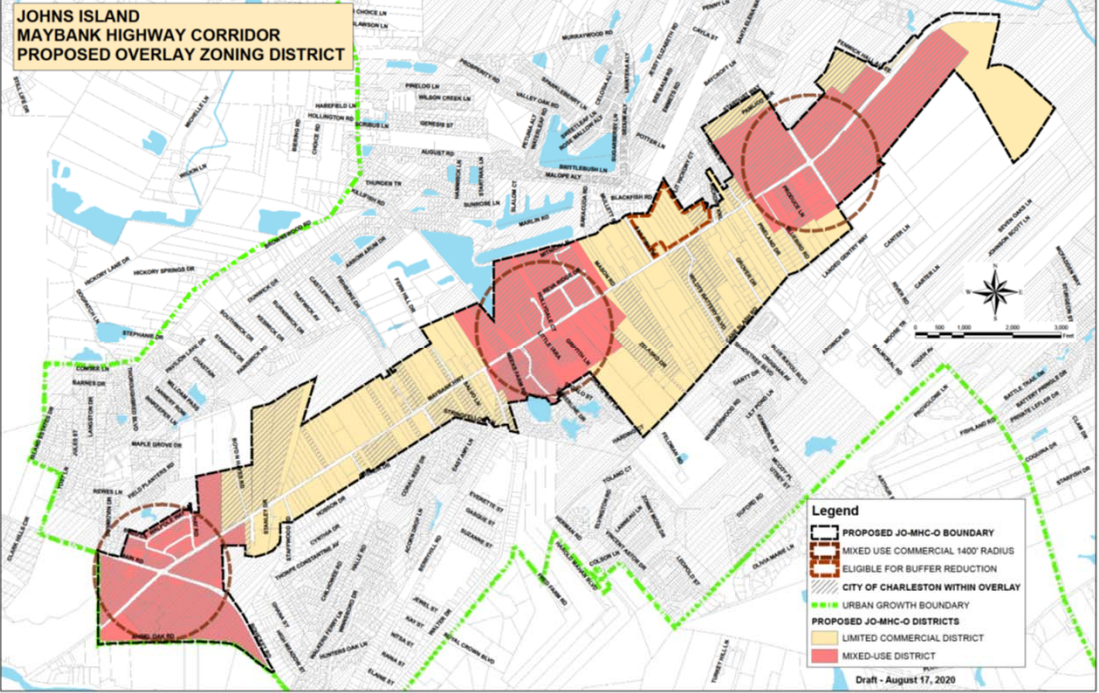
- The JO-MHC-O amends Article 5.4, MHC-O, Maybank Highway Corridor Overlay, to implement the Comprehensive Plan.
- It divides the corridor into two districts:
 - Mixed Use District: Intended for higher intensity commercial development at the three major nodes along the corridor; and
 - Limited Commercial District: intended for service and neighborhood commercial uses.
- The JO-MHC-O also:
 - Implements consistent and coordinated land use, design, and development standards;
 - Addresses stormwater runoff, drainage, and flooding issues; and
 - Supports transportation and pedestrian safety improvements.



**JOHNS ISLAND
MAYBANK HIGHWAY CORRIDOR
CURRENT ZONING**



**JOHNS ISLAND
MAYBANK HIGHWAY CORRIDOR
PROPOSED OVERLAY ZONING DISTRICT**

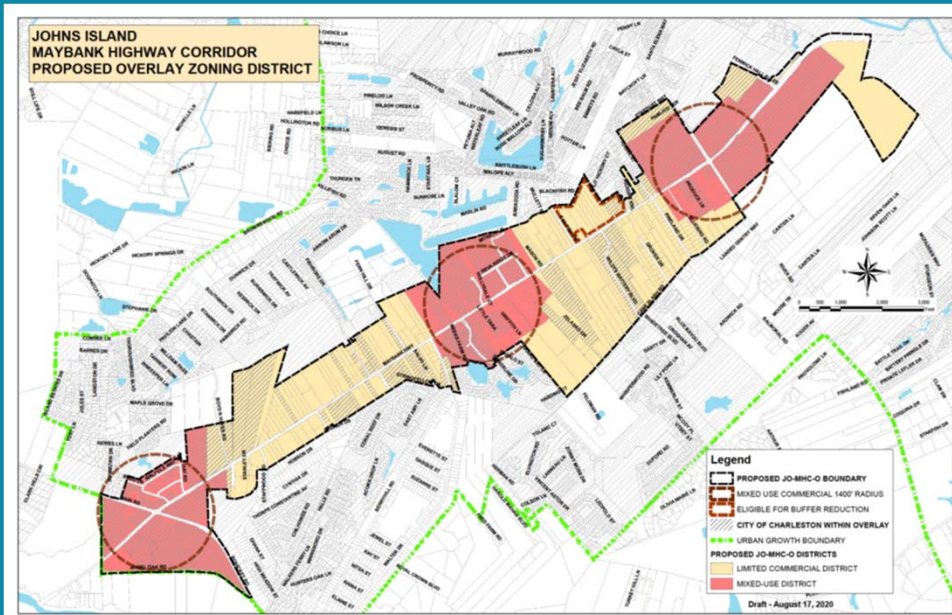


Limited Commercial District

- ▶ Farmers markets, crop production, agricultural processing and sales
- ▶ Single-family, duplexes, townhomes, tri/fourplexes
- ▶ Civic/institutional
- ▶ Offices and health care services
- ▶ Restaurants and bars
- ▶ Repair and maintenance services
- ▶ Retail sales
- ▶ Personal services
- ▶ Limited manufacturing and production
- ▶ Special trade contractors

Mixed Use District

- ▶ Farmers markets, crop production, agricultural processing and sales
- ▶ Single and **multi-family**, duplexes, townhomes, tri/fourplexes
- ▶ Civic/institutional
- ▶ Offices and health care services
- ▶ Restaurants and bars
- ▶ Repair and maintenance services
- ▶ Retail sales
- ▶ **Home improvement centers, gas stations, warehouse clubs**
- ▶ Personal services
- ▶ **Wholesale sales**
- ▶ Limited manufacturing and production
- ▶ Special trade contractors
- ▶ **Parking garages**



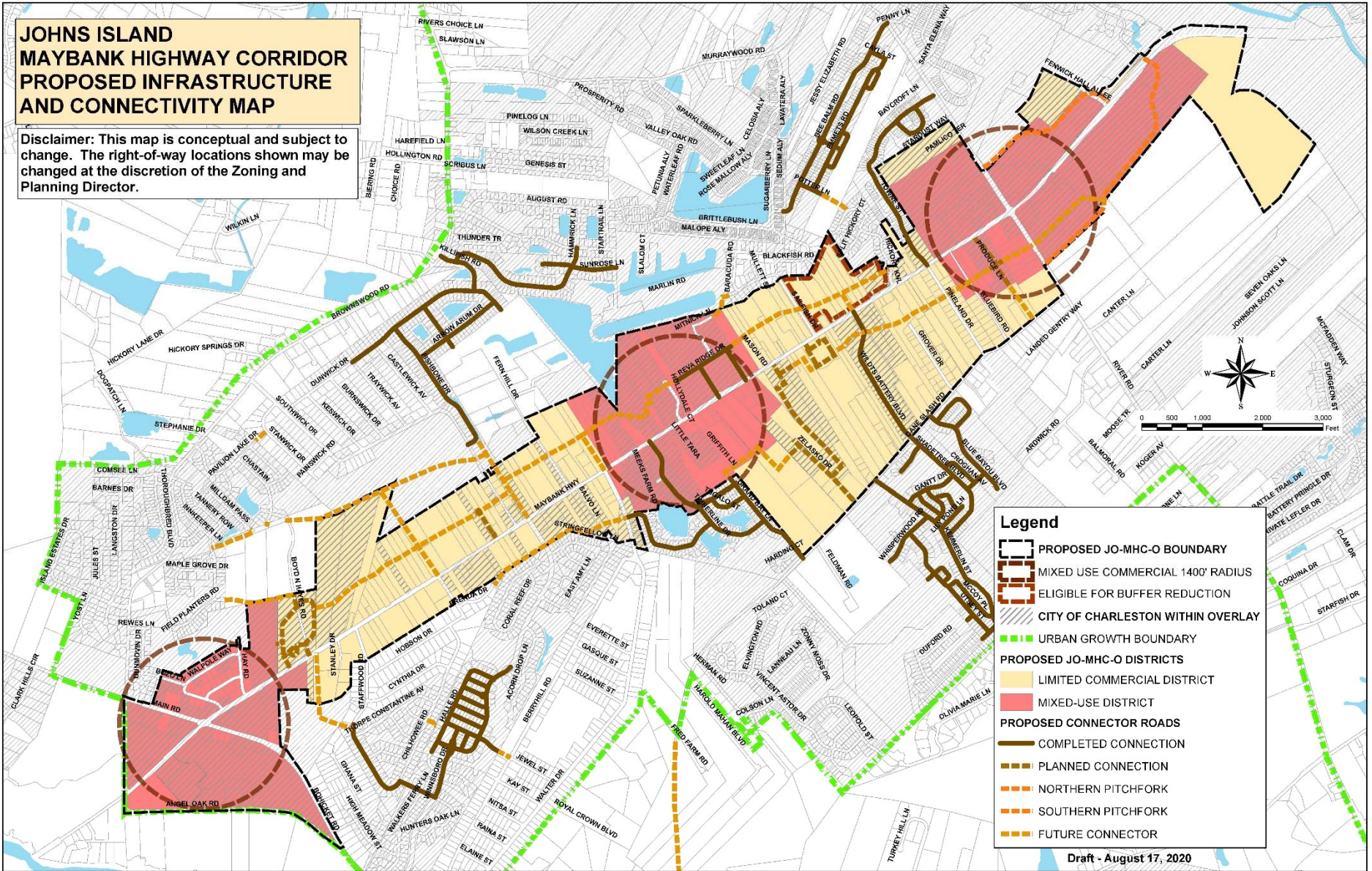
LAND USES

DEVELOPMENT STANDARDS

- Right-in/right-out only required for driveway separations of less than 150'.
- Incentives for dedication of property for a connector road and drainage system:
 - Properties located in the Mixed Use District:
 - Maximum density of 16 Dwelling Units/acre;
 - Maximum building cover of 50%;
 - Maximum building footprint of 20,000 square feet; and
 - Waiver of all required Zoning Permit and approval fees.
 - Properties located in the Limited Commercial District:
 - Maximum density of 12 Dwelling Units/acre;
 - Maximum building cover of 50%;
 - Maximum building footprint of 10,000 square feet;
 - Waiver of all required Zoning Permit and approval fees; and
 - Reduction of the Maybank Highway right-of-way buffer by up to 25 feet. If the right-of-way buffer is reduced, the 25-foot area dedicated to events and stormwater shall be altered accordingly (the first 25 feet shall be dedicated to the multi-use path and the second 25 feet shall be dedicated to treescape/plantings).

JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR PROPOSED INFRASTRUCTURE AND CONNECTIVITY MAP

Disclaimer: This map is conceptual and subject to change. The right-of-way locations shown may be changed at the discretion of the Zoning and Planning Director.



DEVELOPMENT STANDARDS

- Monument style signs:
 - Maximum height: 10'
 - Maximum size: 40 SF
 - Shared signs: Maximum height of 12' and maximum size of 60 SF

- 17' tall street lights required.

- Limits density and lot area calculations to highland areas only (does not include OCRM Critical Line area or freshwater wetlands).

- All construction activities may be subject to Special Protection Area stormwater design criteria.

PEDESTRIAN/BICYCLE ACCESS

10' wide mixed use path required in the Limited Commercial District

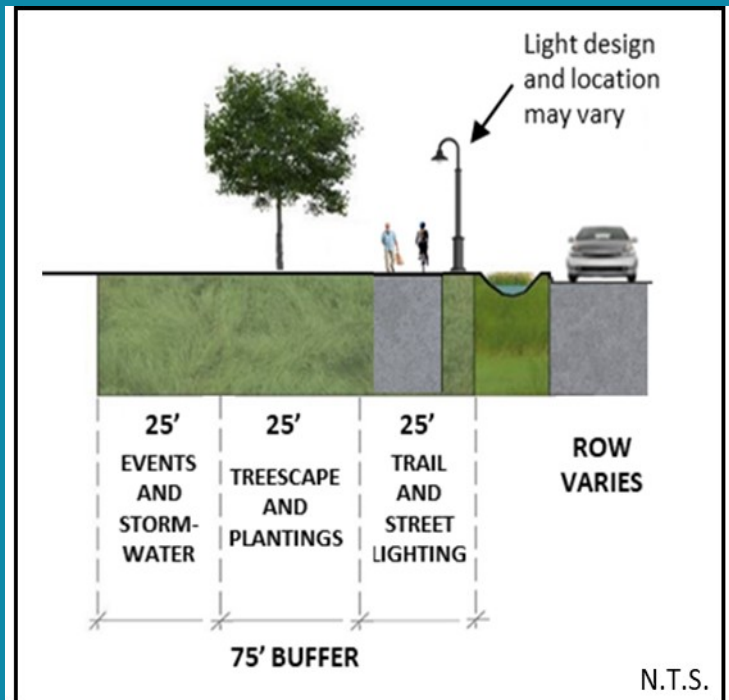


FIGURE 2
RIGHT-OF-WAY BUFFER, MAYBANK HIGHWAY

10' wide sidewalk required in the Mixed Use District

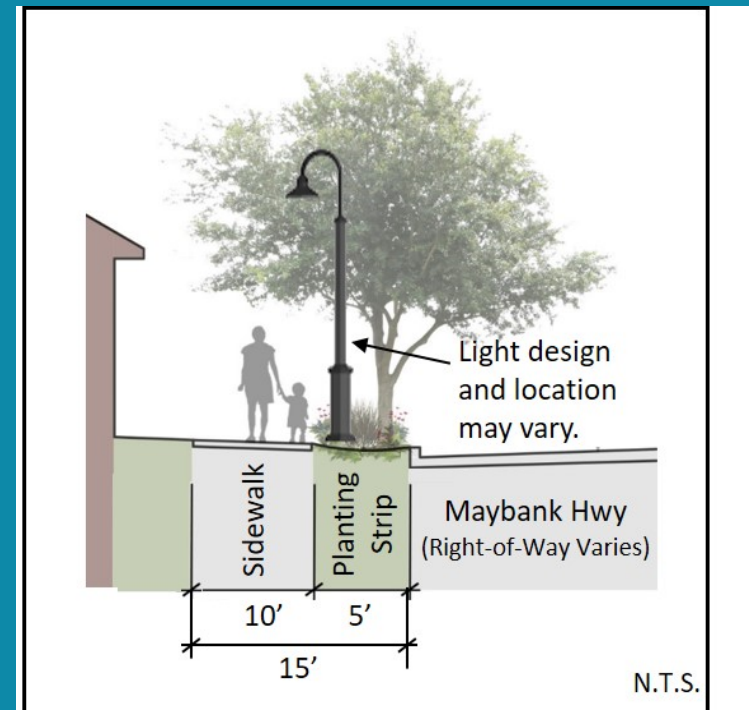


FIGURE 1
RIGHT-OF-WAY BUFFER, MAYBANK HIGHWAY

DENSITY/INTENSITY & DIMENSIONAL STANDARDS

**Table 5.18-5, Limited Commercial (LC) District
Density/Intensity and Dimensional Standards**

MAXIMUM RESIDENTIAL DENSITY	8 dwelling units per acre
MINIMUM LOT WIDTH	12 feet
MINIMUM SETBACKS	Equivalent to required buffer
OCRM CRITICAL LINE	50 feet
MAXIMUM BUILDING COVER	30%
MAXIMUM INDIVIDUAL BUILDING FOOTPRINT	7,500 square feet
MAXIMUM BUILDING HEIGHT	35 feet and 2-1/2 stories

**Table 5.18-3, Mixed-Use (MU) District
Density/Intensity and Dimensional Standards**

MAXIMUM RESIDENTIAL DENSITY	12 dwelling units per acre
MINIMUM LOT WIDTH	12 feet
MINIMUM SETBACKS	Equivalent to required buffers
OCRM CRITICAL LINE	50 feet
MAXIMUM BUILDING COVER	30%
MAXIMUM INDIVIDUAL BUILDING FOOTPRINT	15,000 square feet
MAXIMUM BUILDING HEIGHT	5 stories/55 feet

RIGHT-OF-WAY BUFFER REQUIREMENTS

Table 5.18-4, Limited Commercial (LC) District ROW Buffer Depth and Planting Schedule	
STANDARD	MAYBANK HIGHWAY (JOHNS ISLAND)
MIN. BUFFER DEPTH	75 feet [4]
MINIMUM BUFFER LANDSCAPING (Plants per 100 linear feet)	
Canopy Trees – Must be Live Oak Species	3
Understory Trees (at least 50 percent evergreen]	4
Shrubs	30
Street Trees (may be counted toward canopy tree req.)	2

Table 5.18-2, Mixed-Use (MU) District Buffer Depth and Planting Schedule	
STANDARD	MAYBANK HIGHWAY (JOHNS ISLAND)
MIN. BUFFER DEPTH	15 feet [4]
MINIMUM BUFFER LANDSCAPING (Plants per 100 linear feet)	
Canopy Trees – Must be Live Oak Species	3
Understory Trees (at least 50 percent evergreen]	3
Shrubs	25
Street Trees (may be counted toward canopy tree req.)	2

ASSOCIATED AMENDMENTS TO OTHER SECTIONS OF THE ZLDR

- **ZLDR Sec. 8.4.2.A.4.j, Preliminary Plat Application Requirements:**

To clarify that a United States Army Corps of Engineers (USACE) jurisdictional determination is required for approval of a subdivision plat application as required for the Johns Island Maybank Highway Corridor Overlay Zoning District.

- **ZLDR Sec. 8.5.2.B.9, Final Plat Application Requirements:**

To clarify that a United States Army Corps of Engineers (USACE) jurisdictional determination is required for approval of a subdivision plat application as required for the Johns Island Maybank Highway Corridor Overlay Zoning District.

- **ZLDR Sec. 9.5.4, Table 9.5.4.A.3, Buffer Types by Roadway:**

Amend the table to change "Maybank Highway Corridor Overlay Zoning District" to "Maybank Highway Corridor Overlay Zoning District (Johns Island)", and change footnote 2 to state: "Buffer type shall be as described in the Johns Island Maybank Highway Corridor Overlay Zoning District."

APPROVAL CRITERIA – SECTION 3.3.6

Pursuant to Article 3.3.6 of the Charleston County Zoning and Land Development Regulations Ordinance, text amendments to the ordinance may be approved if the following approval criteria have been met:

- A. **The proposed amendment corrects an error or inconsistency or meets the challenge of a changing condition.**

Response: The proposed overlay zoning district meets the challenge of the changing conditions of the Johns Island Maybank Highway corridor by improving pedestrian mobility, requiring standards for safer and more efficient vehicle access, enhancing right-of-way buffer and signage requirements, and adding more restrictive density/intensity and dimensional standards.

- B. **The proposed amendment is consistent with the adopted Charleston County Comprehensive Plan and goals as stated in Article 1.5;**

Response: The proposed overlay zoning district is consistent with the adopted Charleston County Comprehensive Plan and goals as stated in Article 1.5 as it implements the Comprehensive Plan's priority recommendation to review and revise the Johns Island Maybank Highway Corridor Overlay Zoning District.

- C. **The proposed amendment is to further the public welfare in any other regard specified by County Council;**

Response: The proposed overlay zoning district furthers the public welfare by: improving the safety and efficiency of vehicle, pedestrian, and bicycle in the corridor; creating consistency in permitted land uses; and addressing drainage issues through Special Protection Area stormwater design criteria.

RECOMMENDATION

The ZLDR text amendments
meet the approval criteria.

STAFF RECOMMENDATION:

**APPROVAL OF AMENDMENTS TO ART. 5.4, JO-
MHC-O (CURRENTLY MHC-O), JOHNS ISLAND
MAYBANK HIGHWAY
CORRIDOR
OVERLAY ZONING DISTRICT**

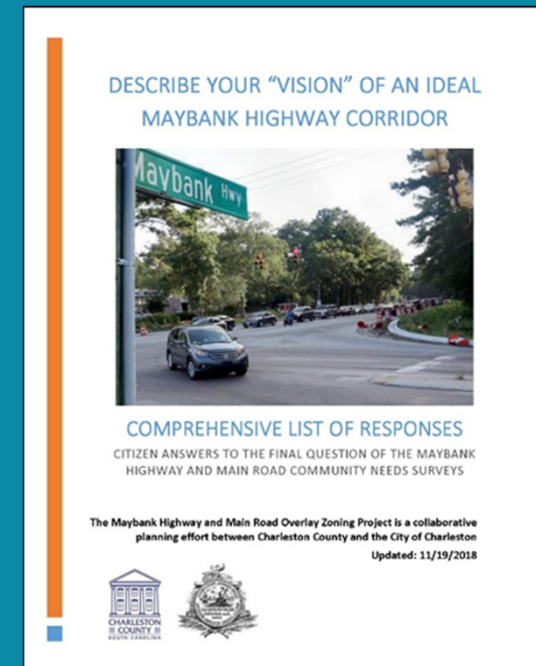
NOTIFICATIONS

- September 25, 2020 (for the Oct. 12, 2020 Planning Commission Workshop & Meeting)
 - 1,434 Planning Commission meeting notifications were sent to individuals on the County's Johns Island, Johns Island Maybank Highway and Main Road, and ZLDR/Comprehensive Plan Interested Parties' Lists, as well as owners of properties within the JO-MHC-O boundary and within 300 feet of it.
 - Ad ran in the *Post & Courier*.

SUMMARY OF THE PUBLIC INPUT RECEIVED FROM COMMUNITY SURVEY

As part of the initial public input process, a Johns Island Community Survey was conducted during the month of August 2018. The survey was specifically tailored to the Maybank Highway and Main Road corridors on Johns Island, and was made available in paper at each of the community meetings, at the Johns Island public library, and online on the Charleston County Planning Department's Maybank Highway and Main Road Overlay Zoning Project webpage. A total of 1,725 Johns Island Community Surveys were completed, with the results showing the following community concerns:

- Widening Maybank Highway to four lanes;
- Improved traffic and road conditions;
- Preserve the Rural Area and natural resources;
- More commercial ventures so residents are not required to leave the island;
- Improved infrastructure to address drainage and flooding threats;
- Address transportation issues, including alternative modes of transportation (bike paths, pedestrian ways, etc.), better connectivity of existing road networks, and evacuation routes; and
- Slowed residential development, at least until infrastructure improvements are made.





JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR OVERLAY ZONING DISTRICT (JO-MHC-O)

Planning Commission Workshop: January 13, 2020

Planning Commission Workshop and Meeting: October 12, 2020

**Draft Article 5.18:
Proposed Johns Island
Maybank Highway
Corridor Overlay Zoning
District ZLDR
Amendments8:**

ARTICLE 5.18 JO-MHC-O, JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR OVERLAY ZONING DISTRICT

Sec. 5.18.1 Statement of Findings

The MHC-O, Maybank Highway Corridor Overlay Zoning District, was first established as part of the Charleston County Zoning and Land Development Regulations Ordinance (ZLDR) when it was adopted on November 20, 2001. The MHC-O only included unincorporated properties along Maybank Highway located on Johns Island. In 2006, the MHC-O district was amended to implement the recommendations of the 2003 *Charleston County Comprehensive Plan Five-Year Review*, but still only included unincorporated properties along Maybank Highway on Johns Island. In 2018, the *10-Year Comprehensive Plan Update* was adopted, and includes the priority recommendation to coordinate with the City of Charleston to review the MHC-O District and extend the overlay boundary onto James Island. The JO-MHC-O, Johns Island Maybank Highway Corridor Overlay Zoning District, implements that recommendation.

This Article replaces the original MHC-O District, and is renamed as JO-MHC-O, Johns Island Maybank Highway Corridor Overlay Zoning District. The corridor is a primary gateway to Johns Island and acts as one of the main thoroughfares for destinations such as Kiawah Island, Seabrook Island, and Wadmalaw Island. Maybank Highway on Johns Island carries a high volume of vehicular traffic each day. Moreover, some of the properties within this corridor are located within the jurisdictional limits of the City of Charleston, while some are located within unincorporated Charleston County. The revised JO-MHC-O District was adopted to implement traffic safety measures, address infrastructure deficiencies, improve the visual character of the corridor, and create consistency between the County of Charleston and the City of Charleston concerning land development regulations.

Sec. 5.18.2 Purpose and Intent

The purpose of the JO-MHC-O District is to create a corridor that is well-planned, attractive, and preserves and improves existing development patterns where appropriate through the implementation of traffic safety measures and land use and design standards. It is also intended to be consistent with similar regulations adopted by the City of Charleston, ensure safe and efficient vehicle, pedestrian, and bicycle movement through traffic safety measures and access management standards, and to address stormwater runoff, drainage, and flooding issues along the corridor.

A separate regional planning effort involving numerous public and private stakeholders was conducted concurrent with the preparation of the JO-MHC-O District. This planning effort, named Dutch Dialogues Charleston, culminated in a final report released in September 2019. The report addresses “long-term planning to manage the risks and the opportunities provided by the Lowcountry’s dynamic water systems.” It should be noted that implementation of recommendations included in the Dutch Dialogues Charleston Final Report may result in future amendments to the JO-MHC-O District.

Sec. 5.18.3 Applicability and Effect of the Overlay Zoning District

A. Applicability

The standards of this Article shall apply to all development of unincorporated properties within the JO-MHC-O District, as shown on the map titled “Johns Island Maybank Highway Corridor Overlay Zoning District,” except Single-Family Detached Dwellings.

B. Effect of Overlay Zoning District

In case of conflict between the regulations of this Article and other regulations in this Ordinance, the regulations of this Article shall control.

C. Variances

The following requirements are ineligible for Zoning Variances in addition to those listed in Sec. 3.10.1, Applicability; Limitations, of this Ordinance:

1. Sec. 5.18.7.B, Vehicle Access;
2. Sec. 5.18.7.D, Pedestrian Access and Sidewalks/Multi-Use Path;
3. Sec. 5.18.8.A.2, MU District Right-of-Way Buffers; and
4. Sec. 5.18.9.A.2, LC District Right-of-Way Buffers.

Sec. 5.18.4 COORDINATION WITH ADJACENT JURISDICTIONS

A letter of coordination from the City of Charleston shall be required as part of all land development applications in the JO-MHC-O District with the exception of applications for Single-Family Detached Dwellings. The purpose of this coordination is to ensure that development is consistent with similar requirements adopted for the JO-MHC-O District by the City of Charleston.

Sec. 5.18.5 USE REGULATIONS

A. Table 5.18-1, Johns Island Maybank Highway Overlay Zoning District Use Table. Table 5.18-1 lists the Principal Uses permitted in the districts as identified and described in this Overlay Zoning District and as shown on the map entitled "Johns Island Maybank Highway Corridor Overlay Zoning District." The following is a description of the codes used in the table:

1. **Uses Allowed by Right.** "A" indicates uses allowed by right.
2. **Uses Subject to Conditions.** "C" indicates uses subject to conditions. A cross-reference to the applicable conditions can be found in the column entitled "Conditions."
3. **Uses Subject to Special Exception.** "S" indicates uses allowed only if reviewed and approved in accordance with the Special Exception procedures of this Ordinance, subject to compliance with use-specific conditions. A cross-reference to the applicable conditions can be found in the column entitled "Conditions."
4. **Prohibited Uses.** Blank cells indicate uses that are not permitted.

B. New or Unlisted Uses and Interpretation. The Zoning and Planning Director shall be authorized to make use determinations whenever there is a question regarding the category of use based on the definitions contained in Chapter 12, Definitions, of this Ordinance.

C. Nonconforming Uses. The requirements of Article 10.2, Nonconforming Uses, of this Ordinance apply to all development subject to the JO-MHC-O District, provided, however, that the following requirement shall apply in place of those contained in Sec. 10.2.4, Loss of Legal Nonconformity Status, sub-section A:

1. **Abandonment.** If a Nonconforming Use is replaced with another use or is discontinued for any reason for a period of more than 36 consecutive months, the use shall be considered abandoned. Once abandoned, the legal nonconforming status of the use shall be lost and re-establishment of a Nonconforming Use shall be prohibited. Any subsequent use of the property shall comply with the regulations of the JO-MHC-O District.

D. Accessory Uses. Accessory Uses shall be allowed pursuant to Art. 6.5, Accessory Uses, of this Ordinance.

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
 October 12, 2020 Planning Commission Workshop and Meeting

Table 5.18-1, Johns Island Maybank Highway Overlay Zoning District Use Table			
Uses	Mixed-Use (MU) District	Limited Commercial (LC) District	Conditions
AGRICULTURAL			
AGRICULTURAL AND ANIMAL PRODUCTION, PROCESSING AND SUPPORT			
Aquaculture			
Mariculture			
Apiculture (Bee Keeping)			
Animal and Insect Production			
Concentrated Animal Feeding Operations			
Horticultural, Greenhouse, Nursery, Crop and Floriculture Production	A	A	
Hemp Crop Production and/or Processing	C	C	§5.18.6.A
Winery			
Agricultural Processing	A	A	
Agricultural Sales or Services	C	C	§5.18.6.B
Roadside Stand	A	A	
Farmers Market	A	A	
Sweetgrass Basket Stand	A	A	
Community Garden	A	A	
FORESTRY AND LOGGING			
Bona Fide Forestry Operations			
Lumber Mills, Planing, or Saw Mills			
RESIDENTIAL			
ASSISTED LIVING			
Assisted Living	A	A	
MANUFACTURED HOUSING			
Manufactured Housing Unit			
Manufactured Housing Park			
MULTI-FAMILY DWELLING			
Dwelling, Multi-Family	A		
Triplex and Fourplex	A	A	
Duplex	A	A	
Dwelling Group			
Dwelling, Single-Family, Attached	C	C	§5.18.6.C
SHORT-TERM RENTAL			
Short-Term Rental Property, Limited Home Rental (LHR)			
Short-Term Rental Property, Extended Home Rental (EHR)			

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
October 12, 2020 Planning Commission Workshop and Meeting

Table 5.18-1, Johns Island Maybank Highway Overlay Zoning District Use Table			
Uses	Mixed-Use (MU) District	Limited Commercial (LC) District	Conditions
SINGLE-FAMILY DWELLING			
Dwelling, Single-Family Detached	A	A	
OTHER RESIDENTIAL USES			
Transitional Housing	A	S	
Child Caring Institution			
Emergency Shelter	A	A	
Affordable Dwelling Units	C	C	§5.18.6.E
Group Residential	A	A	
Farm Labor Housing			
CIVIC / INSTITUTIONAL			
COURTS AND PUBLIC SAFETY			
Court of Law	A	A	
Correctional Institutions			
Parole Office or Probation Office			
Safety Service	A	A	
DAY CARE SERVICES			
Adult Day Care Service	A	A	
Family Home	C	C	§5.18.6.F
Group Home	A	A	
Child Care Center	A	A	
Day Camp	A	A	
DEATH CARE SERVICES			
Cemetery	A	A	
Funeral Services	A	A	
EDUCATIONAL SERVICES			
Pre-school or Educational Nursery	A	A	
School, Primary	A	A	
School, Secondary	A	A	
Higher Education Facility	A	A	
Personal Improvement Education	A	A	
HEALTH CARE SERVICES			
Medical Office or Outpatient Clinic	A	A	
Community Residential Care Facility	C	C	§5.18.6.G
Counseling Service	A	A	

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
October 12, 2020 Planning Commission Workshop and Meeting

Table 5.18-1, Johns Island Maybank Highway Overlay Zoning District Use Table			
Uses	Mixed-Use (MU) District	Limited Commercial (LC) District	Conditions
Intermediate Care Facility for Individuals with Intellectual Disabilities	A	A	
Health Care Laboratory	A	A	
Home Health Agency	A	A	
Hospital; Hospice Facility	A	A	
Outpatient Facility for Chemically Dependent or Addicted Persons	A	A	
Rehabilitation Facility	A	A	
Residential Treatment Facility for Children or Adolescents (mental health treatment)	A	A	
MUSEUM, HISTORIC SITE AND SIMILAR INSTITUTIONS			
Historic Site	A	A	
Library or Archives	A	A	
Museums	A	A	
Nature Exhibition	A		
Botanical Gardens	A		
Zoo	S		
POSTAL SERVICE			
Postal Service, United States	A	A	
RECREATION AND ENTERTAINMENT			
Community Recreation	A	A	
Fishing, Hunting or Recreational Guide Service	A	A	
Golf Courses or Country Clubs			
Parks and Recreation	A	A	
Recreation and Entertainment, Indoor	A	A	
Recreation and Entertainment, Outdoor	C	C	§5.18.6.H
Drive-In Theater			
Golf Driving Range			
Outdoor Shooting Range			
Special Events	C	C	§5.18.6.I
RELIGIOUS, CIVIC, PROFESSIONAL AND SIMILAR ORGANIZATIONS			
Business, Professional, Labor, Political Organizations; Social or Civic Organization; Social Club or Lodge	A	A	
Religious Assembly	A	A	
UTILITIES AND WASTE-RELATED USE			
Utility Service, Major	C	C	§5.18.6.J
Utility Service, Minor	A	A	

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
October 12, 2020 Planning Commission Workshop and Meeting

Table 5.18-1, Johns Island Maybank Highway Overlay Zoning District Use Table			
Uses	Mixed-Use (MU) District	Limited Commercial (LC) District	Conditions
Septic Tank Installation, Cleaning, or Related Services	A	A	
Solid Waste Disposal Facility (Public or Private)			
COMMERCIAL			
ACCOMMODATIONS			
Short-Term Rental Property, Commercial Guest House (CGH)	C	C	§5.18.6.D
Hotels or Motels	A		
RV (Recreational Vehicle) Park			
Campground			
ANIMAL SERVICES			
Stable, Commercial			
Stable, Private			
Kennel	A	A	
Pet Store or Grooming Salon	A	A	
Small Animal Boarding	A	A	
Veterinary Service	A	A	
FINANCIAL SERVICES			
Banks or Financial Services	A	A	
Short-term Lender			
FOOD SERVICES AND DRINKING PLACES			
Bar or Lounge	S	S	
Catering Service	A	A	
Restaurant, Fast Food	C	C	§5.18.6.K
Restaurant, General	C	C	§5.18.6.K
Sexually Oriented Business			
INFORMATION INDUSTRIES			
Communication Service; Data Processing Service; and Publishing Industry	A	A	
Communications Towers	C	C	§6.4.5
OFFICES			
Administrative or Business Office; Government Office; Professional Office	A	A	
OTHER NONRESIDENTIAL DEVELOPMENT			
Convention Center or Visitors Bureau	A	A	
Heavy Construction Service or General Contractor	C	C	§5.18.6.L
Billboard			
Special Trade Contractor (Offices/Storage)	C	C	§5.18.6.L

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
October 12, 2020 Planning Commission Workshop and Meeting

Table 5.18-1, Johns Island Maybank Highway Overlay Zoning District Use Table			
Uses	Mixed-Use (MU) District	Limited Commercial (LC) District	Conditions
PARKING, COMMERCIAL			
Parking Lot	A	A	
Parking Garage	A		
RENTAL AND LEASING SERVICES			
Charter Boat or other Recreational Watercraft Rental Service			
Commercial or Industrial Machinery or Equipment, Construction Tools or Equipment, Heavy Duty Truck or Commercial Vehicle Rental or Leasing	C	C	§5.18.6.M
Consumer Goods Rental Centers	A	A	
Self-Service Storage Facility	C	C	§5.18.6.N
Vehicle Rental or Leasing	C	C	§5.18.6.M
REPAIR AND MAINTENANCE SERVICES			
Boat Yard	A	A	
Repair Service, Consumer	C	C	§5.18.6.L
Repair Service, Commercial	C	C	§5.18.6.L
Vehicle Repair or Service	A	C	§5.18.6.L
RETAIL SALES			
Nonstore Retailers	A	A	
Fuel Dealer; Heating Oil Dealer; Liquefied Petroleum Gas (Bottled Gas) Dealer	C	C	§5.18.6.O
Home Improvement Centers	A		
Food Sales	A	A	
Food Truck	A	A	
Liquor, Beer, or Wine Sales	S	S	
Retail Sales or Services, General; Building Materials or Garden Equipment and Supplies Retailer	A	A	
Convenience Stores	A		
Duplicating or Quick Printing Service; Private Postal or Mailing Service	A	A	
Pawn Shop			
Warehouse Clubs or Superstores	A		
Service Stations, Gasoline	C		§5.18.6.R
Truck Stop			
Vehicle Sales	C	C	§5.18.6.M

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
October 12, 2020 Planning Commission Workshop and Meeting

Table 5.18-1, Johns Island Maybank Highway Overlay Zoning District Use Table			
Uses	Mixed-Use (MU) District	Limited Commercial (LC) District	Conditions
Heavy Duty Truck or Commercial Vehicle Dealer; Manufactured Home Dealer	C	C	§5.18.6.M
Vehicle Parts, Accessories or Tire Stores	A	A	
RETAIL OR PERSONAL SERVICES			
Consumer Convenience Service	A	A	
Hair, Nail, or Skin Care Services	A	A	
Job Training or Placement Service	A	A	
Personal Improvement Service	A	A	
Physical Fitness or Health Club	A	A	
Tattoo Facility			
Services to Buildings or Dwellings	A	A	
Landscaping and Horticultural Service	A	A	
VEHICLE AND WATERCRAFT STORAGE			
Vehicle Storage	C	C	§5.18.6.P
Impound Yard			
Towing Facility			
Boat Ramps			
Community Dock			
Commercial Dock			
Marina			
WHOLESALE SALES			
Wholesale Sales	A		
Clay or Related Products, Construction Material Wholesaler	A		
Flower, Nursery Stock or Florists' Supplies Wholesalers	A		
Petroleum Wholesalers			
INDUSTRIAL			
INDUSTRIAL SERVICES			
Laundry, Dry Cleaning, or Carpet Cleaning Plants			
Photo Finishing Laboratory			
Research and Development Laboratory	A	A	
Scrap and Salvage Service			

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
 October 12, 2020 Planning Commission Workshop and Meeting

Table 5.18-1, Johns Island Maybank Highway Overlay Zoning District Use Table			
Uses	Mixed-Use (MU) District	Limited Commercial (LC) District	Conditions
MANUFACTURING AND PRODUCTION, GENERAL			
Artisan and Craftsman	A	A	
Manufacturing and Production	A		
Aircraft Manufacturing and Production, including Related Parts			
Chemical Manufacturing and Production			
Clay or Related Products, Furniture, Cabinets or Related Products, Toy or Artwork, or Wood Products Manufacturing and Production	A	A	
Microbrewery	C	C	§5.18.6.Q
Pulp Mill or Paper Mill, Rendering Plant			
Slaughter House and Meat Packing			
Stone or Shell Products Manufacturing and Production			
WAREHOUSE AND FREIGHT MOVEMENT			
Warehouse and Distribution Facility			
Container Storage Facility			
Freight Forwarding Facility			
Fuel Storage Facility			
Grain Terminals and Elevators			
Stockpiling of Sand, Gravel, or other Aggregate Materials			
Storage or Manufacturing of Weapons or Ammunition			
OTHER USES			
RECYCLING SERVICES			
Recycling Center			
Recycling Collection Facility	A	A	
RESOURCE EXTRACTION/MINING			
Resource Extraction/Mining			
TRANSPORTATION			
Aviation			
Private Air Strip			
Railroad Facility			
Sightseeing Transportation, Land or Water	A		
Taxi or Limousine Service	A		
Urban Transit Systems	A	A	
Water Transportation			

Sec. 5.18.6 Use Conditions

A. Hemp Production and Processing

1. **Industrial Hemp License.** Evidence of the appropriate South Carolina Department of Agriculture Industrial Hemp License (Grower or Processor) shall be submitted with all Site Plan Review applications.
2. **Approval from South Carolina Department of Agriculture.** Evidence of the appropriate South Carolina Department of Agriculture Hemp Grower or Processor application approval shall be submitted with any Site Plan Review Application.

B. Agricultural Sales or Service, Retail Sales or Service, General, and Building Materials or Garden Equipment and Supplies Retailer

Tractor-trailer containers are prohibited in outside storage areas.

C. Single-Family Attached Dwellings

Single-Family Attached Dwellings shall be subject to the following standards:

1. **Number of Attached Units in a Single Structure.**
No single structure may contain more than eight Single-Family Attached Dwellings.
2. **Density/Intensity and Dimensional Standards.**
Single-Family Attached Dwellings shall comply with the applicable Density/Intensity and Dimensional Standards of this Article.
3. **Accessory Structures.**
All Accessory Structures shall be located on the same Lot as the associated principal Single-Family Attached Dwelling and shall be for the private use of the property occupant(s). A minimum interior setback of three feet is required between an Accessory Structure and the interior lot lines, provided that an Accessory Structure may be located on one of the zero lot lines when constructed of a material finish matching the Dwelling Unit exterior or when the Accessory Structure is the same height and materially a part of a fence or wall.
4. **Design Standards.**
 - a. *Front Façade.* The front façade of a Single-Family Attached Dwelling may not include more than 40 percent garage wall area.
 - b. *Roof.* The roof of each Single-Family Attached Dwelling must be distinct from the other through either separation of roof pitches or direction, or other variation in roof design.
 - c. *Windows.* At least 10 percent of the area of each façade that faces a street must be comprised of windows.

D. Short-Term Rentals, Commercial Guest House (CGH)

Short-Term Rentals, Commercial Guest House (CGH) use shall comply with the requirements of Article 6.8 of this Ordinance as regulated for the Community Commercial (CC) Zoning District.

E. Affordable Dwelling Units

1. Affordable Dwelling Units in the Mixed-Use (MU) District shall comply with the requirements of Article 6.4.19 of this Ordinance as regulated for the Community Commercial (CC) Zoning District.
2. Affordable Dwelling Units in the Limited Commercial (LC) District shall comply with the requirements of Article 6.4.19 of this Ordinance as regulated for the Neighborhood Commercial (NC) Zoning District.

F. Family Home

A Family Home, as defined in this Ordinance, does not require compliance with the Site Plan Review procedures contained within this Ordinance.

G. Community Residential Care Facility

A Community Residential Care Facility that provides care for nine or less persons shall be considered a Family, and is an allowed use in all Zoning Districts pursuant to the Fair Housing Act, Sec. 800 [42 U.S.C. 3601].

H. Outdoor Recreation and Entertainment

Any structure or activity use area established in connection with Outdoor Recreation and Entertainment uses shall have a vegetated land use buffer of not less than 50 feet from any property that contains a residential use or is located in an agricultural, residential or office Zoning District, except where such property line abuts a street, in which case the front setback established for the Zoning District shall apply.

I. Special Events

Special Events established as a principle use in the MU and LC Districts shall comply with the requirements of Article 6.7 of this Ordinance as regulated for the Community Commercial (CC) Zoning District.

J. Utility Service, Major

1. Sewage Disposal Facility, Water and Sewage Treatment Facility, Water Storage Tank, and Electric or Gas Power Generation Facility

Vegetated Buffers. Any structure established in connection with a Water Storage Tank, Water and Sewage Treatment Facility, Sewage Disposal Facility, or Electric or Gas Power Generation Facility shall have a vegetated buffer of not less than 50 feet from any property line, in compliance with the Chapter 9 buffer standards of this Ordinance.

2. Utility Substation, Electrical or Telephone Switching Facility, Sewage Collector or Trunk Line, Utility Pumping Station, and Water Main

- a. *Vegetated Buffers for Structures Less than 120 Square Feet.* Above ground structures that have a cumulative area of 120 square feet or less, associated with underground utilities such as meters, which are necessary for maintenance and monitoring, shall have a vegetated buffer of 10 feet from all property boundaries, in compliance with Chapter 9 buffer standards of this Ordinance;
- b. *Vegetated Buffers for Structures Greater than 120 Square Feet.* Above ground structures that have a cumulative area of greater than 120 square feet established in connection with a Utility Substation, Electrical or Telephone Switching Facility, Sewage Collector or Trunk Line, or Utility Pumping Station shall have a vegetated buffer of 25 feet from all property boundaries, or the minimum setback of the overlay district, whichever is greater; and
- c. *Storage of Vehicles and Equipment.* The accessory storage of vehicles and equipment on the premises shall be prohibited in the LC District.

K. Restaurants

All proposed Restaurants serving beer or alcoholic beverages located within 500 feet of the property line of a lot in a residential Zoning District or a lot containing a residential use shall require review and approval in accordance with the Special Exception procedures of this Ordinance. Distances shall be measured from the nearest property line of the subject parcel to the nearest property line of a lot containing a residential use or located in a residential Zoning District.

L. Heavy Construction Service or General Contractor, Special Trade Contractor, Consumer and Commercial Repair Service, and Vehicle Repair or Service

All materials and equipment associated with this use shall be entirely screened from view of adjacent properties, and public or private right-of-ways, by the use of solid fencing or appropriate landscaping.

M. Commercial or Industrial Machinery or Equipment, Construction Tools or Equipment, Heavy Duty Truck or Commercial Vehicle Rental or Leasing, Vehicle Rental or Leasing, and Vehicle Sales and Heavy Duty Truck or Commercial Vehicle Dealer; Manufactured Home Dealer

1. Vehicles must be located behind a commercial or office building as viewed from the facing street frontage.
2. In the LC District, Vehicle Sales, Dealers, Rental, and Leasing uses, including all associated vehicle storage areas, must not exceed one acre in size; otherwise, these uses are prohibited.

N. Self-Service Storage Facility

1. Performance Standards

- a. **Street Frontages and Mixed-Use Development**
 - i. In the MU District, a Self-Service Storage Facility shall be part of a mixed-use development where the ground floor use facing street frontages (entire street frontage) are commercial or office space independent of the Self-Service Storage Facility business.
 - ii. In the LC District, a Self-Service Storage Facility shall be either:
 - a) Part of a mixed-use development where the ground floor use facing street frontages (entire street frontage) are commercial or office space independent of the Self-Service Storage Facility business, or
 - b) Part of a mixed-use development where the Self-Service Storage Facility business is contained within a separate building or buildings located behind a commercial or office space business as viewed from the facing street frontage.
 - b. **Front Setback**

All structures, including the accessory manager's office/residence must be set back a minimum of 25 feet from the right-of-way or the district minimum setback, whichever is greater.
 - c. **Side and Rear Buffers/Screening**
 - i. *Office, Commercial or Industrial Zoned Lots.* Where projects abut lots zoned office, commercial, or industrial, no side and rear setbacks are required.
 - ii. *Residential Zoned Lots.* Where sites abut residentially zoned properties, buildings adjacent to the perimeter must face inward with their doors away from such areas.
 - d. **Building Lengths and Access**

To ensure ease of access for emergency vehicles, no building shall exceed 300 feet in length. Spaces between ends of buildings shall be at least 30 feet.
 - e. **Accessory Office/Apartment**

One management office and/or accessory residence shall be permitted.
 - f. **Parking and Circulation**
 - i. *Entrance.* Project entrances shall be 30 feet in width.
 - ii. *Roadway Widths.* Roadway widths on interior drives shall be at least 24 feet in width where buildings face and open onto such drives on only one side. Where buildings face and open onto drives on both sides, widths of such drives shall be at least 34 feet.
 - iii. *Turning Radii.* Turning radii, whether provided at the terminus of interior drives or at points between buildings, shall be at least 30 feet to provide for the maneuverability of emergency vehicles.
 - g. **Signs**

Signs shall comply with the requirements contained in this Article and Chapter 9 of this Ordinance.
2. **Operating Conditions**
 - a. **Commercial Activities**

The manufacture or sale of any commercial commodity or the provision of any service from the premises is prohibited.
 - b. **Commercial Repair Activities**

Commercial repairs of vehicles autos, boats, motors, furniture, or other items on the premises are prohibited.
 - c. **Storage of Flammable Substances**

Storage of flammable chemical substances within the complex is prohibited.
 - d. **Open Storage**

Open storage of automobiles vehicles and boats is permitted only where such areas are screened to comply with Landscaping, Screening and Buffer requirements contained in Chapter 9 of this Ordinance.
- O. **Liquefied Petroleum Gas Dealers**

The amount of storage for Liquid Petroleum Gas Dealers shall be limited to 40,000 gallons per site.
 - P. **Vehicle Storage**

Open storage of vehicles and boats shall be entirely screened from view of adjacent properties, and public or private right-of-ways, by the use of solid fencing or appropriate landscaping.

Q. Microbrewery

1. *Barrel Capacity.* A Microbrewery shall have a maximum capacity of 5,000 barrels per year; otherwise, this use shall comply with the Special Exception procedures contained in this Ordinance.
2. *On-Site Consumption.* A Microbrewery shall require review and approval in accordance with the Special Exception procedures of this Ordinance if they allow on-site consumption of beer or alcoholic beverages in conjunction with the Microbrewery use or an Accessory Use.
3. *Proximity to Residential District or Use.* A Microbrewery shall require review and approval in accordance with the Special Exception procedures of this Ordinance if they are located within 500 feet of the property line of a lot in a residential Zoning District or a lot containing a residential use. Distances shall be measured from the nearest property line of the subject parcel to the nearest property line of a lot containing a residential use or located in a residential Zoning District.
4. *Accessory Uses and Structures.* All Accessory Uses and Structures shall comply with the requirements of Article 6.5 of this Ordinance.
5. *Special Events.* All Special Events uses shall comply with the requirements of Article 6.7 of this Ordinance.
6. *Third Party Vendors.* The Site Plan shall demonstrate that all required parking is maintained and remains unencumbered when third party vendors or structures are onsite.

R. Gasoline Service Stations

Gasoline Service Stations shall have a maximum of four fuel dispensing stations and a maximum of eight vehicle fueling positions (VFP). VFP's are the number of vehicles that can be fueled simultaneously at a gasoline service station.

Sec. 5.18.7 GENERAL DEVELOPMENT STANDARDS AND REQUIREMENTS (ALL DISTRICTS)

A. Residential Density

1. *Maximum Residential Density.* The Density/Intensity and Dimensional Standards listed in Table 5.18-3 of this Article shall apply to all properties in the MU District, and the Density/Intensity and Dimensional Standards listed in Table 5.18-5 of this Article shall apply to all properties in the LC District.
2. *Calculation of Residential Density.* Residential density shall be calculated by dividing the number of Lots/Dwelling Units on a site by the net area (in acres) of highland of the site on which the Lots/Dwelling Units are located. Net highland acres includes all acreage that is not below the Office of Coastal Resource Management Critical Line or identified as freshwater wetlands. Site Plan Review and Subdivision applications shall include all freshwater wetland metes and bounds, and total freshwater wetland acreage based on a United States Army Corps of Engineers (USACE) Approved Jurisdiction Determination (AJD). Accessory Dwelling Units (ADUs) are not included in the calculation of residential density.

B. Vehicle Access. Site Plan Review and Subdivision Plat applications shall include suitable access management plans demonstrating compliance with the requirements below:

1. *Driveway Separation.* For driveways directly accessing Maybank Highway, driveway separation shall be a minimum distance of 150 feet from the intersection of the right-of-way lines to the closest edge of driveways. Where the driveway separation is less than 150 feet the proposed driveway shall be designed as a right-in, right-out (RIRO) driveway and conform to the SCDOT standards for a Typical Right-in, Right-out Driveway Design.
2. *Driveway Width.* Vehicular access from Maybank Highway and side (secondary) streets to properties shall be confined to access drives not exceeding 30 feet in width at the street line. Driveways which include a median strip to separate traffic flow in opposite directions shall not exceed 60 feet in width at the street line.

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
October 12, 2020 Planning Commission Workshop and Meeting

3. *Distance between Driveways.* The distance between the proposed driveway and an existing adjacent driveway shall be the greatest distance feasible.
4. *Limitation to Number of Driveways.* For parcels with a right-of-way frontage equal to or less than 150 feet in length, the development is limited to one driveway, unless single lane driveways are provided in accordance with Sec. 5.18.7.B.5.; for parcels with a frontage greater than 150 feet and less than 300 feet in length, the development is limited to two driveways; for parcels with a frontage equal to or greater than 300 feet the development may have up to three driveways.
5. *Single Lane Driveways.* For parcels with a right-of-way frontage equal to or less than 150 feet in length, two single lane driveways may be allowed. Each single lane drive shall not exceed 15 feet in width at the street line, and the single lane driveway separation shall be a minimum distance of 100 feet from the intersection of the right-of-way lines to the closest edge of driveways.
6. *Corner Lots.* Notwithstanding the above stated requirements, access drives on corner lots shall be located only on the side (secondary) street and such driveways shall be a minimum distance of 50 feet from the street intersection as measured from the edge of the intersecting roadway to the beginning of the driveway radius. However, the Zoning and Planning Director may require the access to be located on the primary street to avoid undue interference with, or hazard to, traffic on the roadways.
7. *Shared Access Requirements.* Shared access is encouraged between adjoining parcels. Driveways for all uses except Single-Family Detached Dwellings should be located in a manner where they can be shared between adjacent parcels as described below:
 - a. The applicant must request a shared access with the adjacent property if the adjacent property does not contain a Single-Family Detached Dwelling.
 - b. Shared access should be located along a common property boundary, if feasible.
 - c. If the owner of the adjacent parcel does not agree to share access, the applicant shall provide one of the following to the Zoning and Planning Department:
 - i. A letter from the adjacent property owner denying access; or
 - ii. If the adjacent property owner refuses to provide a letter, an affidavit that documents attempts that the applicant made to request shared access and that the neighboring property owner refused to provide a letter. If this subsection applies, a new curb cut is permitted on the subject parcel only with a recorded agreement that the property owner will allow adjacent properties to share access when developed and/or redeveloped.
 - d. Shared access agreements shall be recorded with the Register of Deeds (ROD) Office.
 - e. Each parcel involved in a shared access agreement shall be allowed an increase in building cover up to a maximum of 40 percent of the lot.
8. *Transportation Coordination.* Prior to the issuance of a Building Permit, the applicant must show conformance with all requirements included in Letters of Coordination from the South Carolina Department of Transportation (SCDOT), Charleston County Public Works Department, and the Charleston Area Regional Transportation Authority (CARTA).
9. *Infrastructure Connectivity Dedication Incentives.*

The map titled "Johns Island Maybank Highway Corridor Overlay Zoning District Infrastructure and Connectivity" identifies the approximate location of a future connector road and drainage system intended to support improved vehicular circulation and stormwater drainage within the JO-MHC-O District. Property owners may be eligible for certain incentives (as listed below) in exchange for dedicating land, easements, and/or rights-of-way for one or more segments of the planned connector road and drainage system as shown on the map titled "Johns Island Maybank Highway Corridor Overlay Zoning District Infrastructure and Connectivity" to the City of Charleston. These incentives shall only apply upon submittal of documentation that such land, easements, and/or rights-of-way have been dedicated to and accepted by the City of Charleston.

 - a. Properties located in the Mixed Use District shall be allowed the following:

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
October 12, 2020 Planning Commission Workshop and Meeting

- i. Maximum density of 16 Dwelling Units/acre;
 - ii. Maximum building cover of 50%;
 - iii. Maximum building footprint of 20,000 square feet; and
 - iv. Waiver of all required Zoning Permit and approval fees.
- b. Properties located in the Limited Commercial District shall be allowed the following:
- i. Maximum density of 12 Dwelling Units/acre;
 - ii. Maximum building cover of 50%;
 - iii. Maximum building footprint of 10,000 square feet;
 - iv. Waiver of all required Zoning Permit and approval fees; and
 - v. Reduction of the Maybank Highway right-of-way buffer by up to 25 feet. If the right-of-way buffer is reduced, the 25-foot area dedicated to events and stormwater shall be altered accordingly (the first 25 feet shall be dedicated to the multi-use path and the second 25 feet shall be dedicated to treescape/plantings).

C. Traffic Impact Analysis

All Site Plan Review applications for properties within the JO-MHC-O District shall comply with the requirements of Article 9.9, Traffic Impact Studies. Compliance with Art. 9.9, Traffic Impact Studies, is also required for Subdivision Plat applications that meet the thresholds contained in Art. 9.9.

D. Pedestrian Access and Sidewalks/Multi-Use Paths

1. *On-Site Pedestrian Access.* On-site pedestrian access shall comply with the requirements of this Ordinance and shall be included in site design illustrating access linkage to existing sidewalks, adjacent parcels, and within the development area. At-grade and grade-separated pedestrian walkways shall provide direct connections from the street to the main entrance and to abutting properties. Pedestrian walkways shall be designed and located in a manner that does not require pedestrians to walk through parking lots or within driveways.
2. *Maybank Highway Sidewalk and Multi-use Path Requirements.* A minimum 10-foot wide concrete sidewalk shall be required for properties in the Mixed Use (MU) District and a minimum 10-foot concrete multi-use path shall be required for properties in the Limited Commercial (LC) District. Sidewalks and multi-use paths must comply with the requirements listed below and the right-of-way buffer requirements contained in this Article. This Section applies when properties are developed or redeveloped in accordance with Sec. 3.7.1 of this Ordinance.
 - a. The minimum 10-foot wide concrete sidewalk or multi-use path, located as shown in Figures 1 and 2, as applicable, shall extend the length of the entire property frontage;
 - b. Where applicable as determined by the Zoning and Planning Director, asphalt surface material may be used in place of concrete to mitigate damage to existing trees that are to remain;
 - c. The property owner shall obtain and submit all required encroachment permits as part of the Site Plan Review application;
 - d. The property owner shall provide written documentation from the City of Charleston that the City will maintain the sidewalk or multi-use path upon approval of Certificates of Occupancy. This requirement is applicable regardless of whether the sidewalk or multi-use path is located in a right-of-way or within an easement. Such documentation shall be submitted as part of the Site Plan Review application;
 - e. If the sidewalk or multi-use path is located on private property, the property owner shall record an easement for the safe movement of pedestrians and the maintenance of the sidewalk; and
 - f. Sidewalks/multi-use path shall be installed and inspected prior to issuance of Certificate(s) of Occupancy.
3. *All Other Roads Sidewalk Requirements.* Sidewalk requirements for all roads other than Maybank Highway in the Overlay Zoning District shall comply with the sidewalk requirements of Chapter 9 of this Ordinance.

E. Signs

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
October 12, 2020 Planning Commission Workshop and Meeting

All signage must comply with the requirements of this Section in addition to the applicable requirements of Article 9.11, Signs, of this Ordinance.

1. Free-standing Signs

- a. All new freestanding signs are to be designed as monument signs.
 - i. Signs shall have a maximum height of 10 feet and a maximum size of 40 square feet.
 - ii. Shared free standing signs shall be allowed with a maximum height of 12 feet and a maximum size of 60 square feet.
- b. All sign illumination:
 - i. Illuminated signs located adjacent to any residential area shall be controlled so as not to create excessive glare to properties within adjacent residential areas.
 - ii. Electronic Copy Readerboard signs and Billboards are prohibited.
 - iii. No illumination that simulates traffic control devices or emergency vehicles shall be used.
 - iv. All illumination must be from a steady, stationary light source.
 - v. Internal Illumination:
 - a) Internally illuminated signs must be constructed of routed aluminum or similar opaque material so that only letters, numbers, and/or logos are illuminated.
 - b) Signs shall not have light reflecting backgrounds or letters.
 - c) All finishes shall be a matte finish.
 - vi. External Illumination:
 - a) Illumination shall be from a steady stationary light source, shielded and directed solely at the sign.
 - b) Light sources to illuminate signs shall be shielded as to not cause glare hazardous to pedestrians or vehicle drivers or so as to create a nuisance to adjacent properties.
 - c) The intensity of light shall not exceed twenty (20) footcandles at any point on the sign face.
 - d) The color of light sources to illuminate signs shall be white.
 - e) Signs shall not have light-reflecting backgrounds or letters.
- c. Nonconforming Signs
 - i. Any Nonconforming sign located on property within this Overlay Zoning District that was legally permitted on or before [Date of Adoption of this Overlay Zoning District] that does not comply with the standards set forth in this Article must come into compliance with the requirements of this Article prior to the issuance of a Zoning Permit for: (1) a new business on the property; and/or (2) all changes other than re-facing and the required addition of pole covers as described in subsection ii below.
 - ii. A Nonconforming Sign may be re-faced without complying with the requirements of this Article provided a pole cover is added to the existing poles pursuant to the following requirements:
 - a) The pole cover shall be at least one-third the width of the sign cabinet; and
 - b) The pole cover shall be at least one-third the overall height of the pole sign, however the pole cover shall not exceed 8 feet in height.

2. Wall Signs

- a. The maximum size of a wall sign shall be in accordance with Table 9.11.4, Wall/Façade Signs, of this Ordinance;
- b. In new multi-tenant developments, such as shopping centers and office parks, all tenant signs are to be similar in type, color, font size, font style, and method of illumination; and
- c. New tenant signs in existing multi-tenant developments shall be consistent with the type and method of illumination of existing tenant signs.

F. Special Stormwater Requirements

All construction activities occurring on properties within the JO-MHC-O District may be subject to Special Protection Area stormwater design criteria as described in the most recent edition of the *Charleston County Stormwater Program Permitting Standards and Procedures Manual*.

Sec. 5.18.8 MIXED-USE (MU) DISTRICT

The MU District includes JO-MHC-O Overlay Zoning District properties grouped in three noncontiguous mixed-use nodes, as shown on the map titled “Johns Island Maybank Highway Corridor Overlay Zoning District.” These mixed-use nodes currently consist of high and medium intensity commercial and residential uses such as shopping centers, restaurants, pharmacies, multi-family dwellings, and various small retail and professional offices, along with a mix of vacant and undeveloped parcels. Two of the three mixed-use nodes are located at major crossroads that currently serve as established and recognized Johns Island commercial centers.

The MU District is intended for higher intensity commercial development and higher density residential uses than those allowed in the LC District of this JO-MHC-O District. Future development in this district should offer a wide variety of comparison and specialty goods and services to a population greater than the immediate neighborhood, additionally providing convenience goods and services to the surrounding neighborhoods. The MU District accommodates different types of compatible land uses close together in appropriate locations to shorten trips and facilitate alternative modes of transportation such as walking, biking, and public transportation. Mixed-use buildings with retail, service and other uses on the ground floor and residential units above the nonresidential space should be encouraged. The following regulations apply in addition to the requirements of Sec. 5.18.7, General Development Standards and Requirements (All Districts), of this Article to unincorporated parcels within the MU District as indicated on the map titled “Johns Island Maybank Highway Overlay Zoning District”:

A. Buffers

1. *Land Use Buffer.* The land use buffer and landscape requirements of Chapter 9 shall apply.
2. MU District Right-of-Way Buffers
 - a. *Buffer Requirement.* For properties with frontage on Maybank Highway, the right-of-way buffer shown in Figure 1 shall be required when such properties are developed or redeveloped in accordance with Section 3.7.1 of this Ordinance:
 - b. *Buffer Description.* The Maybank Highway right-of-way buffer shall be a minimum of 15 feet in depth, and include the following:
 - i. *Planting Strip Requirements.* A five-foot planting strip including:
 - a. Street trees and additional required plantings meeting the requirements of Table 5.18-2. All required trees and plantings shall be planted prior to the issuance of Certificate(s) of Occupancy; and
 - b. Street lights subject to the following street light requirements:
 - 1) Street lights shall be provided as shown in Figure 1 at locations to be determined by the Zoning and Planning Director, provided that property owners shall only be responsible for the street lights located in front of their properties;
 - 2) All required encroachment permits from the SC Department of Transportation shall be included as part of the Site Plan Review application;
 - 3) Property owners/applicants shall provide documentation stating that they shall be responsible for bearing the costs of the conduit(s) for the required street light(s) and coordinating with Dominion Energy to erect the lights prior to the issuance of Certificate(s) of Occupancy;

- 4) Street lights shall have Octagonal heads as defined by Dominion Energy’s light palette and shall be placed on seventeen-foot (17’) tall Fluted poles. Should these designs/light types no longer be in existence at the time of land development application, the Zoning and Planning Director shall determine the appropriate pedestrian scale fixture to be used;
- 5) Street lights shall be installed prior to issuance of Certificate(s) of Occupancy; and
- 6) The property owner shall provide written documentation that the development/redevelopment will comply with the above stated requirements. Such documentation shall be submitted as part of the Site Plan Review application.

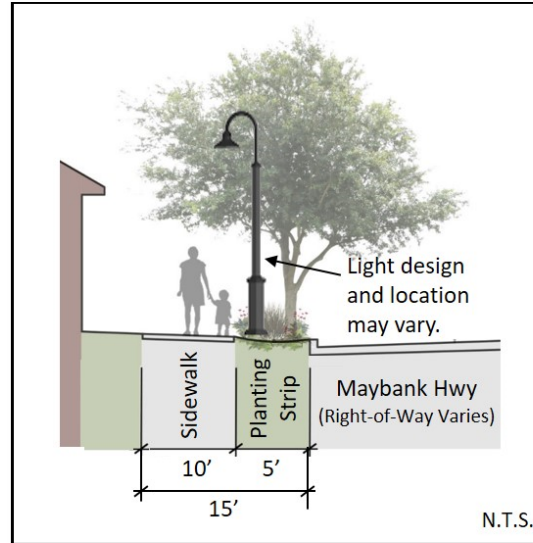


FIGURE 1
 RIGHT-OF-WAY BUFFER, MAYBANK HIGHWAY

- ii. *Sidewalk Requirements.* A 10-foot sidewalk shall be installed subject to the requirements of Sec. 5.18.7.D.2, Sidewalks/Multi-use Paths.

Table 5.18-2, Mixed-Use (MU) District Buffer Depth and Planting Schedule [1]	
STANDARD	MAYBANK HIGHWAY (JOHNS ISLAND)
MIN. BUFFER DEPTH [2] [3]	15 feet [4]
MINIMUM BUFFER LANDSCAPING (Plants per 100 linear feet)[5]	
Canopy Trees[6] [7]	3
Understory Trees (at least 50 percent evergreen)	3
Shrubs	25
Street Trees (may be counted toward canopy tree req.)[8]	2

All trees with a diameter breast height (DBH) of 6 inches or greater within buffers shall be preserved.
 [1] All landscape and plant material shall comply with the minimum standards of Section 9.5.6 of this Ordinance.
 [2] Buffers may be traversed by permitted driveways and pedestrian ways.
 [3] As measured from existing back of curb, where applicable.
 [4] Consisting of a five foot planting strip and ten foot sidewalk as illustrated in Figure 1.
 [5] The Planning Director may waive/modify minimum buffer planting requirements when an undisturbed natural buffer exists that is the same depth and amount of plant material as that which is required.
 [6] Bradford Pears cannot be used to fulfill any of the tree requirements of this Ordinance. Any exotic species which are proposed by the designer are subject to approval of the Planning Director.
 [7] Only Live Oak trees can be used to fulfill canopy tree requirements.
 [8] Street trees are trees planted in rights-of-way for the purpose of fulfilling these requirements. Any planting in rights-of-way must be approved by party(ies) authorized to grant encroachment.

3. If property is dedicated pursuant to Sec. 5.18.7.B.9, the connector road right-of-way buffer shall be eight feet.
4. Right-of-way buffer and sidewalk requirements for all other roads in the MU District shall comply Chapter 9 of this Ordinance.

B. Density, Intensity, Dimensional, and Design Standards. The Density/Intensity and Dimensional Standards contained in Table 5.18-3 shall apply to properties in the MU District:

Table 5.18-3, Mixed-Use (MU) District Density/Intensity and Dimensional Standards	
MAXIMUM RESIDENTIAL DENSITY	12 dwelling units per acre [1] [5]
MINIMUM LOT WIDTH	12 feet
MINIMUM SETBACKS	Equivalent to required buffers [2]
OCRM CRITICAL LINE	50 feet
MAXIMUM BUILDING COVER	30% [3] [5]
MAXIMUM INDIVIDUAL BUILDING FOOTPRINT	15,000 square feet [4] [5]
MAXIMUM BUILDING HEIGHT	5 stories/55 feet
[1] To promote ownership or occupancy of affordable, quality housing, increased densities may be allowed pursuant to Sec. 5.18.6.E. [2] No building setback shall be less than eight feet. [3] Parcels involved in a shared access agreement are allowed a maximum building cover of 40 percent, per Sec. 5.18.7.B, Vehicle Access. [4] No individual building footprint shall exceed 15,000 square feet unless approved pursuant to the Special Exception procedures of this Ordinance. [5] Increased density, building cover, and building footprint may be allowed pursuant to Sec. 5.18.7.B.9, Infrastructure Connectivity Dedication Incentives.	

Sec. 5.18.9 LIMITED COMMERCIAL (LC) DISTRICT

The LC District includes JO-MHC-O Overlay Zoning District properties outside of the mixed-use nodes as shown on the map titled “Johns Island Maybank Highway Corridor Overlay Zoning District.” Existing uses in this district are primarily a mixture of light commercial, and vacant and undeveloped properties surrounded by established residential development.

The LC District is intended for lower intensity commercial development and lower density residential uses than those allowed in the MU District. This district should serve as a transition from the more intense mixed-use node and provide convenience goods and services to the surrounding neighborhoods as well as limited comparison shopping goods for a wider market. The range of comparison goods and services offered should be varied and include specialty retail stores, restaurants, and neighborhood-serving offices. Residential development most appropriate for this district includes small lot single-family dwellings and single-family attached dwellings, such as duplexes and townhouses, and triplexes and fourplexes/quadplexes. The following regulations apply in addition to the requirements of Sec. 5.18.7, General Development Standards and Requirements (All Areas), of this Article to unincorporated parcels within the LC District as indicated on the map titled “Johns island Maybank Highway Overlay Zoning District”:

A. Buffers

1. *Land Use Buffer.* The land use buffer and landscape material requirements of Chapter 9 of this Ordinance shall apply.
2. LC District Right-of-Way Buffers.

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
 October 12, 2020 Planning Commission Workshop and Meeting

a. Maybank Highway LC District Right-of-Way Buffer

- i. *Buffer Requirement.* For properties with frontage on Maybank Highway, the right-of-way buffer shown in Figure 2 shall be required when such properties are developed or redeveloped in accordance with Section 3.7.1 of this Ordinance.
- ii. *Buffer Description.* The Maybank Highway right-of-way buffer shall be 75 feet in depth and include the following:

- 0 – 25 feet from the property line at the right-of-way: a minimum 10-foot concrete multi-use path meeting the requirements of Sec. 5.18.7.D.2 and street lighting meeting the requirements of Sec. 5.18.9.A.3. This area is also reserved for future road widening.
- 25 – 50 feet from the property line at the right-of-way: Street trees and additional required plantings meeting the requirements of Table 5.18-4. All required trees and plantings shall be planted prior to the issuance of Certificate(s) of Occupancy.
- 50 - 75 feet from the property line at the right-of-way: This area is reserved for stormwater infrastructure; however, other temporary activities may occur in this area as approved by the Zoning and Planning Director. When stormwater detention ponds are located within the right-of-way buffer, they are only allowed in this portion of the buffer and they shall not occupy more than 25% of the entire right-of-way buffer area.

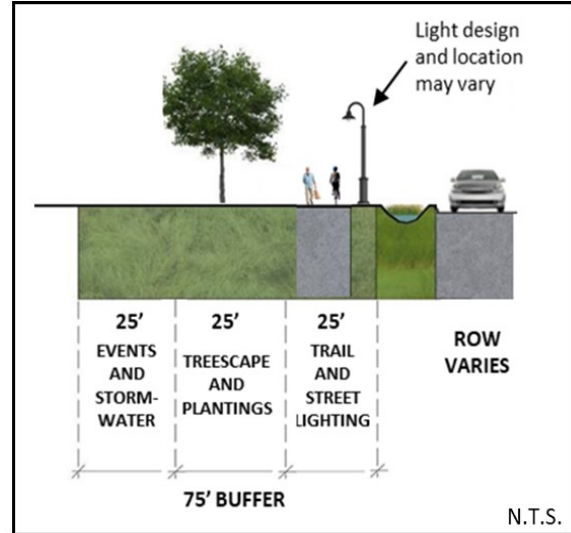


FIGURE 2
 RIGHT-OF-WAY BUFFER, MAYBANK HIGHWAY

Table 5.18-4, Limited Commercial (LC) District ROW Buffer Depth and Planting Schedule [1]	
STANDARD	MAYBANK HIGHWAY (JOHNS ISLAND)
MIN. BUFFER DEPTH [2] [3]	75 feet [4]
MINIMUM BUFFER LANDSCAPING (Plants per 100 linear feet)[5]	
Canopy Trees[6] [7]	3
Understory Trees (at least 50 percent evergreen)	4
Shrubs	30
Street Trees (may be counted toward canopy tree req.)[8]	2
All trees with a diameter breast height (DBH) of 6 inches or greater within buffers shall be preserved.	
[1] All landscape and plant material shall comply with the minimum standards of Section 9.5.6 of this Ordinance.	
[2] Buffers may be traversed by permitted driveways and pedestrian ways.	
[3] As measured from existing back of curb, where applicable.	
[4] Consisting of a minimum 10-foot multi-use path as illustrated in Figure 2.	
[5] The Zoning and Planning Director may waive/modify minimum buffer planting requirements when an undisturbed natural buffer exists that is the same depth and amount of plant material as that which is required.	
[6] Bradford Pears cannot be used to fulfill any of the tree requirements of this Ordinance. Any exotic species which are proposed by the designer are subject to approval of the Zoning and Planning Director.	
[7] Only Live Oak trees can be used to fulfill the canopy tree requirements.	
[8] Street trees are trees planted in rights-of-way for the purpose of fulfilling these requirements. Any planting in rights-of-way must be approved by party(ies) authorized to grant encroachment.	

Draft Johns Island Maybank Highway Corridor Overlay Zoning District
October 12, 2020 Planning Commission Workshop and Meeting

- b. If property is dedicated pursuant to Sec. 5.18.7.B.9, the connector road right-of-way buffer shall be eight feet.
- c. Right-of-way buffer and sidewalk requirements for all other roads in the LC District shall comply with Chapter 9 of this Ordinance.

3. Street Lights

Street Light Requirements. Property owners shall install street lights, as shown in Figure 2, in accordance with the following standards:

- a. Street lights shall be placed in the first 25 feet of the right-of-way buffer at locations to be determined by the Zoning and Planning Director, provided that property owners shall only be responsible for the street lights located in front of their properties;
- b. All required encroachment permits from the SC Department of Transportation shall be included as part of the Site Plan Review application;
- c. Property owners/applicants shall provide documentation stating that they shall be responsible for bearing the costs of the conduit(s) for the required street light(s) and coordinating with electricity provider to erect the lights prior to the issuance of Certificate(s) of Occupancy;
- d. Street lights shall have Octagonal heads as defined by the electricity provider's light palette and shall be placed on 17-foot tall Fluted poles. Should these designs/light types no longer be in existence at the time of land development application, the Zoning and Planning Director shall determine the appropriate pedestrian scale fixture to be used;
- e. Street lights shall be installed prior to issuance of Certificate(s) of Occupancy; and
- f. The property owner shall provide written documentation that the development/redevelopment will comply with the above stated requirements. Such documentation shall be submitted as part of the Site Plan Review application.

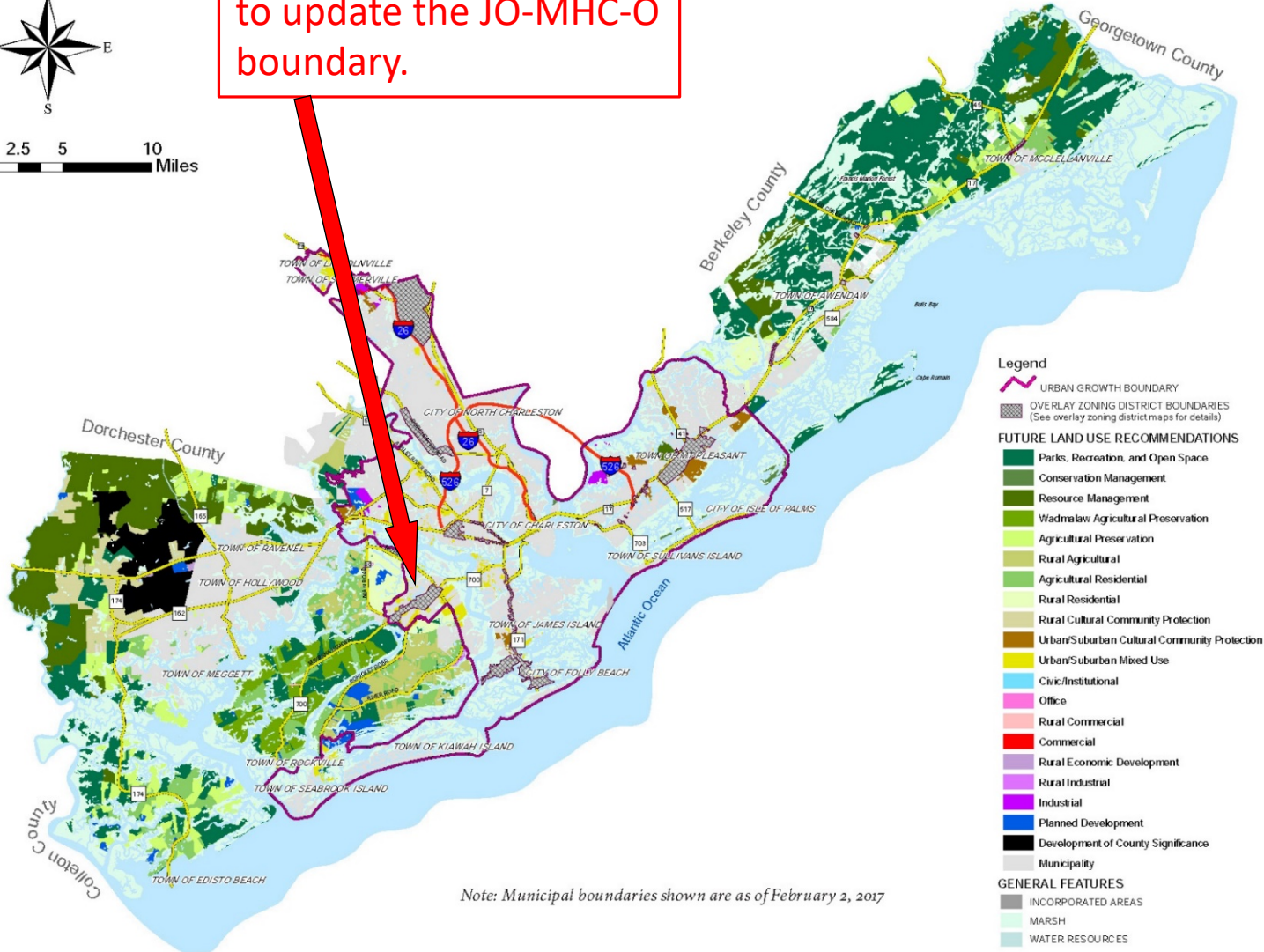
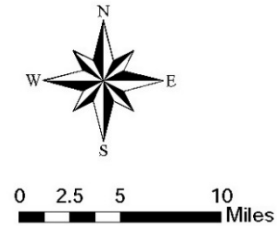
B. Density, Intensity, Dimensional, and Design Standards. The Density/Intensity and Dimensional Standards contained in Table 5.18-5 shall apply to properties in the LC District:

Table 5.18-5, Limited Commercial (LC) District Density/Intensity and Dimensional Standards	
MAXIMUM RESIDENTIAL DENSITY	8 dwelling units per acre [1] [5]
MINIMUM LOT WIDTH	12 feet
MINIMUM SETBACKS	Equivalent to required buffers [2]
OCRM CRITICAL LINE	50 feet
MAXIMUM BUILDING COVER	30% [3] [5]
MAXIMUM INDIVIDUAL BUILDING FOOTPRINT	7,500 square feet [4] [5]
MAXIMUM BUILDING HEIGHT	35 feet and 2-1/2 stories
<p>[1] To promote ownership or occupancy of affordable, quality housing, increased densities may be allowed pursuant to Sec. 5.18.6.E.</p> <p>[2] No building setback shall be less than 8 feet.</p> <p>[3] Parcels involved in a shared access agreement are allowed a maximum building cover of 40 percent, per Sec. 5.18.7.B, Vehicle Access.</p> <p>[4] No individual building footprint shall exceed 7,500 square feet unless approved pursuant to the Special Exception procedures of this Ordinance.</p> <p>[5] Increased density, building cover, and building footprint may be allowed pursuant to Sec. 5.18.7.B.9, Infrastructure Connectivity Dedication Incentives.</p>	

**Johns Island Maybank
Highway Corridor Overlay
Zoning District: Draft
Comprehensive Plan
Amendments**

MAP 3.1.4: FUTURE LAND USE

Update Map 3.1.4:
Future Land Use
to update the JO-MHC-O
boundary.

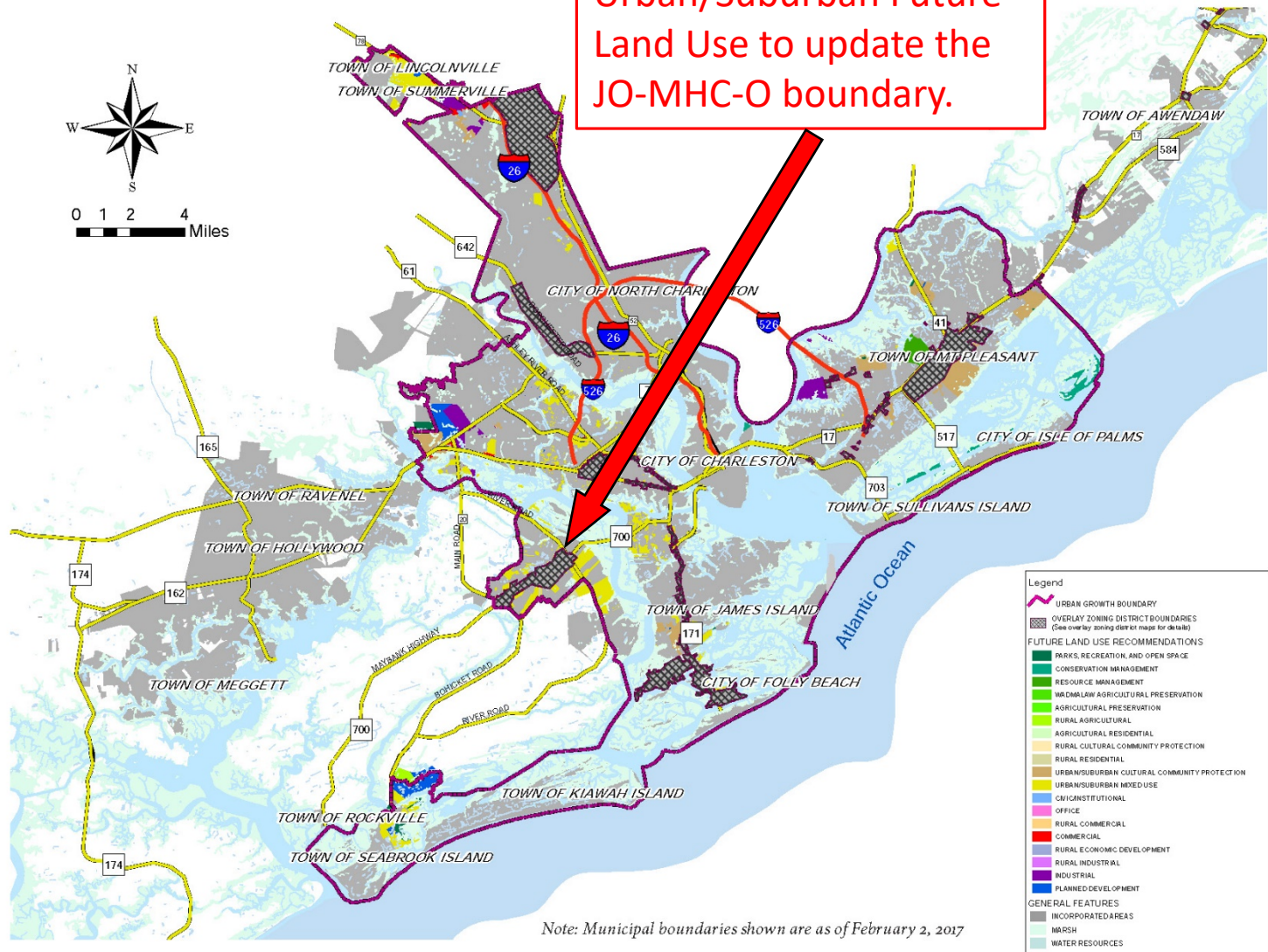


- Legend**
- URBAN GROWTH BOUNDARY
 - OVERLAY ZONING DISTRICT BOUNDARIES (See overlay zoning district maps for details)
- FUTURE LAND USE RECOMMENDATIONS**
- Parks, Recreation, and Open Space
 - Conservation Management
 - Resource Management
 - Wadmalaw Agricultural Preservation
 - Agricultural Preservation
 - Rural Agricultural
 - Agricultural Residential
 - Rural Residential
 - Rural Cultural Community Protection
 - Urban/Suburban Cultural Community Protection
 - Urban/Suburban Mixed Use
 - Civic/Institutional
 - Office
 - Rural Commercial
 - Commercial
 - Rural Economic Development
 - Rural Industrial
 - Industrial
 - Planned Development
 - Development of County Significance
 - Municipality
- GENERAL FEATURES**
- INCORPORATED AREAS
 - MARSH
 - WATER RESOURCES

Note: Municipal boundaries shown are as of February 2, 2017

MAP 3.1.5: URBAN/SUBURBAN FUTURE LAND USE DETAIL

Update Map 3.1.5:
Urban/Suburban Future
Land Use to update the
JO-MHC-O boundary.



Note: Municipal boundaries shown are as of February 2, 2017

Update Map 3.1.8: Johns Island Maybank Highway Corridor Overlay Zoning District

Map: 3.1.8: Johns Island Maybank Highway Corridor Overlay Zoning District

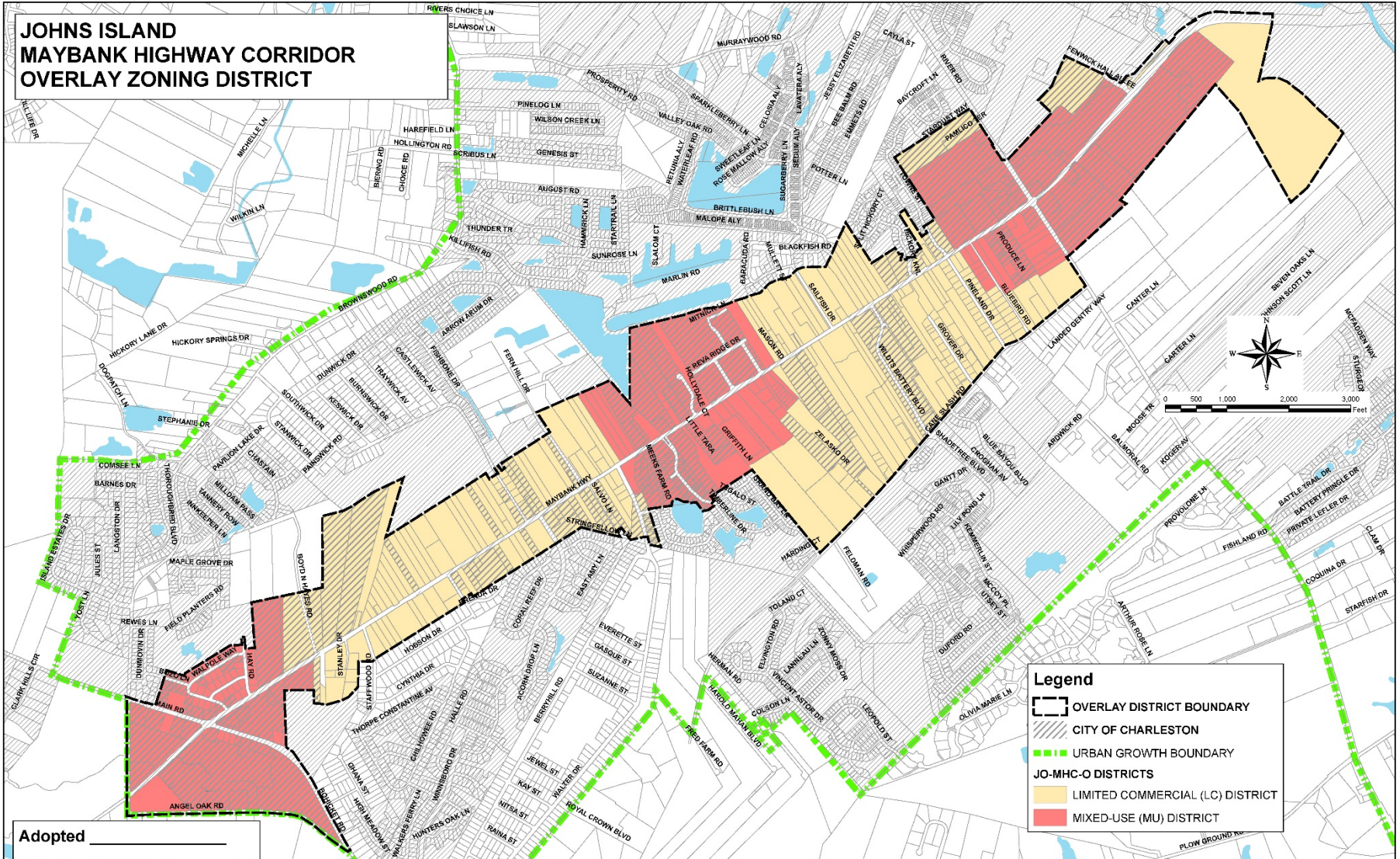


TABLE 3.9.1: PROJECT STATUSES FOR 2013-2014 FIVE-YEAR REVIEW PRIORITY RECOMMENDATIONS

Project	Intergovernmental Coordination	Project Status
Proposed Spring Grove Development (formerly East Edisto)	Towns of Hollywood, Meggett, and Ravenel	Development Agreement, Development of County Significance, and Form-Based Zoning District Adopted Project development plan review will continue through 50-year life of the Development Agreement
Historic Resources Survey Update	State Historic Preservation Office	Completed in 2016
West Ashley Consistency Review	City of Charleston	Completed DuPont Wappoo Community Plan and Overlay Zoning District in 2016; participating in City of Charleston Ashley Master Plan project
Rural Area Cultural Community Protection Overlay Zoning District (Parkers Ferry Area)	Applicable jurisdictions and service providers	Completed in 2017
ReThink Folly Road Implementation	City of Charleston, Town of James Island, City of Folly Beach, and service providers	Plan adopted in 2016; implementation included in 2018-2023 Work Plan
ZLDR Comprehensive Review	All municipalities and service providers within Charleston County	Began in 2017; included in 2018-2023 Work Plan as it is a multi-year project
Urban/Suburban Area Cultural Community Protection Overlay Zoning District	Applicable jurisdictions and service providers	Completed Sol Legare Community Plan and Overlay Zoning District in 2018; other communities included in 2018-2023 Work Plan
Implementation of Urban/Suburban Future Land Use Designation	All municipalities within the Urban Growth Boundary	Included in 2018-2023 Work Plan as part of the ZLDR Comprehensive Review Project
Implementation of the Rural Residential Future Land Use Designation	City of Charleston and Town of Mount Pleasant	Included in 2018-2023 Work Plan as part of the ZLDR Comprehensive Review Project
Maybank Highway Corridor Overlay Zoning District (consistency review with the Johns Island Plan and extension onto James Island)	City of Charleston	Included in 2018-2023 Work Plan
Main Road (River Road to Maybank Highway, including Kitford Road Area)	City of Charleston	Included in 2018-2023 Work Plan
Mount Pleasant Overlay Zoning District - Sweetgrass Basket Stand Special Consideration Area	Town of Mount Pleasant	Included in 2018-2023 Work Plan
Reinforce the location of the Urban Growth Boundary	City of Charleston, City of North Charleston, Town of Mount Pleasant, and relevant service providers	Included in 2018-2023 Work Plan
Voluntary Agricultural and Forestal Areas Ordinance	Applicable jurisdictions	Included in 2018-2023 Work Plan

In Table 3.9.1, update the Project Status column to show "Completed in 2020"

Note: Projects listed above are in addition to staff's daily permitting and application review functions, such as zoning permit review and issuance; review of variance, special exception, appeals, rezoning, planned development, subdivision, and site plan review applications; preparation of staff reports for Board of Zoning Appeals, Planning Commission, and County Council; pre-application meetings with applicants and property owners; code enforcement; and addressing. The County Planning Department also provides similar services to the Towns of Kiawah Island, Meggett, Ravenel, and Rockville.

TABLE 3.9.2: 2018-2023 PRIORITY RECOMMENDATIONS

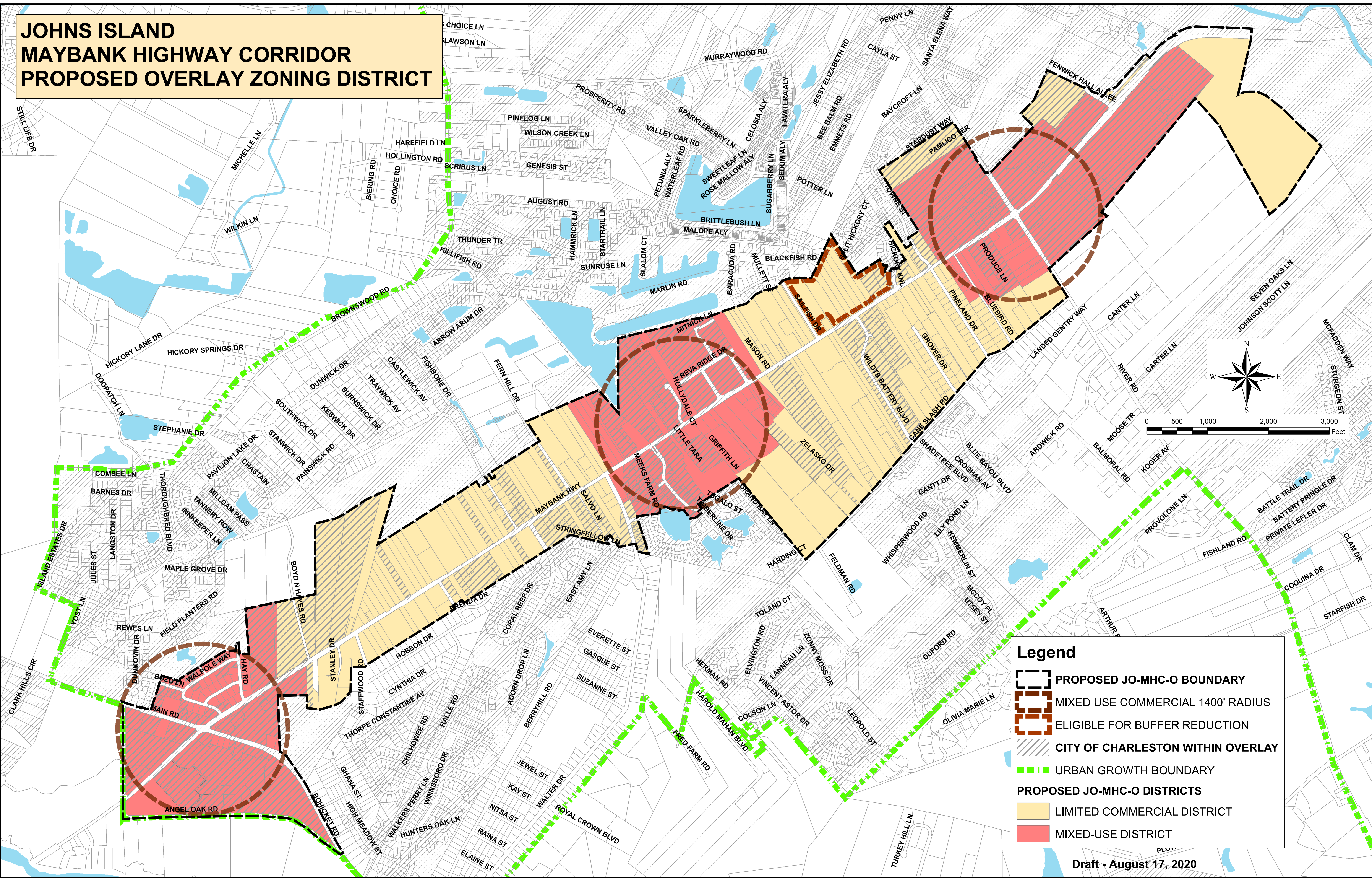
Project		Recommended Time Frame*	Intergovernmental Coordination
1	ZLDR Comprehensive Review	2-3 Years	All municipalities and service providers within Charleston County
2	Maybank Highway Corridor Overlay Zoning District (consistency review with the Johns Island Plan and extension onto James Island) & Main Road (River Road to Maybank Highway, including Kitford Road Area)	1-2 Years	City of Charleston
3	Mount Pleasant Overlay Zoning District - Sweetgrass Basket Stand Special Consideration Area	1-2 Years	Town of Mount Pleasant
4	Urban/Suburban Area Cultural Community Protection Overlay Zoning Districts for Historic African-American Communities, as identified in the 2016 Charleston County Historic Resources Survey Update (e.g. Phillips, Snowden, Ten Mile, Grimball Farm, etc.)	2-3 Years	Applicable jurisdictions
5	Reinforce the location of the Urban Growth Boundary	1-2 Years	City of Charleston, City of Mount Pleasant, and Town of Mount Pleasant
6	Coordinate with Charleston County Economic Development Department to identify economic development opportunities in the Western area of Charleston County	1-2 Years	Towns of Hollywood and Mount Pleasant
6	Future Land Use Consistency Review in Highway 78 Area	2-3 Years	Town of Lincolnville, Berkeley County
7	Community Resiliency Element for Comprehensive Plan	2-3 Years	All municipalities and service providers within Charleston County
8	Voluntary Agricultural and Forestal Areas Ordinance	1-2 Years	Applicable jurisdictions
9	Wadmalaw Island Community Plan	2-3 Years	
10	Edisto Island ZLDR/Comprehensive Plan Amendments	2-3 Years	
11	Implementation of adopted Community Plans (e.g. DuPont Wappoo, Parkers Ferry, Sol Legare)	Ongoing	Applicable jurisdictions
12	ReThink Folly Road Implementation	Ongoing	City of Charleston, Town of James Island, City of Folly Beach, and service providers
13	2016 Charleston County Historic Resources Survey Update Implementation	Ongoing	Applicable jurisdictions and State Historic Preservation Office

Remove the Maybank Highway Corridor Overlay Zoning District project from Table 3.9.2.

*Time frame based on availability of adequate resources, to be directed and reviewed annually by County Council.

**Draft Johns Island
Maybank Highway
Corridor Overlay
Zoning District
Boundary Map**

JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR PROPOSED OVERLAY ZONING DISTRICT



Legend

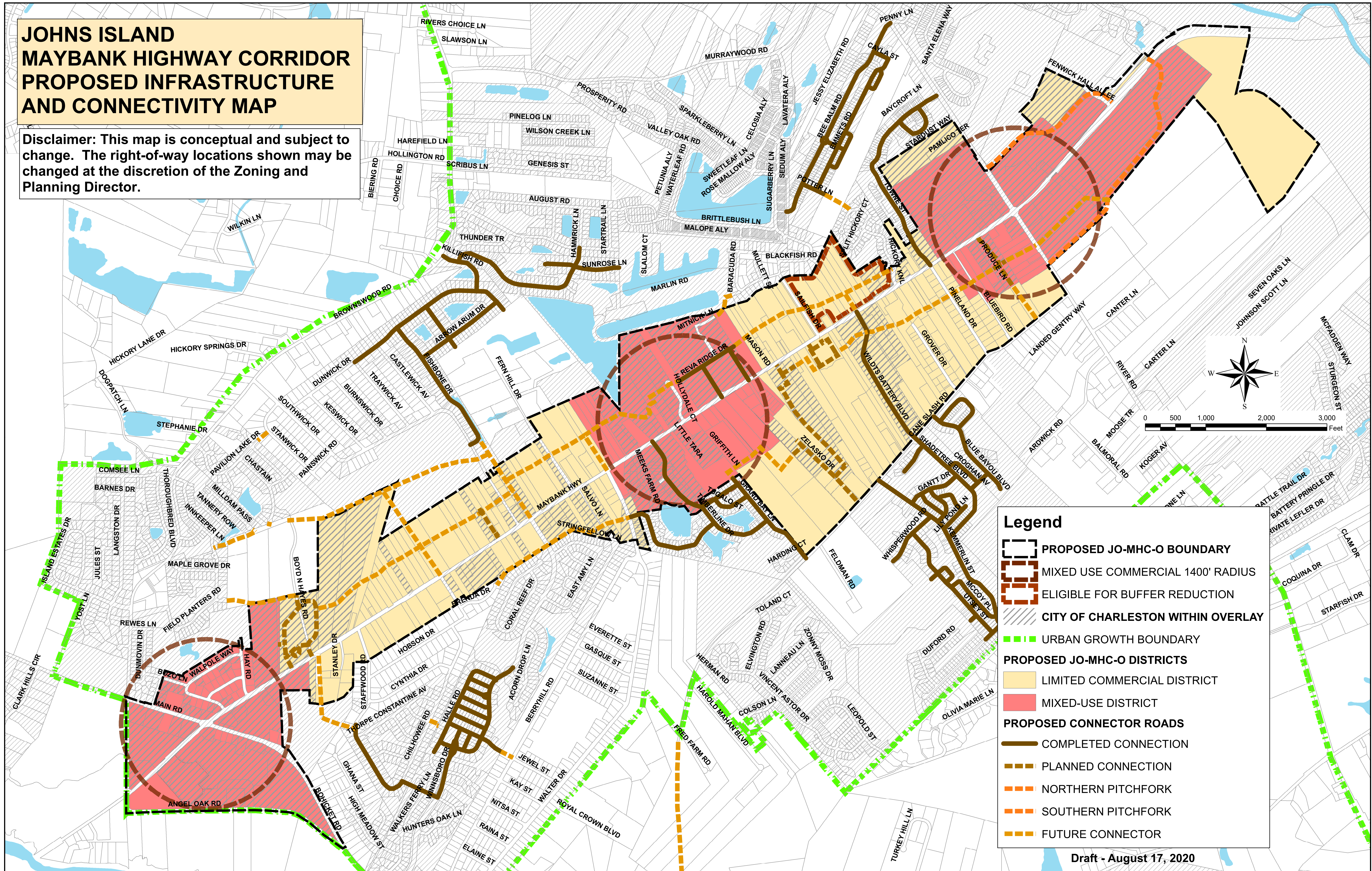
- PROPOSED JO-MHC-O BOUNDARY
- MIXED USE COMMERCIAL 1400' RADIUS
- ELIGIBLE FOR BUFFER REDUCTION
- CITY OF CHARLESTON WITHIN OVERLAY
- URBAN GROWTH BOUNDARY

PROPOSED JO-MHC-O DISTRICTS

- LIMITED COMMERCIAL DISTRICT
- MIXED-USE DISTRICT

JOHNS ISLAND MAYBANK HIGHWAY CORRIDOR PROPOSED INFRASTRUCTURE AND CONNECTIVITY MAP

Disclaimer: This map is conceptual and subject to change. The right-of-way locations shown may be changed at the discretion of the Zoning and Planning Director.



Legend

- PROPOSED JO-MHC-O BOUNDARY
- MIXED USE COMMERCIAL 1400' RADIUS
- ELIGIBLE FOR BUFFER REDUCTION
- CITY OF CHARLESTON WITHIN OVERLAY
- URBAN GROWTH BOUNDARY
- PROPOSED JO-MHC-O DISTRICTS**
 - LIMITED COMMERCIAL DISTRICT
 - MIXED-USE DISTRICT
- PROPOSED CONNECTOR ROADS**
 - COMPLETED CONNECTION
 - PLANNED CONNECTION
 - NORTHERN PITCHFORK
 - SOUTHERN PITCHFORK
 - FUTURE CONNECTOR

**Draft Johns Island
Maybank Highway Corridor
Overlay Zoning District:
ZLDR Related
Amendmentss**

Amend Sec. 8.4.2.A.4.j, as follows:

CHAPTER 8 | SUBDIVISION REGULATIONS

ARTICLE 8.4 PRELIMINARY PLAT

§8.4.2.A.4 Application; Requirements

A. Requirements

The following shall be submitted:

4. The following information shall be required on each plat:

- j. ***United States Army Corps of Engineers Jurisdictional Determination.*** A United States Army Corps of Engineers (USACE) jurisdictional determination is not required for approval of a subdivision plat application except: ***(1) as required for the Main Road Corridor Overlay Zoning District, James Island Maybank Highway Corridor Overlay Zoning District, and Johns Island Maybank Highway Overlay Zoning District; and (2)*** that an Accurate-Approved jurisdictional determination is required for areas located within proposed publicly dedicated rights-of-way and/or easements prior to Preliminary Plat approval. (...)

Amend Sec. 8.9.2.B.9, as follows:

CHAPTER 8 | SUBDIVISION REGULATIONS

ARTICLE 8.5 FINAL PLATS

§8.5.2.B Application;

B. The Final Plat Shall Show the Following:

9. ***United States Army Corps of Engineers Jurisdictional Determination.*** A United States Army Corps of Engineers (USACE) jurisdictional determination is not required for approval of a subdivision plat application except: ***(1) as required for the Main Road Corridor Overlay Zoning District, James Island Maybank Highway Corridor Overlay Zoning District, and Johns Island Maybank Highway Overlay Zoning District; and (2)*** that an Accurate-Approved jurisdictional determination is required for areas located within proposed publicly dedicated rights-of-way and/or easements prior to Final Plat approval. (...)

Amend Sec. 9.5.4, Table 9.5.4.A.3, Buffer Types by Roadway, as follows:

Table 9.5.4.A.3, Buffer Types by Roadway			
ROADWAY	BUFFER TYPE	ROADWAY	
Abbapoola Road	S4	Magwood Road	S3
Ashley Hall Road	S1	Main Road (Limehouse <u>Bridge</u> to Maybank Hwy.)	S5
Hwy. 61/Ashley River Road (Saint Andrews Boulevard to Sam Rittenberg Boulevard)	S1	Main Road (Bees Ferry Road to Limehouse <u>Bridge</u>)	S4
Hwy. 61/Ashley River Road (Sam Rittenberg Boulevard to Mark Clark Expressway)	S2	Manse Road	S4
Hwy. 61/Ashley River Road (Church Creek to Muirfield Parkway/MacLaura Hall Ave.) [1]	S5	Mary Ann Point Road	S3
Hwy. 61/Ashley River Road (Muirfield Parkway/ MacLaura Hall Avenue intersection to Charleston County Line)[1]	S6	Mathis Ferry Road [1]	S4
Bears Bluff Road	S5	Maybank Highway Corridor Overlay District [Johns Island]	[2]
Bees Ferry Road	S4	Maybank Highway Corridor Overlay Zoning District [James Island]	[4]
Belvedere Road	S4	Maybank Hwy (Main Road to Rockville)	S5
Betsy Kerrison Parkway [1]	S5	Meeting <u>Street</u>	S1
Bohicket Road [1]	S5	Murraywood Road	S4
Botany Bay Road	S4	Old Georgetown Road	S4

2. Buffer type as described in the Johns Island Maybank Highway Corridor Overlay Zoning District.

Public Input

From: [Joel Evans](#)
To: [Dan J. Frazier](#); [Andrea Pietras](#)
Subject: FW: Maybank Overlay district
Date: Tuesday, June 18, 2019 10:56:39 AM

From: A V Rawl [mailto:avrawl@aol.com]
Sent: Tuesday, June 18, 2019 10:18 AM
To: Joel Evans <JEvans@charlestoncounty.org>
Subject: Fwd: Maybank Overlay district

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

For your info.
Sent from my iPhone

Begin forwarded message:

From: ggpine@aol.com
Date: June 18, 2019 at 9:01:52 AM EDT
To: avrawl@aol.com, ajohnson@charlestoncounty.org
Subject: Maybank Overlay district

I attended the meeting last night presented by the County and City. It looks like our governments are continuing down the path of making poor decisions. The decision to make Maybank 3 lanes form River rd to the bridge was a poor decision, now they are talking about a lane reversal scheme to fix the 3 lane problem they created....what a disaster. Now they are talking about right in and right out for new development on Maybank. Think about that, you will have to turn right out of an establishment and can not turn left into an establishment, so you have to find a place to turn around then drive back so you can turn right into it. Think how much this will add to the congestion on Maybank.

This city concept of three nodes, with the areas between them at a lower density "town and Country zoning", will not allow Johns Island to have all the much needed commercial services that we so desperately need. Why do we want to restrict commercial development on the only road that has all the infrastructure, Water, sewer, internet and gas, that commercial businesses need to operate.

Why cant we put a turn lane down the middle of Maybank? Why cant we have commercial development along all of Maybank. There is no Country on Maybank its our commercial business district!

It appears to many of us that this attempt to "fix things" will only make things worse, I hope it is not too late to change directions

Thank you for your service

George Gratzick MD

From: [Samuel Furr](#)
To: [Robin Lewis](#); [Holly, Aaron](#)
Cc: [Dan J. Frazier](#); [Anna B. Johnson](#); [Jenny C. Honeycutt](#); [Joel Evans](#); wagnerm@charleston-sc.gov; morganc@charleston-sc.gov; citizenservices@charleston-sc.gov; [Transportation Development](#)
Subject: Maybank Highway Overlay ordinance improvement and comments
Date: Wednesday, November 13, 2019 3:41:06 PM
Attachments: [JI diagram2.pdf](#)

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Dear Ms. Lewis and Mr. Holly,

I thank you and all the County and City staff who have been working hard and doing a good job on the Johns Island Maybank Highway Overlay District. As a owner of multiple properties and stake-holder on Maybank Highway, I have the following comments that I would like to see incorporated into the new Overlay ordinance.

1. Building heights in this urban LC district should allow for at least 3 1/2 stories (M zoning to be 5 1/2) in order to keep the footprints smaller and allow for more traditional, low-country higher ceilings. 35' is much too low for sloped roofs and this traditional language.
2. Do not require right in-and-out until there is a road to support u-turns. (A divided road with median of 40' min.)
3. More mixed use zoning throughout the urban corridor will allow market flexibility and can better support public transit and walkable neighborhoods. This is a proven and very successful urban use. Zone more mixed use areas at walkable intersections near neighborhoods. Perhaps "transit oriented development" every half mile. See attached diagram.
4. A 75' buffer is a suburban, car oriented buffer. A pedestrian friendly buffer should be much smaller to keep the scale of a walkable, family friendly neighborhood and not a super-highway. I do appreciate allowing swales and event areas but the landscape buffer should be incorporated within these and at least 25' eliminated.
5. Allow people to donate or plat right-of-ways without having to build roads and sidewalks to let non-developer and smaller property owners get the ball rolling on the new connector road system. Location of connector roads is good but I, and other smaller property owners, will not be able to pay to put in the road and drainage, however, the presence of these right-of-ways can facilitate this network happening sooner.
6. Regulating that the signs sit below 8' makes them anti-pedestrian friendly and blocks views to the landscaping, business establishments, as well as visibility to the road. They should be allowed to be put above the head-height and sight lines of pedestrians in a more traditional manner. Perhaps the signs could be smaller if elevated.

As a general comment, I believe that many people who have been interested in the Maybank Highway Overlay have been under the impression that it was about improving the public roadway. Many have expressed the idea that it should work for cars, bikes, pedestrians, and

transit as well as be a beautiful, verdant, connector of family neighborhoods. However, the focus has been primarily on zoning the private properties along a very needy highway. If there is any way to address this large discrepancy, it would be very helpful for all the stake-holders of the area.

Hopefully these items can be incorporated. I would be willing to come down and meet, explain, and help if necessary.

Sincerely,

Sam Furr

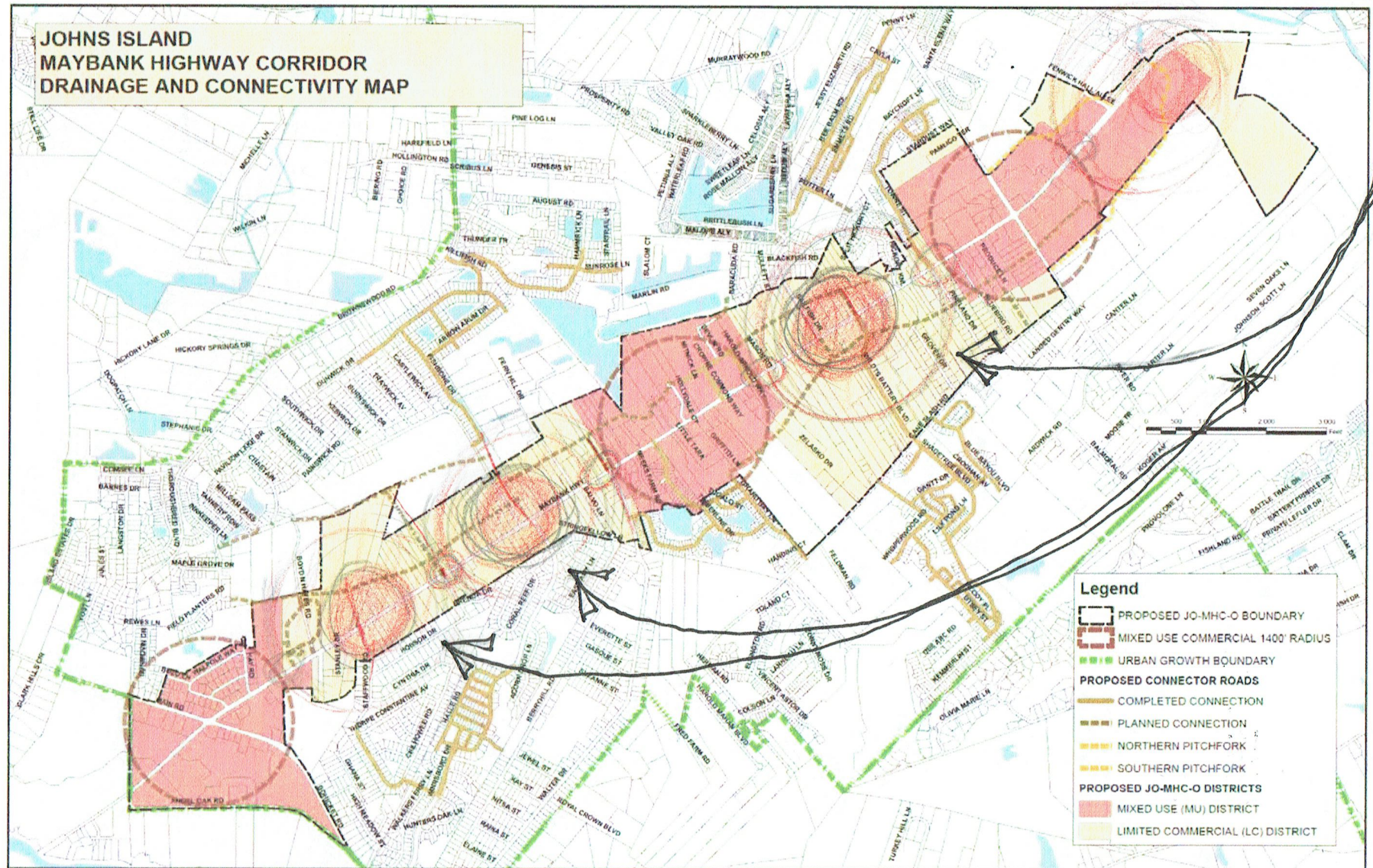
Samuel N. Furr, CNU, NCARB

Samuel Furr Architects
2968 Maybank Highway
Johns Island, SC 29455
843.559.5524

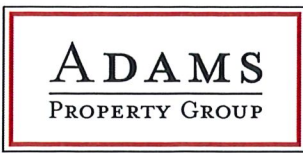
www.samfurrarchitects.com

JO-MHC-O DISTRICT PROPOSED CONNECTOR ROAD SYSTEM

MORE MIXED-USE (HIGHER DENSITY)
NODES AT CATCHMENT LOCATIONS
FOR TRANSIT AND WALKABLE-NEIGHBORHOODS



**Workshop Presentation:
Conceptual Planned
Development (PD) Plan, Oak
Plantation Tract
[TMS 286-00-00-004, -005]
(West Ashley/Johns Island
area)**



AUGUSTA OFFICE
111 SHARTOM DRIVE
AUGUSTA, GA 30907

CHARLESTON OFFICE
2298 MT PLEASANT STREET
CHARLESTON, SC 29403

WWW.ADAMSPROPGROUP.COM

October 5, 2020

Ms. Niki Grimbball
Planning Staff
Charleston County Zoning & Planning Department

RE: Oak Plantation Campground PUD

Dear Ms. Grimbball

Please allow this letter to memorialized Adams Property Group's interest in exploring a potential PUD Zoning designation for the Oak Plantation Compground (Parcels 286000004 and 286000005) in Charleston County, SC. Adams Property Group envisions the property as a Hub/Gathering Spot for the western portion of West Ashley, Johns Island and communities further west on HWY 17. The intent would be to create a live/work/play development featuring the natural live oaks and existing ponds/creeks on the property. We are actively engaged with the County Transportation Department and SCDOT to address suitable access which will be altered by the planned HWY 17 & Main Road realignment project, specifically Alternate 5A as selected, which will be paramount to the viability of the project. The sites natural features, including live oaks, wetlands and critical areas, will be acutely studied with efforts to preserve as much of the natural beauty as possible, while providing adequate drainage to meet and exceed the County's standards. The proposed development program contemplates the following general uses:




- Full Scale Grocery Store: +/- 50,000-60,000 SF depending on prototype selected.
- Traditional Grocery-anchored "Small Shops": +/- 15,000-20,000 SF.
 - Targeted tenants would include: Sandwich Shop, Fedex/UPS store, Beverage Store, Nail Salon, ETC. that you would find in traditional up-scale grocery-anchored centers.
- Retail Village Space centered around the natural features (trees/ponds/canal): +/- 20,000-30,000 SF.
 - Targeted tenants would include: Local Restaurants, Small footprint fitness/yoga, Boutique Retail, Bike Shops, etc
 - See Nexton Village as comparable vision for the Retail Village vision
- Traditional Outparcel Users
 - Targeted tenants would include: Fast Food, Coffee, Dental, Gas, etc.
 - Conceptual Plan is showing 7 outparcels along HWY 17/Main Road, and would possibly include grocery store fueling station. We are showing 1 outparcel on Main Rd/Bees Ferry. Proposed use could be upscale convenience store with fuel.
- Potentially 1 hotel site. Would be similar ALOFT/Hilton Garden Inn type hotel. Est 125-150 rooms
- 2 phases of Multifamily development. Phase I on west side of property, Phase II on Bolton Landing side of creek. Phase II may be townhomes/single family depending on wetlands/demand for housing product
- Current concept plan contemplates 88 townhomes







We look forward to working with Charleston County staff and the surrounding residents to determine the viability of this proposed project. We take great pride in having multiple rounds of feedback sessions with surrounding residential neighborhoods prior to any formal submittal of zoning application. We are committed to listening to the feedback/concerns of residents and trying to properly address and mitigate that feedback through future iterations of the conceptual plan.

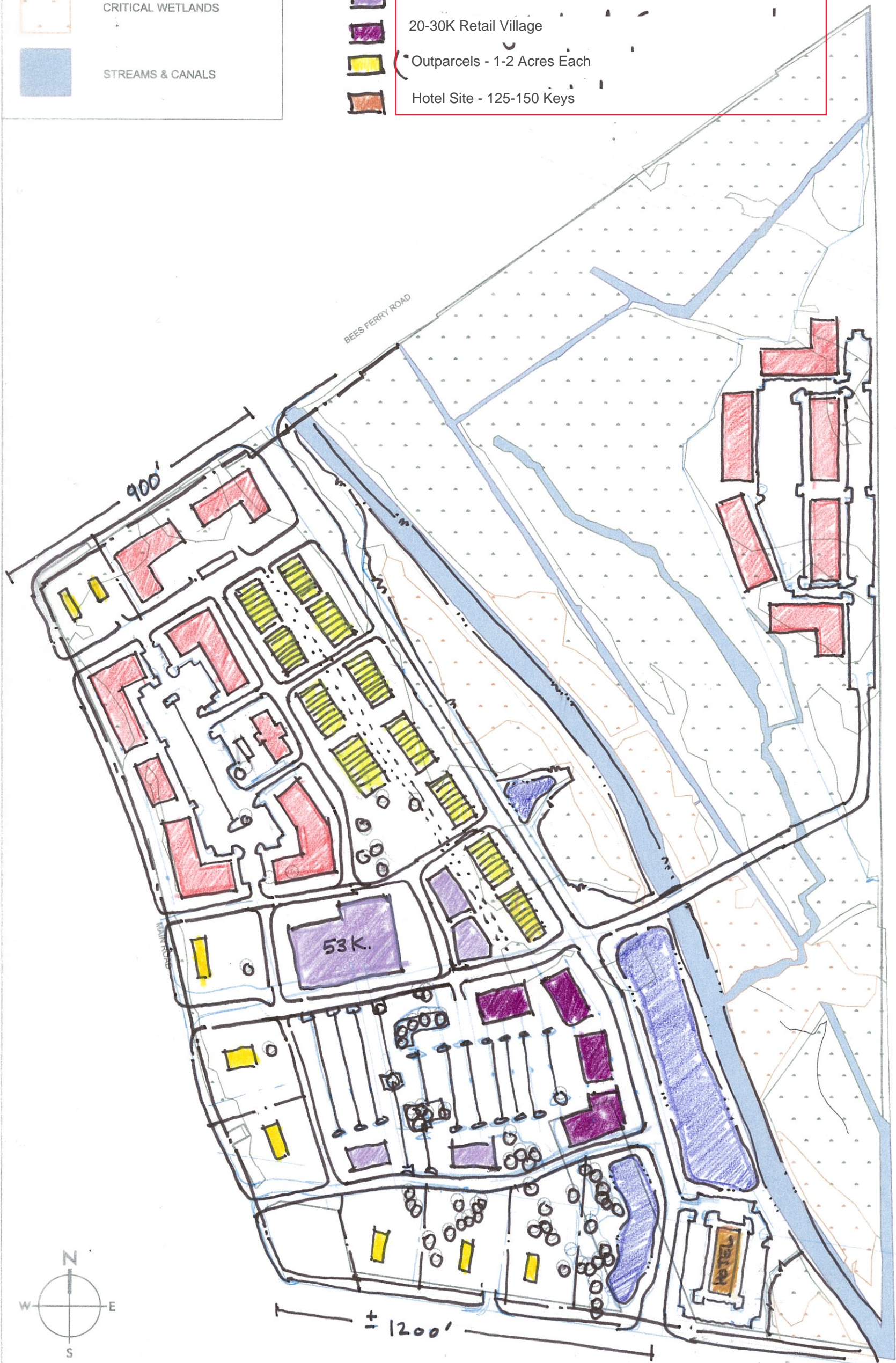
Sincerely,

Jack Coupland
Adams Property Group
843-941-4027
jcoupland@adamspropgroup.com

LEGEND

-  FRESHWATER WETLANDS
-  CRITICAL WETLANDS
-  STREAMS & CANALS

-  Phase I - 350 Apts, Phase II - 250 Apts or single family/towns
-  88 Townhomes
-  Grocery Anchored Retail. 50-60k SF grocery + 15-20k SF retail
-  20-30K Retail Village
-  Outparcels - 1-2 Acres Each
-  Hotel Site - 125-150 Keys



OAK PLANTATION
 PROJECT # 8335
 DATE: 2/6/2020
 SCALE: 1" = 300'

APPROXIMATE WETLANDS
 OVERLAY

Public Input

From: [Gillian McLernon](#)
To: [CCPC](#)
Subject: Oak Plantation Campground
Date: Tuesday, September 29, 2020 8:59:43 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Please, please, please don't allow them to build a new hotel, or 88 townhomes, or 350 apartments!! Why they thought we would welcome that development is beyond me. People of Johns Island have pleaded with the planning dept. to stop development without the infrastructure to support it!

Nothing makes sense anymore!

Gillian McLernon

From: [Pamela](#)
To: [CCPC](#)
Subject: Oak Plantation Campground Development
Date: Tuesday, September 29, 2020 4:45:23 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

To whom it may concern:

I am a resident of West Ashley and homeowner.

I want to let you know I am vehemently opposed to the plan of adding apartments, townhouses, a hotel, and retail to the area currently known as the Oak Plantation Campground.

Traffic in this area is horrendous, completely out of control for the growth and lack of more roads. The flooding is also a huge concern as that area currently floods easily.

We simply cannot have another 1000+ vehicles trying to come/go in an area that is already currently over congested.

I appreciate you taking my opinion into consideration. I would also like the county to think about a moratorium on development in that area rather than expansion.

Thank you,

Pamela Gerthoffer

1594 Bluewater Way

From: [Robert Greenberg](#)
To: [CCPC](#)
Subject: Opposition to Development of Campground Area on Savannah Highway
Date: Tuesday, September 29, 2020 5:57:25 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

I have received notification regarding a Planned Development (PD) consisting of integrated commercial and residential uses at TMS [268-00-00-004](#) and TMS [268-00-00-005](#) (Savannah Highway). I discourage heartily any plan of this sort for increased density of residence or commercial uses of this land without significant effort to accommodate current and increased traffic it would create. The Bees Ferry and Savannah Highway traffic is already congested throughout the day, with no alternative traffic option for residents or tourists passing through. It is time that the planning commission limit development in the area until increased infrastructure for traffic is provided. There are far too many isolated subdivisions that have only one access point to these major roads—thus all traffic must pass on two arteries, Bees Ferry and 17, that are already overflowing. Please do not approve any zoning for this sort of development until an effective traffic plan is put into place. If you do not limit this kind of development, Charleston will soon be known for congestion and long drive times, undoing the positive reputation developed in past years.

Bob Greenberg
1762 Batten Drive
Charleston, 29414

Sent from my iPad

From: [service](#)
To: [CCPC](#)
Cc: vabboyz231@gmail.com
Subject: Plans for Oak Plantation Campground Property
Date: Tuesday, September 29, 2020 4:09:35 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

To whom it may concern,

If the information I received was correct, this property was sold and the buyers are proposing a development with a new hotel, 88 town homes and 350 apartments. If this is true, that would add a minimum of close to 1000 vehicles on the road per day. This area is already a traffic nightmare almost daily. The infrastructure of roads cannot handle this additional traffic.

I oppose this development strongly and will forward to many other local voters and homeowners from West Ashley.

Thank you,

Victor Baumrind
Service Dispatcher

Smoaks Comfort Control
1781 Harmony St
Charleston, S.C. 29407
843-556-9550



Virus-free. www.avg.com

From: [Robert Frederick](#)
To: [CCPC](#)
Subject: Oak plantation campground
Date: Tuesday, September 29, 2020 4:03:03 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

If you think the widening project for Glenn McConnell will accommodate the increased traffic, you are kidding yourselves. The first widening couldn't handle the increased homes along Bees Ferry. And don't tell me it's for people who will work up Dorchester and Summerville. These people will be coming into West Ashley like EVERYONE else.

From: [Mary Grady](#)
To: [CCPC](#)
Subject: Oak Plantation development project (Main Road and Savannah Highway)
Date: Wednesday, September 30, 2020 7:03:02 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

With all due respect, this project needs to be put on a back burner until the issues of traffic and wetlands can be properly addressed. This project is being planned for an intersection that is becoming increasingly dangerous. There have been far too many accidents with fatalities because we are still driving the same intersection that was there when my parents bought a home at Main Road and Morse Avenue in 1967. Nothing has changed relative to the configuration of that intersection, but in the subsequent 53 years, Kiawah and Seabrook Islands have been developed, Johns Island has seen exponential growth, areas on Main Road an back to Bees Ferry Road have been developed and even the communities south of this intersection have seen significant growth. However, in spite of this growth, the county is still attempting to funnel far too many vehicles through this intersection. Now someone is presenting a plan to add 88 townhomes, a 125-room hotel, a shopping center and several hundred apartments and the only road to access this development is on Main Road. That will add an additional 1,000 vehicles a day at that intersection.

For several years, ideas have been floated to improve this intersection, including the construction of a flyover. Should this work be done prior to any further development? Would it be easier to make the necessary changes now before adding the additional traffic? Would it be more cost efficient? Please consider these issues before approving any further development in this area.

Mary Grady
519 Beechcraft Street

From: [Dana Dawson](#)
To: [CCPC](#)
Subject: Oak plantation campground
Date: Wednesday, September 30, 2020 9:40:28 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Good evening,

I am writing this letter in opposition of a PD and the development of Oak plantation campground. The roads surrounding the campground cannot handle the extra housing and commercial plan. As an Islander, I frequently travel by the campground and bees ferry. The past has proven many examples as to why it is more important to focus on fixing the intersection of main and 17 versus adding a few additional homes and businesses. Most recently, when the stand off occurred by the intersection, traffic was backed up for hours as police tried to reroute cars back down 17 or to bees ferry. Furthermore, having lived in Charleston county my entire life and experiencing numerous hurricanes, I do not think the roads can handle traffic even when both lanes are directed away from Johns Island. Also a consideration should be made that the low lands by Gilligan's have already been filled with houses that flood not only in the development, but onto main road. Further development of the area and the introduction of less porous material such as concrete will cause more flooding and problems with standing water. I ask the board to deny the PD and to stop the over development of all areas in Charleston county.

Dana Dawson
Wadmalaw island

[Sent from Yahoo Mail on Android](#)

From: [Tara Saunders](#)
To: [CCPC](#)
Subject: New Plans for Development between Hwy 17 and Bees Ferry
Date: Thursday, October 01, 2020 6:26:27 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Good morning, I am writing in response to the proposed plans to develop the campground between Hwy 17 and Bees Ferry. We the locals understand how lovely our great city is and appreciate some of the changes taking place however we don't agree with all. Almost everyday there seems to be a siting for a new neighborhood or plaza. Which increases revenue into our city but sadly decrease support in our infrastructure resulting in continual traffic overflow, severe flooding and lost habitats for some lowcountry animals. I simply can not stand with this proposal and strongly suggest we put a hold on developments and focus our attention more on problems that have yet to be resolved such as fixing our roads, ensuring that all traffic lights run on adequate times (because some are way too long), land/animal conservation, funding for schools in smaller communities and bringing appropriate developments to help communities thrive such as gas stations, landfills, parks and recreation. Our city is growing but that doesn't mean all development is good. I just ask that we slow down on apartment developments and new suburbs for a while.

Sincerely,

Tara Anderson
Johns Island
Teacher

Sent from my iPhone

From: [Elizabeth Beirne](#)
To: [CCPC](#)
Subject: Oak Plantation Development on HWY 17 & Bees Ferry
Date: Thursday, October 01, 2020 8:16:43 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Dear Charleston County,

Please stop over developing our Lowcountry. By allowing these developments to infiltrate our community you are damaging the livability of this area. The infrastructure of Savannah Highway cannot handle the development of John's Island as it is. With the addition of Oak Plantation Development the area will become a parking lot making the lives of us living in the area miserable. The people moving to Oak Plantation will be commuting into West Ashley adding to the already overcrowding environment.

The county continues to allow new developments without improving the infrastructure. This has to stop.

Elizabeth Beirne

From: [Patricia Shanteau](#)
To: [CCPC](#)
Subject: Proposed development named "Oak Plantation Tract"
Date: Thursday, October 01, 2020 9:49:50 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Hello Planning Commission Members,

I am writing to oppose the current the current plan for the Oak Planation Tract. I am a home owner in a nearby neighborhood, on Morse Ave off Main Road.

First I want to inform the commission that there is a bald eagle nest on the property. This information has been intentionally withheld from the property seller to facilitate the sale of the property. Please be sure to require an ecological survey of this property prior to approving any development.

I believe the density as well as the usage of the proposed development is completely out of line with the area. We most definitely do not need more apartments, houses or townhomes in Charleston. It is Charleston County's plan to allow unattractive tract housing developments on every available piece of land? We moved off Johns Island to get away from uncontrolled housing development, only to be greeted by a current development across the street from our neighborhood, and now this proposal as well. The only type of housing needed in Charleston is affordable housing, not apartments that rent for \$1000+ a month, or houses that sell for \$300,000+. Additionally, we also do not need another Harris Teeter. A Harris Teeter on Bees Ferry, which is less than 3 miles away, opened just this year.

A lesser but still valid critique of this development is the proposed name. I strongly urge you not to allow any proposed development to include the word plantation. This word carries very negative connotations, and in this day and age we are frankly better than the casual use of such a problematic term in the naming of anything.

Thank you for your time.

Sincerely,
Dr. Trisha Shanteau

From: [kristin henderson](#)
To: [CCPC](#)
Subject: Oak Plantation Campground Plans!
Date: Thursday, October 01, 2020 10:17:22 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

We vote NO on these plans!

When is enough development enough?

Plans for the Oak Plantation Campground include 600 apartments, 88 townhomes, a hotel, Harris teeter and out parcels. Our infrastructure cannot handle this. Glen McConnell is already a parking lot!!

NO MORE!

https://www.charlestoncounty.org/departments/zoning-planning/planning-agenda/agenda.pdf?v=192&fbclid=IwAR2i_Shk-B3R3J8RYII6ToRumoVw3LvidOCJwC2Ac2xwD4nX7oSQKw0c7kk

--
null

From: [Kathy Trott](#)
To: [CCPC](#)
Subject: Proposed building for Oak Plantation Campground
Date: Thursday, October 01, 2020 11:12:38 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Please put the brakes on this project and slow down! The last thing a low area needs is more fill and build. Furthermore, the infrastructure (actually, lack of infrastructure) will create more gridlock in this area into the West Ashley area.

Please address infrastructure first and stop fill and build practices.

Kathy Trott
Long time resident

From: [Kimble, Sarah A.](#)
To: [CCPC](#)
Subject: Oak Plantation Campground Development concern
Date: Thursday, October 01, 2020 12:20:24 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Dear Charleston City Planning Commission members,

As a resident of West Ashley for over 15 years, I am very concerned about the recent development plans for the land which used to be the Oak Plantation Campground. I live in the Bolton's Landing neighborhood right next door. My husband and I have been very happy there for over 10 years and have two young children.

Over the last decade, I have definitely seen a large increase in traffic and subsequently, car accidents as a result, particularly at the Main Rd/17 intersection. I am not concerned as much about the development (as it would be nice to have restaurants in the area) as I am about the further increase in traffic that will result. This intersection is already extremely dangerous, and I don't feel safe going through it. I know other neighbors of mine feel the same. Because of that, we then divert to Glenn McConnell, which then becomes further congested and therefore, unsafe. The current transportation infrastructure absolutely cannot handle another large number of apartments and other stores. It is vitally unsafe and unethical to do so until the intersection of Main/17 AS WELL AS Glenn McConnell have been modified to allow for many more additional cars to pass through effectively. For the health and well-being of the communities around this area of West Ashley, please do not allow further development to take place until the safety of our roads are established first. This is paramount, and I fear that if the roads are not made safe first, we, along with many others, would have to move out of the area.

Thank you for your consideration and support,

Sarah Kimble

From: [Hank Simon](#)
To: [CCPC](#)
Subject: Oak Plantation Campground
Date: Thursday, October 01, 2020 2:37:13 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

We do not have the infrastructure, road capacity, traffic signal controls, and flooding alleviation to support our current commuter traffic from 6am to 9:30am, 11am to 2:30pm, and 3:30 pm to 6:00pm. Planned improvements to Glenn McConnell pkwy were proposed to reduce traffic congestion. However, even if the budget is released for development, multiple phased improvements are scheduled to take the rest of this decade, optimistically. We can't support additional development and traffic at this time.

The people who support the plans for the Oak Plantation Campground will profit from this development, but won't suffer from the excessive traffic congestion and degraded quality of life that results. Glenn McConnell Pkwy would have to be expanded to a 10 lane super highway to adequately support the planned development. We need a strong moratorium on this development, until the support infrastructure and fully funded and put into place. Otherwise, what we have is a subsidy for the developers, drained from taxes and lifestyle of the current residents.

Hank Simon

From: [Alissa Ferguson](#)
To: [CCPC](#)
Subject: Plans for the Oak Plantation Campground
Date: Thursday, October 01, 2020 8:59:02 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Please do not allow this development to be built. Our infrastructure cannot handle this. 1. Glen McConnell is already a parking lot!! 2. There needs to be a flyover or something done to fix the Savannah Highway intersection before anything like this is built in that area. As a nearby resident please do not allow this type of development to take over the green space and natural habitats that we have left in West Ashley as well as the livability and road safety of those that live in West Ashley.

Thank you,
Alissa Ferguson
West Ashley resident

From: [Mary Vogel](#)
To: [CCPC](#)
Subject: Oak Plantation Campground rezoning
Date: Friday, October 02, 2020 12:12:23 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

I STRONGLY do NOT support the plans for this area.

West Ashley roads are seriously OVER CAPACITY and until new roads are completed, there should be NO further congestion.

It simply is NOT fair to the residents of West Ashley. Increased building causes more flooding and the traffic congestion is beyond its limits!!

Thank you.
Mary Vogel
2465 Vaocluse Rd
Charleston 29414

From: [Caroline Conrad](#)
To: [CCPC](#)
Subject: Development of campground
Date: Saturday, October 03, 2020 11:09:45 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Good morning,

It came to my attention, only through emails from other residents of Charleston County, about the proposed use of the 150 acres of campground to become an obviously overdeveloped monstrosity called "Oak Plantation".

Given the current economy, trashing further preserved sections of our beautiful low country seems gauche and irresponsible. Time, effort, and money would be better spent on improving infrastructure and addressing the already large issue of flooding. As a veterinarian I am seeing the results of overdevelopment with wildlife being displaced and increasing interactions between pets and public. This use of land would only worsen wildlife displacement and likely increase zoonotic disease rates.

Why can't you just leave a part of our county green and free of overpriced/underutilized housing? Create a park or walking trails. There are apartments sitting empty and housing developments, all over throughout Charleston county, half finished because of the current state in the nation. There are plenty of grocery stores available and convenient to this area. Instead of pushing for more corporate chains of food distribution why not use the land to encourage more farmers markets and local food distribution? Keep the economy and money flow local.

I strongly oppose further development with housing and corporate food chains. Expansionism is finite and this is grossly overstepping the boundaries of acceptable land use. We should be looking to preserve our land and watershed. This is not Myrtle Beach or Jersey Shore or Atlanta. What makes the low country so beautiful is the land.

Please consider the wishes of your current citizens and your responsibility as stewards to Charleston rather than lining your pockets with money from out of state corporations. Money doesn't filter the air and create oxygen, money from developing does not improve the planet for future generations.

--

Carly Conrad, DVM, CVA
Relief Veterinarian
LSU SVM Alumnus 2013
UGA Alumnus 2008

From: [Renee Ettline](#)
To: [CCPC](#)
Subject: Opposition to proposed development of Oak Plantation Campground
Date: Saturday, October 03, 2020 3:23:07 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

In light of the already excessive traffic on Hwy. 17 from the Main Road intersection toward downtown, we are writing you in opposition to the proposed development of Oak Plantation Campground. We understand this plan includes a hotel, 88 town homes, and 350 more apartments. The roads in this area are already seriously overloaded and hazardous without the additional traffic this would create.

During morning rush hour those turning onto Hwy 17 at the Carolina Bay traffic light often turn onto Hwy 17 and immediately enter a traffic jam that extends all the way into downtown. Conversely, in the afternoon rush hour out of town it already takes one hour to make a trip that used to be 15 to 20 minutes max. Additional traffic will create an unreasonable burden on the roads, reduce the quality of life for current residents, and increase safety concerns.

We earnestly request that you do NOT approve the proposed development of Oak Plantation Campground.

Respectfully,
Renee and Fred Ettline
2057 Rookery Lane
Charleston, SC 29414

From: [Barrie Tyler](#)
To: [CCPC](#)
Subject: [***Spam***]
Date: Wednesday, October 07, 2020 10:49:03 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

As a resident of John's Island traffic will not be able to have the added traffic influx from these buildings. Traffic is already a nightmare in the mornings but especially in the afternoons. You sit long periods waiting on turn signals. I know there is a plan to eventually have a flyover but that is many years in the making. There is a Harris Teeter within 3 miles of this planned store on Glen McConnell. Please make this project smaller .

From: [Pamela H Smith Smith](#)
To: [CCPC](#)
Subject: constructon on campground
Date: Wednesday, October 07, 2020 10:13:09 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Please before anything is built do something with the intersection at Main Road. Traffic is so bad, can't take it anymore . The turning lane to Johns Island already blocks one lane of traffic going towards Hollywood.what happened to plans of a fly over?? Please think twice about this!

Pamela Smith
Cloudmont RD
Hollywood, SC

From: [Kristen L. Salisbury](#)
To: [Andrea Melocik](#); [Niki R. Grimball](#); [Joel Evans](#); [Anna C. Kimelblatt](#); [Bill L. Tuten](#)
Subject: Fwd: Objection to Planned Development: Oak Plantation Campground
Date: Wednesday, October 07, 2020 9:30:43 AM

FYI

Get [Outlook for Android](#)

From: Glenn Meadows <gmeadowsctr@gmail.com>
Sent: Wednesday, October 7, 2020 8:40:34 AM
To: Kristen L. Salisbury <KSalisbury@charlestoncounty.org>
Cc: Beautiful Mermaid <mrsmeadows2014@yahoo.com>
Subject: Objection to Planned Development: Oak Plantation Campground

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Ms Salisbury,

Please review for consideration and also forward this email to Mr. Tuten.

07 October 2020

Clerk of Council Office
Lonnie Hamilton, III, Public Services Building
4045 Bridge View Drive
North Charleston, SC 29405

RE: Objection to the Proposed Development of the Oak Plantation Campground, Charleston SC

As a home owner and resident of Bolton's Landing (Charleston, SC), I am writing to express my adamant objection to the proposed development of the Oak Grove Campground, 3540 Savannah Hwy, Charleston, SC 29455, by the Adams Property Group, 2298 Mt Pleasant St, Charleston, SC 29403, phone 843- 941-4001. Concurrent with that, I am asking you to take all necessary steps to stop the future development. At a minimum, I request that you take steps to significantly delay the proposal until relevant federal and state agencies conduct environmental and infrastructure studies to determine the negative impact the development might have on our area.

A planned development of up to 600 apartments, 88 townhomes, a grocery store, and a 125-room hotel will certainly affect traffic, crime rates, emergency response, and quality of life for the residents in the area. As per the 2019 Average Annualized Daily Traffic Counts Map, Area 3 – Southern Charleston County, produced by the S.C. Department of Transportation, traffic along Savannah Highway in a 24-hour period was as high as 54,600 vehicles, with traffic counts as high as 28,700 on Main Road as it intersects Savannah Highway. Bee's Ferry Road saw an annual daily count of 25,500 vehicles, while Glen McConnell Parkway approaching Bee's Ferry recorded an average of 40,300 vehicles. The Charleston County Consolidated 9-1-1 Center reported 314,880 9-1-1 calls for

calendar year 2018; 67,880 EMS/Fire/Rescue incidents; and, 820,529 law enforcement incidents. While this is not specific to the precise area surrounding the proposed development, it does illustrate how busy our emergency services and law enforcement agencies already are. In a report published by the City of Charleston Business & Neighborhood Services Division, dated 11/28/2016, West Ashley accounted for 44.29% of the total population for Charleston, with expected to growth to 81,526 by 2021. With an average family size of 2.87 persons per household, the proposed increase of 688 housing units could add over 1974 residents to the local area. At an average median age of 38, this means additional vehicles on already crowded roadways. The transient nature of a 125-room hotel adds additional traffic and crime related concerns, such as crimes against property (larcenies from vehicle, stolen vehicles, burglaries) and crimes against persons (robberies, sexual assaults, and human/child trafficking and/or exploitation). With Bolton's Landing being in such close proximity to the proposed hotel, easily accessed by foot or car, this is particularly concerning as a husband, parent and retired law enforcement officer.

Of similar serious concern, the development could affect the local watershed and storm water runoff, with potential and subsequent coastal and tidewater impacts. Our area enjoys abundant nature and, so far, minimal development. Bolton's Landing is not currently prone to flooding and homeowners are not required to maintain flood insurance. Storm water runoff/pooling is also minimal. However, developing the Oak Grove Campground enough to support the proposed living and retail units will likely create problems for us. The developers will need to bring fill dirt in to support the expanded growth and this could cause ripple effects in our neighborhood and others. Without environmental and infrastructure studies, no one can accurately predict how bad that will be. According to the U.S. Geological Survey, Fact Sheet 076-03, "Changes in land use associated with urban development affect flooding in many ways. Removing vegetation and soil, grading the land surface, and constructing drainage networks increase runoff from rainfall and snowmelt. As a result, the peak discharge, volume, and frequency of floods increase. Changes during urban development can limit the capacity to convey floodwaters. Roads and buildings constructed in flood-prone areas are exposed to increased flood hazards, including inundation and erosion, as new development continues".

The potential increase in water pollution is another serious concern. According to the EPA, "When rain hits impervious surfaces such as roofs, streets, and parking lots, it flows off in large quantities, carrying pollutants it picks up from the surfaces. The runoff's increased quantity and speed erode stream channels and destabilize their banks, while pollutants harm plants and wildlife in rivers, streams, and bays. Additionally, increased runoff can cause water treatment plants to overflow, releasing untreated sewage into water bodies".

I am sure you can understand my concerns and personal objection to the proposed development. I am also certain that you will find many other Bolton's Landing homeowners who have similar concerns and objections. I look forward to your cooperation and willingness to keep our area safe, beautiful and enjoyable.

Respectfully,

Thomas Glenn Meadows
1462 Seabago Drive
Charleston, SC 29414
540-656-6940

From: [Stephanie Nettis](#)
To: [CCPC](#)
Subject: Oak plantation development
Date: Wednesday, October 07, 2020 8:19:13 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

As a homeowner in Bolton's Landing, I am writing to express my serious concerns regarding the potential development of the Oak Plantation property. Myself and other residents are very distressed with the idea of adding major development to an area that already has significant traffic issues. The intersection of Main Rd and 17 has accidents on a weekly basis due to the high traffic volume. It is already very difficult for residents of Bolton's Landing to exit the neighborhood due to the ever building traffic volume on Highway 17. In addition, as a resident I have major concerns with potential flooding. I have lived in this neighborhood for 6 years. While our neighborhood has not flooded, the area slated for development floods on a frequent basis. After watching what has happened to other neighborhoods in West Ashley, I have to wonder if we will be next. It seems irresponsible to build in an area with known flooding and runoff issues. If my home starts flooding due to this development, who is going to pick up the tab for the damages? Last but not least is the quality of life issue. I bought my home in a quiet, wooded area of town because I enjoy living in a small, close knit neighborhood that is safe for my child navigate by foot or bicycle. This proposed development would allow for cut through streets to a residential area. We do not want additional traffic on our residential streets. These streets were built to support only the homes in our neighborhood and should not be accessible with a cut through from a shopping center or apartment complex. We already have difficulty with people using Bluewater way as a cut through to and from highway 17 and have even had fatal accidents from people cutting through. Adding access roads from development would only exacerbate the problem. There has been a history of poor decision making with regard to unchecked over development in the outer 526 area of West Ashley. Please do not let our neighborhood be the next victim.

Stephanie Nettis

From: [Dale Mack](#)
To: [CCPC](#)
Subject: Proposed Development
Date: Wednesday, October 07, 2020 8:13:31 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

The proposed development for apartments, townhouses and shopping at Main Rd and 17 would create even more problems at this intersection than exist today. The proposed building on this property which already floods each time we get a heavy rain is ill advised. The intersection at main and 17 has long been affected with heavy traffic, backups, and accidents. To build in this area without a complete rework of the road infrastructure would be a traffic nightmare. Take the time and allocate the resources to study and put infrastructure in place before considering new development in this area.

Thank you,

Dale Mack
Hollywood SC

From: [Carolyn Vancleef](#)
To: [CCPC](#)
Subject: Planned Development at Oak Plantation Campground
Date: Wednesday, October 07, 2020 7:57:57 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

I strongly object to the planned development at Oak Plantation Campground. I have lived on Johns Island for 10 years and have steadily seen traffic in this area become more and more of a nightmare because of construction of homes and apartment complexes. There needs to be a moratorium on new construction until traffic issues are addressed....please please!

Sent from my iPhone

From: [Brian DeRosa](#)
To: [CCPC](#)
Subject: Oak Plantation Campground
Date: Tuesday, October 06, 2020 11:26:35 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

If this proposal goes through without the proper road and traffic improvements it will be disastrous more than it already is. Carolina Bay has a proper traffic signal to enter 17, Bolton's Landing does not. The entire intersection at Main Road is a chaotic parking lot on the weekends and during daily rush hours. Surely these problems are of concern to the city?

Brian DeRosa
Bolton's Landing

From: [LAURIE INFINGER](#)
To: [CCPC](#)
Cc: Lorraine@scelp.org; filippo@scelp.org
Subject: PD Oak Plantation Campground
Date: Tuesday, October 06, 2020 10:58:48 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

To the Planning Commission,

I have lived here since 1969 so I've seen massive growth in Charleston County and the surrounding areas. You owe it to the taxpayers of this community and the constituents who elected you to plan and proceed cautiously and with serious consideration of all the impacts it will have. Has there been a traffic study done for this project? An environmental impact study? Any wetlands or flooding impact studies? Will you be applying the appropriate findings of the Dutch Dialogues report to this PD? Will the results of these studies and reports be well advertised to the public in a timely manner?

The development of Oak Plantation Campground, as proposed, will cause severe detriment for the following reasons (not all inclusive):

- This intersection and nearby roadways and businesses already flood. The adjacent boat business fronting Highway 17, part of the campground, and the intersection roadways are already underwater during hurricanes, rains that fall at high tides, etc.
- Road infrastructure in this location is already stressed due to poor pavement condition, too much traffic volume during peak hours, gridlock when there is an accident (and there are plenty). At a minimum, the intersection itself needs to be reworked before any building construction starts, to provide safer commuting, eliminate flooding and facilitate traffic flow.
- Residential density for this small amount of space is WAY too high. Surrounding infrastructure cannot support that number of new residents and the burden they will place in that small amount of space.
- The community does not need another Harris Teeter when there is a brand new one, along with multiple commercial outparcels, only 2.5 miles away and a Publix right across Main Rd. And "OhByTheWay", the Publix parking lot is already a frustrating madhouse due to the existing density of all the existing neighborhoods in the area.
- Are there sufficient schools (with qualified staffing) to support the influx of children this will bring?
- More green space is needed. Couldn't you use most of this for a recreation area with limited building/development? Sure, the WA greenway is right across Hwy 17 off of Main Road, but would YOU want to walk across that intersection to access it? And having people drive through to access it means you would need to build a bigger parking lot, causing more runoff, and increasing the amount of cars on the roads for a ridiculously short trip. Rather oxymoronic, wouldn't you say?

I think most people in the Charleston area realize that development is inevitable. But haven't any commissioners, regulators, developers, etc. learned anything yet from the Long Savannah project? It's likely tied up in appeals for years because the citizens of Charleston County have made it clear that this ridiculous pace of mindless development is not needed or wanted. I get that you want more tax income from all the people this development would bring in, but this "fill and build" mindset will just continue to ruin the quality of life that attracted so many new taxpayers to our beautiful Lowcountry to begin with. ALL our residents, natives and newcomers alike, deserve and expect you as our elected officials to support, sustain and maintain the quality of life in this beautiful area. Please keep these things in the forefront of your planning efforts, and keep the local area, environment, and citizens' quality of life at the heart of your planning.

Thank you very much for your time and consideration of these major issues for the people of the Lowcountry.

Respectfully submitted,
Laurie Infinger
Ravenel

From: [Linda S. Geronilla PHD](#)
To: [CCPC](#)
Subject: New Proposal
Date: Tuesday, October 06, 2020 9:02:35 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

My husband and I are opposed to the new development at the Oak Plantation Campground which Adams Property Group proposes that could include 600 apartments, 88 townhomes, a grocery store, and a 125-room hotel. This will increase flooding and traffic that are already problems that need to be resolved.

--

Linda Geronilla, Ph.D.
726 Sonny Boy Lane,
Johns Island, SC 29455

Dante R. Geronilla
726 Sonny Boy Lane
Johns Island, SC 29455

From: [Ginny Williams](#)
To: [CCPC](#)
Subject: WA development
Date: Tuesday, October 06, 2020 8:41:23 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

I live nearby in West Ashley and as a constituent I would like for you to know that I am against developing the Oak Planation Campground into hotels, grocery stores, townhomes, and apartments. It would cause even more traffic congestion, is not needed (there are 2 nice grocery stores nearby and several hotels already), change the floodplain, and would lower property values.

Virginia M Williams

Sent from my iPhone

From: [Mary Beth Osusky](#)
To: [CCPC](#)
Subject: I oppose New development at oak plantation campground
Date: Tuesday, October 06, 2020 8:17:36 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

The intersection cannot handle the amount of traffic that is already present because you have already allowed too much building for the current infrastructure. The area at Main Rd near highway 17 already floods at heavy rain and at high tides ever since the large complex was built on Main Rd near the river.

Please fix the traffic problem and flood problem that already exists before you compound it even worse by adding hundreds of new units.

You need the infrastructure first. If you cannot fix the problems you already created, please don't make it worse.

Mary Osusky

Sent from my iPhone

From: [MarySue Black](#)
To: [CCPC](#)
Subject: Oaks campground development
Date: Tuesday, October 06, 2020 7:16:08 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

To whom it may concern,

I am a resident of west Ashley and live in Grand Oaks off of Bees Ferry Road. During a typical school year, it takes nearly half an hour to get from my house past the intersection with Glenn McConnell and Wildcat Blvd/Mary Ader. It takes an additional 15 minutes to then get to 526. I am concerned about the amount of traffic that this new proposed development will bring at the current time. We simply do not have the infrastructure in this part of west Ashley to support the existing traffic, let alone an additional 700 apartments. Additionally, I am very concerned about the impact this will have on flooding, as we know that we have had multiple flood events in the past several years. These flood events did not happen until the widening of Bees Ferry Rd. I don't know if there's any hope or stopping this development at this point, but I want it on record that I am extremely disappointed. Our quality of life has decreased because of overdevelopment and lack of appropriate infrastructure.

MarySue Black

Sent from my iPhone

From: [Nancy Walden](#)
To: [CCPC](#)
Subject: Re: Development of Oak Plantation Campground
Date: Tuesday, October 06, 2020 5:45:24 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

I would like to add my voice to the unbelievable development of the campground into apartments, townhomes, etc. This is one of the most idiotic plans I have heard. Hwy 17 is already overburdened by traffic and congestion. Even when the intersection of 17 and Main is fixed, I can't believe the highway can handle this much development. It will certainly add to the flooding problem we have in West Ashley. When is enough enough? Is this another fill and build? Won't somebody think with their head instead of their wallet and end this!

Nancy Walden
713 Banner Road
Charleston, SC 29414

From: [Thuery](#)
To: [CCPC](#)
Subject: New development could bring hundreds of apartments to West Ashley, Johns Island area
Date: Tuesday, October 06, 2020 5:44:31 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Good evening Charleston County Zoning & Planning Department,

This news is disturbing to say the least. More apartments and more houses at the Oak Plantation Camp Ground site will only bring more traffic to already overloaded roads in West Ashley. The roads need to be updated first so they can handle the traffic long before more homes and apartments are built. I urge you to not approve the plans for more development which includes more residential development in West Ashley.

Thank you.

Tim Huey

Concerned Grand Oaks Plantation Resident

This message was sent from a mobile device and may contain undetected spelling and grammatical errors.

From: [David Delli-Gatti](#)
To: [CCPC](#)
Subject: Proposed Development at Oak Plantation Campground
Date: Tuesday, October 06, 2020 3:31:14 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Dear CCPC,

My name is David Ryan Delli-Gatti. I've lived in West Ashley my entire life and drive around the area of Oak Plantation Campground frequently. I don't want to waste the time of whoever is reading this, so I'll be blunt:

Why are you even considering approving the development proposed by Adams Property Group?? You know that NOBODY wants more development in this area. Traffic is already a nightmare. Flooding is already a nightmare. This development will only make both of those worse. If/when this gets approved (Let's be honest. The developers always win, right?), anybody that allows this to happen should be ashamed of themselves. Despite how Adams Property Group may try to sell this, it'll only lower the quality of life in the area.

Listen to the residents, not the developers.

Sincerely,
David

From: [Kristen Young](#)
To: [CCPC](#)
Subject: Oak Plantation Campground Development - Comments
Date: Tuesday, October 06, 2020 10:30:26 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Good Morning:

As a resident of Charleston County, I want to include my comment into consideration regarding the proposed development on Oak Plantation Campground as referenced in [this article](#).

Charleston County Zoning & Planning - please consider the negative impacts that such development will have on the already serious flooding and traffic issues in our area. Natural, undeveloped lands act as a barrier during the inevitable storms we will have. It is fundamentally unethical to propose such a development in an area that already struggles with proper drainage, in addition to removing even more natural habitat for Charleston's unique wildlife.

Charleston County should focus on infill development as much as possible instead of looking to develop the precious natural areas that make Charleston so special. For Councilmember Rawl to suggest that the developers "already have it" also undermines the important process of zoning & planning review, especially given that a cornerstone of his platform is "responsible development".

Thank you,
Kristen Young
10 Planfield Drive, Charleston

From: e.hanson12@yahoo.com
To: [CCPC](#)
Subject: Oak plantation campground
Date: Tuesday, October 06, 2020 9:28:02 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Hello,

I am writing this in protest of the new housing development going in where the campground is now. My family and I live in Boltons Landing and know that we do NOT have the infrastructure for this! Or the schools! We cannot handle that many more cars on the road. Traffic is already terrible! This is a terrible idea for this already busy area.

Elizabeth Rizzo

Sent from my iPhone

From: [Jonathan Bazzle](#)
To: [CCPC](#)
Cc: ["Rebecca Bazzle"](#)
Subject: NO Development of Live Oak Campground near main road
Date: Tuesday, October 06, 2020 8:33:34 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Sirs

I am writing to let you know my disapproval of this development. Charleston can no longer sustain this type of development. This is a disaster waiting to happen

- The eco system is not able to support the water runoff. Tecklenburg's drain system is creating a flood disaster for anyone with a house near a creek. We are already being re zoned into an X flood zone to an AE because of his run off re direction
- The roads cannot handle the traffic. Main road is a disaster and it spills out onto 17 in both directions in the morning and afternoon. We are nt LA it shouldn't take an hour to go places in Charleston.
- Our culture cannot maintain this influx of people. The very framework of what makes Charleston an enticing place to live is what is destroying it. The small city, friendly nature, and know your neighbor feel is being left far behind as apartments are shoe horned into any corner of space that can be found.

Stop the madness. Stop this development.

Jonathan Bazzle

SVC Manager - Sales and Service
Helping the World Keep Promises.®



Office: (843) 856-7224

Mobile: (843) 469-7679

Fax: (843) 856-4208

Email: Jonathan.Bazzle@odfl.com

Old Dominion Freight Line, Inc.
1938 Clements Ferry Road
Wando, SC 29492



[odfl.com](#) [LinkedIn](#) [Facebook](#) [Twitter](#)

CONFIDENTIALITY NOTICE: The information contained in this message may be confidential, privileged, proprietary, or otherwise legally exempt from disclosure. If the reader of this message is not the intended recipient, or an employee or agent responsible for delivering this message to the intended recipient, you are hereby notified that you are not authorized to read, print, retain, copy or disseminate this message, any part of it, or any attachments. If you have received this message in error, please delete this message and any attachments from your system without reading the content and notify the sender immediately of the inadvertent transmission. Thank you for your cooperation.

From: [Kristi Frame](#)
To: [CCPC](#)
Subject: Proposed development off 17 in West Ashley near Oak Creek Campground
Date: Tuesday, October 06, 2020 8:31:22 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

To Whom it May Concern,

As a resident of the Hunt Club neighborhood and West Ashley resident, I strongly encourage you to rethink any further development around this area. The proposed apartments, hotel, etc off Hwy 17 near the Oak Creek Campground is ridiculous and completely unneeded. This area is so congested as is that I can't even drive out of my neighborhood to 17 without going around the longer way out Bees Ferry and around. You are making this area miserable for residents as its already too full of people without the proper infrastructure to support it.

I am 100% against any further development in this area and will do whatever I can to voice my opinion everywhere and anywhere I can – as will my neighbors in Hunt Club, Grand Oaks and Shadowmoss. STOP bringing in more apartments and other useless things. Our neighborhood is fine as is.

From: [Ryan Davis](#)
To: [CCPC](#)
Subject: New apartments near main/Maybank
Date: Tuesday, October 06, 2020 7:43:43 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Why??? Good luck turning left to get into that complex. Good luck turning left to get out of the complex. That will only make getting into Johns island that much more impossible. If approved every single one of you should be required to wait in that hour long line everyday for a year. Where are these people going to work? There are not many jobs left in Charleston, so when there is the inevitable collapse of the Charleston market, we will have this lovely empty eyesore. QUIT BUILDING HOUSING AND BUILD COMMUNITY SPACE.

J. Ryan Davis, MS, ATC
MUSC Orthopaedics
AT in Foot & Ankle Physician Practice

From: [chuck Weatherford](#)
To: [CCPC](#)
Subject: Oak plantation development
Date: Tuesday, October 06, 2020 12:00:57 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

I'm very much in disagreement on this project. There's already a great deal of flooding in area this would only make it worse. Then there's the traffic on 17 where you can't hardly get through because of the one lane turning to main road. On average you would possibly add an additional 2000 vehicles or more. This area should be managed better for flooding and traffic that's already a problem !!! This would be detrimental to the area. This is ridiculous to propose or approve something like this until these current problems can be fixed. I would like some feedback on this please.

Sent from my iPhone

From: [Susan Nial](#)
To: [CCPC](#)
Subject: CONCEPTUAL PLANNED DEVELOPMENT (PD) PLAN, OAK PLANTATION TRACT [TMS 286-00-00-004, -005] (West Ashley/ Johns Island area).
Date: Monday, October 05, 2020 11:29:13 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

To the Members of the Commission:

I oppose the approval of the above proposal. Developers always claim that their proposals are “thoughtful” or that their proposal won’t do anything but benefit the community and surrounding neighborhood. Unfortunately for those of us who live in Charleston and the West Ashley area, in particular, once those proposals are approved the “thoughtfulness“ goes out the window profit takes over and we pay the price.

No permits or approvals should be granted to any new building project that proposes the destruction of any part of our natural water courses or defenses against flooding. We don’t need any more developments that will add to the flooding problems we already have.

I urge you to reject the CONCEPTUAL PLANNED DEVELOPMENT (PD) PLAN, OAK PLANTATION TRACT [TMS 286-00-00-004, -005] (West Ashley/ Johns Island area).

Thank you.

Susan Nial
Sent from my iPad

From: [Kisa Hammet](#)
To: [CCPC](#)
Subject: Oak plantation development
Date: Monday, October 05, 2020 10:53:08 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

This development idea ,is a bad idea ,for the simple reason that any more development in that area will impact the marsh and cause flooding as we have already seen. Traffic is already heavy from building on Johns Island and there not even finished many projects (they)have started on Johns Island. We already need more schools. This idea needs to go away.

Sent from my iPhone

From: [Sean Nolan](#)
To: [CCPC](#)
Subject: Oak Plantation Dvelopment
Date: Monday, October 05, 2020 10:14:13 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Hello,

This development is very concerning. The intersection at main and HWY 17 is already extremely dangerous because of the amount of vehicles as well as the erratic driving to avoid traffic. Adding more persons/vehicles does not solve the problem that is already at hand, which is simply infrastructure. Why are problems not being resolved before more problems arise? This development will only cause more problems unless the original ones are resolved. Including traffic accidents and deaths.

Not to mention the flooding that is already concerning in that same area.

Great job Charleston leaders! Just talk about one issue and provide no action and then move on to a development to bring in that cash! Really looking out for the citizens!

Johns Island resident,
Sean Nolan

From: [Susan Rowland](#)
To: [CCPC](#)
Subject: Oaks Plantation Campground
Date: Monday, October 05, 2020 4:26:55 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Please do not develop this acreage! Glen McConnell as well as Savannah Highway are past the saturation point. Traffic is horrendous daily and we do NOT need more development making the already bad situation worse. Please vote against this ridiculous development.

Sent from my iPhone

From: [Gwen Petty](#)
To: [CCPC](#)
Subject: Public comments on Plans for Oak Plantation Campground
Date: Wednesday, October 07, 2020 11:15:33 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

To whom it may concern:

I along with all of my Bolton's Landing neighbors are seriously concerned about your proposed development for the Campground Property.

For me, and others on my street, (Bimini) this will mean there will be an apartment complex positioned basically right in front of the corner where my house sits at this point. This also means that traffic from a very busy road (hwy 17) will then have direct access straight through my neighborhood from yet another angle. Already the cut throughs of folks from 17 to Bees Ferry are unregulated and dangerous. Also, the noise level from hwy 17 even with the buffer behind the campground is already very loud. This plan will eliminate any buffer whatsoever. It will make being in this section of the neighborhood basically miserable at night due to the road noise.

The second reason is that you will be building on a flood plain by filling it in. Who even does this anymore in Charleston anyway? We already have a very serious flooding problem in West Ashley. Those of us on Bimini and Seabago are just at the edge of safety from flooding due to the Campground buffer. Once you fill in and build in that area, there will be no barrier at all to storm surge. This will increase everyone's flooding risk. Again making the livability of this neighborhood substantially less.

The third reason, should be the most obvious but never seems to be a consideration for any developer. We have a very dangerous intersection at Main Road and Hwy 17. It is always absolute gridlock and so many bad accidents occur there due to the high volume of traffic moving through. If you bring a multi use site to that area that includes a store, a hotel and a massive apartment complex you have basically ruined the plan on any kind of traffic solution for our area. We have to figure out the infrastructure of the roads as they are, before any other development is completed.

You say that your vision is to create a hub/gathering spot for the communities but it will actually be at the expense of our neighborhood, and the safety of our surrounding community due to increased flood risk and serious traffic complications.

Please consider other options that would actually add to the value of the area.

Thank you.

Gwen Petty

From: [Ashley West](#)
To: [CCPC](#)
Subject: Opposition to proposed development at 3550 Savannah Hwy
Date: Wednesday, October 07, 2020 11:50:01 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Regarding a Planned Development (PD) consisting of integrated commercial and residential uses at TMS [286-00-00-004](#) and [286-00-00-005](#) ([3550 Savannah Highway](#))

As a homeowner in the Hunt Club, I object to the current proposal. I believe it will create more flooding issues, congestion, and severely impact quality of life for current residents in the area. I would be accepting of single family homes at 4 per acre max in that area. But allowing a developer to put in more apartments etc will lead to disastrous consequences for all. There is already severe congestion in the area and it cannot handle many more residents if any at all. We also must consider the overcrowding of public schools as well. There has to be a limit of the number of housing units.

Thank you for allowing current residents to provide input.

Ashley West Beaman

Sent from my iPad

From: [Bill Zakrzewski](#)
To: [CCPC](#)
Subject: TMS 286-00-00-004 and 286-00-00-005 (3550 Savannah Highway)
Date: Wednesday, October 07, 2020 12:46:33 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

We are **opposed** to this proposed development.

Regarding a Planned Development (PD) consisting of integrated commercial and residential uses at TMS 286-00-00-004 and 286-00-00-005 (3550 Savannah Highway).

Highway 17 and Main Road are too congested due to the existing homes and businesses in the area. My wife, Stacie, drives home from the Citadel mall and sits an extended period of time at that light. Johns Island development is already out of control and now you want to build apartments, townhouses and hotels in an overly congested area.

I own an investment property in Hunt Club and live on Johns Island so this traffic nightmare will impact me twice as much by reducing both my property values. I am opposed to high occupancy housing in this area as we have plenty of apartments and townhomes already in the area. There are NO attractions in the area that justify a hotel in this area. I would not be opposed to a development of single-family homes as that will limit the number of additional vehicles.

Please do NOT approve this proposed planned development!!

William and Stacie Zakrzewski
205 Old Hickory Crossing
Johns Island, SC 29455
and 840 Bibury Court

From: [Kirk Mortimer](#)
To: [CCPC](#)
Subject: Oak campground
Date: Wednesday, October 07, 2020 4:39:07 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Please do not approve this development not enough infrastructure now.

From: [Jennifer Kliner](#)
To: [CCPC](#)
Subject: Proposed Development @3350 Savannah Hwy
Date: Wednesday, October 07, 2020 4:56:52 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

To the Planning Commission:

As I will be out of town on business and unable to attend the public meeting regarding the proposed development please consider this my formal statement.

I find it irresponsible for the Commission to consider a request of this density on that site. The current road infrastructure is already insufficient to handle the current travel load without severe delays and frequent, often serious, accidents. As this would presumably fall within St. Andrews Public Service District you would be putting additional strain on a very small fire department. There is also the issue of school overcrowding, which already exists. And I find little to no justification for the addition of another Harris Teeter when there is one less than 2 miles away and a Publix immediately across the street. I am not against development. I am against developments of this size and scope who then leave the current residents to deal with the ongoing issues. If the property is to be developed, do it under the density allowed by current zoning of no more than 4 homes per acre. The will still be an additional 220 homes in the area. And if the developer's argument is that people need rental properties I would personally challenge them to try to secure a lease on the median income of a Charleston resident. Because she I needed to rent not a single apartment complex I reached out to in this are of West Ashley would take me due to my income. As for hotels, there are plenty down Savannah Hwy less than 10 minutes away.

This is simply an unsustainable plan and I sincerely urge you not to allow it to move forward.

Sincerely,

Jennifer Kliner
jkliner12@gmail.com
Homeowner
851 Bibury Ct
Charleston, SC 29414

From: [Bailey Westmoreland](#)
To: [CCPC](#)
Subject: Proposed building objection
Date: Wednesday, October 07, 2020 7:57:55 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

07 October 2020

RE: Objection to the Proposed Development of the Oak Plantation Campground, Charleston SC

To whom this may concern,

As a home owner and resident of Bolton's Landing (Charleston, SC), I am writing to express my adamant objection to the proposed development of the Oak Grove Campground, 3540 Savannah Hwy, Charleston, SC 29455, by the Adams Property Group, 2298 Mt Pleasant St, Charleston, SC 29403, phone 843- 941-4001. Concurrent with that, I am asking you to take all necessary steps to stop the future development. At a minimum, I request that you take steps to significantly delay the proposal until relevant federal and state agencies conduct environmental and infrastructure studies to determine the negative impact the development might have on our area.

A planned development of up to 600 apartments, 88 townhomes, a grocery store, and a 125-room hotel will certainly affect traffic, crime rates, emergency response, and quality of life for the residents in the area. As per the 2019 Average Annualized Daily Traffic Counts Map, Area 3 – Southern Charleston County, produced by the S.C. Department of Transportation, traffic along Savannah Highway in a 24-hour period was as high as 54,600 vehicles, with traffic counts as high as 28,700 on Main Road as it intersects Savannah Highway. Bee's Ferry Road saw an annual daily count of 25,500 vehicles, while Glen McConnell Parkway approaching Bee's Ferry recorded an average of 40,300 vehicles. The Charleston County Consolidated 9-1-1 Center reported 314,880 9-1-1 calls for calendar year 2018; 67,880 EMS/Fire/Rescue incidents; and, 820,529 law enforcement incidents. While this is not specific to the precise area surrounding the proposed development, it does illustrate how busy our emergency services and law enforcement agencies already are. In a report published by the City of Charleston Business & Neighborhood Services Division, dated 11/28/2016, West Ashley accounted for 44.29% of the total population for Charleston, with expected to growth to 81,526 by 2021. With an average family size of 2.87 persons per household, the proposed increase of 688 housing units could add over 1974 residents to the local area. At an average median age of 38, this means additional vehicles on already crowded roadways. The transient nature of a 125-room hotel adds additional traffic and crime related concerns, such as crimes against property (larcenies from vehicle, stolen vehicles, burglaries) and crimes against persons (robberies, sexual assaults, and human/child trafficking and/or exploitation). With Bolton's Landing being in such close proximity to the proposed hotel, easily accessed by foot or car, this is particularly concerning as a stay-at-home mom, to two small children.

Of similar serious concern, the development could affect the local watershed and storm water runoff, with potential and subsequent coastal and tidewater impacts. Our area enjoys abundant nature and, so far, minimal development. Bolton's Landing is not currently prone to flooding and homeowners are not required to maintain flood insurance. Storm water runoff/pooling is also minimal. However, developing the Oak Grove Campground enough to support the proposed living and retail units will likely create problems for us. The developers will need to bring fill dirt in to support the expanded growth and this could cause ripple effects in our neighborhood and others. Without environmental and infrastructure studies, no one can accurately predict how bad that will be. According to the U.S. Geological Survey, Fact Sheet 076-03, "Changes in land use associated with urban development affect flooding in many ways. Removing vegetation and soil, grading the land surface, and constructing drainage networks increase runoff from rainfall and snowmelt. As a result, the peak discharge, volume, and frequency of floods increase. Changes during urban development can limit the capacity to convey floodwaters. Roads and buildings constructed in flood-prone areas are exposed to increased flood hazards, including inundation and erosion, as new development continues".

The potential increase in water pollution is another serious concern. According to the EPA, "When rain hits

impervious surfaces such as roofs, streets, and parking lots, it flows off in large quantities, carrying pollutants it picks up from the surfaces. The runoff's increased quantity and speed erode stream channels and destabilize their banks, while pollutants harm plants and wildlife in rivers, streams, and bays. Additionally, increased runoff can cause water treatment plants to overflow, releasing untreated sewage into water bodies”.

I am sure you can understand my concerns and personal objection to the proposed development. I am also certain that you will find many other Bolton's Landing homeowners who have similar concerns and objections. I look forward to your cooperation and willingness to keep our area safe, beautiful and enjoyable.

Respectfully,

Bailey Westmoreland
864-580-1033
1602 Seabago Drive
Charleston, SC 29414

From: [Shuey](#)
To: [CCPC](#)
Subject: Oak Plantation Campground development
Date: Wednesday, October 07, 2020 8:05:42 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

I don't know if it will make a bit of difference, but I would like to share my concerns regarding the proposed development for the Oak Plantation Campground. I would like to know what plan is in place to alleviate the flooding and traffic congestion that will take place after this development is completed, not to mention where the kids will attend school seeing how all of our current schools are already overcrowded. I find this ridiculously insane and greedy to be honest. Does anyone even care about the people of West Ashley or is it just all about money? Everyone involved in this should be ashamed of themselves. We the people, the current residents, are not for it! We say NO!

Thanks for your time,
Shannon

Sent from my mobile device

From: [Ashley Chambers](#)
To: [CCPC](#)
Subject: Building absurdities
Date: Wednesday, October 07, 2020 10:15:12 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

It is absolutely absurd that anyone thinks more homes/apartments & shopping centers “need” to be built in Charleston or the immediate surrounding areas (West Ashley, Mount Pleasant, Summerville, North Charleston). You are ruining these cities with insurmountable traffic issues & potential flooding issues to homes & businesses. Charleston has been the top city in the United States, but won’t stay this way...mark my words, the proposed infrastructure won’t support it & people will run out of these towns. Save the wetlands, protect the trees, & care for those people that currently live here. Make Charleston more like Hilton Head. Make the property values here increase instead of decrease. We don’t need more...we NEED you to listen to the residents & fix the problems here instead of creating more!

Concerned Lowcountry resident,
Ashley

From: [Janice Deziel](#)
To: [CCPC](#)
Cc: [Dave Deziel](#)
Subject: Planned Development Discussion: TMS #286-00-00-004/-005
Date: Thursday, October 08, 2020 9:53:46 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Dear Planning Board Members:

As abutters to the proposed Planned Development (TMS # 286-00-00-004/-005) in Bolton's Landing between Bees Ferry Road and Savannah Highway, my husband and I would like to respectfully offer a couple of recommendations for consideration. Charleston is an extremely vibrant and beautiful city which is what attracted us to the area. The green spaces it contains add value to all properties and enhances the lives of all who reside there.

Economic development is vital and we recognize its importance. What we ask of you is to please consider the following in the planning and permitting process:

- Maintain a "green space" between the proposed development and Bolton's Landing along the entire border. A "green space" serves as both a natural demarcation and barrier between the properties while maintaining the value of homes located along the perimeter in Bolton's Landing.
- Require that commercial properties be located only along the Bees Ferry Road and Savannah Highway ends of the parcel leaving the center portion for residential properties.

Thank you for your time and thoughtful consideration.

Janice and David Deziel
1573 Seabago Drive
Charleston, SC. 29414

Sent from my iPad

From: [Derek Wacaster](#)
To: [CCPC](#)
Subject: Oak Plantation Campground Development
Date: Thursday, October 08, 2020 10:55:19 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

As a homeowner in West Ashley, I am deeply interested in the development of the area and excited to see improvements that will benefit our residents. I was aware of the potential of future growth when my wife and I purchased our current home. This naturally comes with West Ashley being a growing area within an attractive living destination such as Charleston. However...

The purchase and development of the Oak Plantation property intersecting Main Road, Bees Ferry and Savannah Highway is deeply concerning at this point in time. This area is (and has been) undergoing significant traffic issues; not only at Glenn McConnell and Bees Ferry intersections, but also most critically at Main Road and Savannah Highway. These areas simply cannot support the scale of the proposed development at this time.

The Main Road/Savannah Highway intersection is both a traffic choke point and safety hazard for the area. Light wait times are ridiculously long at peak times, and it has been the site of numerous serious accidents. Our city and county government must not only come to a conclusion of resolution for that intersection, but to have it completed BEFORE any additional infrastructure occurs. As we understand, plans have been discussed, but it will be a major undertaking that will require significant time to complete. This development will also bring a large increase of traffic. One must ask whether our current budget can handle the increase of wear and tear on our streets, as well as traffic and emergency services needed in our area?

I would also certainly hope our leaders would conduct the critical studies necessary to ensure no issues of potential flooding and increased high-water tables in the surrounding areas of this development. We have witnessed the entrance to this property completely underwater several times, and development will mean that water will be diverted to surrounding properties. There should be a solid plan in place to ensure this flooding does not become someone else's problem.

While it is unfortunate this beautiful piece of property and natural habitat will be demolished, obliterated and built upon in the name of progress, this is a fact of our human way of life—paving paradise and putting up a parking lot, as the song says. However, it is critical to slow this development down to ensure the critical steps of studies, infrastructure, funding and long-term planning are in place prior to approving ANY work moving forward on the Oak Plantation property development.

Sincerely,

Derek Wacaster

2921 Red Sky Drive, Charleston, SC 29414

From: [Rick](#)
To: [CCPC](#)
Subject: Oak Plantation Campgrounds development
Date: Thursday, October 08, 2020 11:01:48 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

I don't see how anyone can possibly consider this proposal do-able without first fixing the intersection at Main Rd and Route 17.

That interesection is a nightmare and has long needed a complete make over and now they want to dump several hundred more residences right in the thick of it!! I see the group pushing this proposal is out of Mt Pleasant so they won't feel any effects of it.

PLEASE, give this proposal consideration ONLY if the developer is willing to pay for a serious redesign of the Main Rd/17 intersection.

Rick Sheridan
1447 Downing St
Chstn 29407

From: [Holder Bridget J](#)
To: [CCPC](#)
Cc: [Travis Holder](#)
Subject: Plans for Oak Plantation Campground in West Ashley
Date: Thursday, October 08, 2020 11:17:17 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

To whom it may concern,

My family, my neighbors, and I; are begging you to please reconsider these plans for the Oak Plantation Campgrounds. While we have welcomed the growth of West Ashley with new restaurants, businesses, and shops; we do not welcome the 88 townhomes and especially the 600 apartments. In our neighborhood alone, there are already two apartment complexes with hundreds of apartments. These new 600 apartments will literally be in our backdoor. This is a space that we were told was protected wetlands when we decided to build here and picked the lot we did. We have our power lines and trees behind us here on this side of Seabago Drive, and we like it that way.

The infrastructure of roads in West Ashley can't handle these additional cars on them, nor can our neighborhood handle the thru traffic. Traffic is already a nightmare on Savannah Highway/Main Road and Glenn McConnell. By continuing to build more houses, apartments, and townhomes in this area of West Ashley; you are endangering the lives of millions. Emergency services such as fire, police, and EMS; cannot respond quickly enough due to the traffic and trying to get through. You are straining those resources. By continuing to build more apartments, townhomes, and houses, you are also straining the school systems here. We currently only have three middle schools in West Ashley and one high school. Where will the children of these families go? Schools already have lengthy waitlist and not enough teachers or resources.

I ask you all to look at the big picture, and the negative impact these apartments and townhomes will have on millions for years to come. Enough is enough. Please allow the infrastructure, roads, resources, and schools to catch up with the demand that is already here. Do not continue to add to the problem. Please do not take a town and place we love and destroy it, by making it unbearable for those already here. Please consider more than the money it will put in your pockets, and think of those that will be impacted. We deserve to continue to love our neighborhood. In a world with so much negativity and uncertainty, please do not make our neighborhood another peg on your board by making it a place of sadness and despair for us.

Thank you,

Bridget J. Holder & Travis Holder
Members of the Bolton's Landing Community

From: [Lawrence Owens](#)
To: [CCPC](#)
Subject: Development at Oak Plantation Campground
Date: Thursday, October 08, 2020 1:30:07 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Please stop this plan from moving forward!

I live off of Main Road and this site is in a flood plain. There will be no place for the water to drain to if this development moves forward. How did this ever get this far in the approval process.

Members of the planning commission need to visit this site on a rainy day to observe what is happening. Don't forget your boots.

From: [Eric & Linda Willson](#)
To: [CCPC](#)
Subject: Oak Plantation Campground Development
Date: Thursday, October 08, 2020 1:38:56 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

October 8, 2020

To the Commission Board:

First, thanks for patronizing us by accepting comments, but I seriously doubt we have any influence over whether or how this development happens. This is just another charade and waste of time for residents. At the end of the day....Big Business, Business Partners, Banks, Politics, and \$\$\$\$\$ prevail!!

But just for laughs.....

Please explain how 600 apartments, 88 townhomes, a grocery store and a 125-room hotel equates to a 'hub gathering' for the community?

A hub gathering of what?..... of concrete, drywall, plumbing and monumental traffic?

Oh and add a sprinkle of some under utilized shops and services that are already duplications of what we have anchored with an already existing Harris Teeter and Publix in the same area....like Circle K, Subway, UPS, Bank, Vitamin Store, Liquor Store. To complete the duplication, a car wash and another car lot/boat lot would round it out.

Not to mention the traffic nightmare that already exists on Main Road and Hwy 17 and the flooding issues that seemed to halt development of the much promised and anticipated West Ashley Circle.

What a waste of valuable greenspace to just add another concrete jungle!!! It would be nice to have something developed that can serve the many communities that ALREADY EXIST in this area and have paid years of taxes. West Ashley is as dry as a desert of any type of cultural, universal retail, entertainment, educational outlets for existing residents to access and enjoy a nice quality of life. I thought that was what a 'hub gathering' would look like.

I really do not see the vision of the Nexton Village that was referenced. I see a copy/paste of the Harris Teeter shopping center that was just constructed less than a mile from this development.

We will probably pay our taxes to Berkley/Dorchester County in the not too distant future.

Dishearten and disappointed,

Eric and Linda Willson
Hunt Club Residents for 16 years
West Ashley Residents for 31 years

From: [Skylar Stewart-Clark](#)
To: [CCPC](#)
Subject: Proposed Development at Main Rd/Hwy 17
Date: Thursday, October 08, 2020 4:05:31 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

My name is Skylar Stewart-Clark, and I have been a resident of the Hunt Club Subdivision for a little more than 3 years. I am writing to strongly express my opposition to an integrated commercial and residential PD at TMS 286-00-00-0004 and 286-00-00-05 (3550 Savannah Highway).

Development of this property to the degree proposed will be detrimental to the area. Residents of my neighborhood and nearby subdivisions are opposed to the addition of another grocery store, 600 apartments, 88 townhomes, a 250 room hotel, and 7 outparcels. This plan will be disruptive, create safety and traffic problems and most certainly will create problems with schools such as CE Williams, which though just constructed, was knowingly over capacity for the number of students slated to be attending. Why put another 688 families in the area? Busses and mass transit is already inadequate.

Why would the county allow a developer to come in and exacerbate problems families are already experiencing? It is already difficult to leave Hunt Club, cross Bees Ferry Rd, and get to Savannah Highway on many occasions. Infrastructure such as public utilities, landfills, and protections against flooding have to be considered. Further, per my research, the property is zoned industrial, which does not allow for apartments and townhomes.

I implore you to disapprove of this development. From discussions with neighbors, I know my thoughts are shared by many.

Thank you for your service to our communities.

Respectfully

Skylar Stewart-Clark, PhD, PA-C

From: [Philip Smith](#)
To: [CCPC](#)
Subject: Oak Plantation Development
Date: Thursday, October 08, 2020 4:08:57 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Recently became aware the Oak Plantation Campground is the subject of plans for redevelopment into a housing area. We strongly urge that this not happen for the following two reasons. First the area is subject to flooding. As a part of a flood plain, extraordinary water events would be denied a large area into which to drain excess water. Flooding is already a major topic in this area. No new fuel is needed for flooding concerns that already exist. Secondly road traffic is already a major problem for that area. More vehicles are not required to make the roads impassable- that characteristic already exists. Thank you for the opportunity to voice our concerns. Philip G. & Anna C. Smith, 2226 S. Dallerton Cir. Charleston 29414

From: [Michael Willson](#)
To: [CCPC](#)
Subject: Proposal for 2860000004
Date: Thursday, October 08, 2020 5:01:55 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

With reference to the proposed plan for tms 286 0000004 (the Camp Ground Site) The plan is too dense and shows no provisions for increased runoff, traffic and other matters of interest. What about the remainder of the tract not shown as being part of this proposal? As a West Ashley home owner I am concerned with the ever increasing problems caused by developments such as this and urge your rejection.

Thank you for your consideration:

M i c h a e l W i l l s o n
greydog165@homemail.com
843 556 0828
1235 Kensington Drive
Charleston, SC 29407-7734

From: [Matthew Nathan](#)
To: [CCPC](#)
Subject: Development
Date: Thursday, October 08, 2020 7:49:52 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Hello,

I just wanted to say I think the development of this area is a great thing. It will increase home values as well as provide jobs and income to the area. I hope the people against this will not hold back this areas growth.

Kind Regards,

Matthew Nathan, DVM

Sent from my iPhone

From: [LaDon Wallis](#)
To: [CCPC](#)
Subject: Opposition to Oak Plantation Campground Proposed PD
Date: Thursday, October 08, 2020 7:53:23 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Dear Charleston County Planning Commission,

I am writing to express my STRONG opposition to consider the planned development at the Oak Plantation Campground, as it is currently proposed. This property is zoned industrial, and according to the ZLDR, townhomes and apartments are not allowed as of right, and only single family detached up to 4 per acre are allowed, with special exception. The other proposed development in this PD is allowed as of right. There is no need to request a PD. Also, the density requested for this planned development (600 apartments and 88 townhomes) exceeds that allowed in a PD (assuming no more than 8 to 8.8 per acre, and as such, it appears the developer has allotted appx 20 acres for housing, thus only appx 200 units should be allowed). Regardless, this property sits at the edge of the urban growth boundary and according to the comprehensive plan's land use elements, density should be limited near this area. 688 units is NOT limited density.

The land sits just at or above 6 feet, and a Category 3 hurricane storm surge will completely inundate the developed area. The adjacent creek is part of the church creek basin. Dutch dialogue recommendations and resilience elements should be implemented.

Flooding will be worse once appx 80 acres are paved. Unless pervious surfaces and native plants are used, this will contribute to, and worsen, the flooding. The wetlands hold the water runoff from the current unpaved property, but they will be overwhelmed once this area is paved.

Traffic will be infinitely worse as you will be adding more than appx 1000 cars on the roads, which are inadequate and cannot handle the volume that already exists. Even in light of the proposed GM and MR corridor improvements, there are still no alternative means of rapid transportation available to this area. EVERYONE drives their cars. An 8 mile commute takes me 45 minutes from Bees Ferry and GM to St. Andrews Blvd near Cosgrove (pre-COVID traffic). Traffic studies need to consider that less traffic is currently on the roads. There are also too many apartments currently planned along Bees Ferry, Glenn McConnell, and Magwood. In addition to the 950 homes Dorchester County is planning to build along 61, and possible townhomes on 61, this would be creating more strain on the infrastructure.

There is already a Publix across the street and thus no need for a Harris Teeter (or any grocery store). We currently have 4 stores within a 4 mile radius, while other parts of West Ashley and N. Chas have food deserts.

I URGE the planning department to DENY consideration of this proposed PD. Only consider uses that are allowed in the industrial zone and DO NOT approve the housing developments. Charleston County has a chance to get this right and consider the negative impacts this development will have on the environment and the livability of those who live in this area.

Very respectfully,
LaDon Paige

886 Hunt Club Run
Charleston, SC 29414
706-255-8616

Sent from my iPhone

From: [Park Paige](#)
To: [CCPC](#)
Subject: Planned Development Oak Plantation
Date: Thursday, October 08, 2020 7:59:17 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

I oppose approval of the PD for the land which sits at the current OPC site that would allow 600 apartments and 88 townhomes. This density is too much for the current infrastructure and will adversely affect the livability of the residents of West Ashley.

Thank you for your consideration to deny this proposal.

Park Paige
Hunt Club Subdivision

Sent from my iPhone

From: [Megan](#)
To: [CCPC](#)
Subject: Grand Oak Plantation Campground
Date: Thursday, October 08, 2020 8:26:32 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Members of the Charleston County Planning Commission,

>>

>> The possible rezoning to permit a Planned Unit Development on the Oak Plantation Tract at Main Road and Savannah Highway has come to my attention. This potential for extensive development deeply concerns me for a variety of reasons, and I am writing in opposition to rezoning which would enable such development at that location.

>> This 150 acre tract of land is a significant environmental resource to the area. It is a rare situation to have such a large tract of minimally developed land in this area. Development of the tract could result in well over 1 million square feet of pervious surface being removed from an area which already suffers from extensive flooding in minor rain events. Where will 100,000 cubic feet of water go in a one inch rainfall? Our storm infrastructure is already overburdened and as a consequence taxpayers such as myself are needlessly bearing the cost of ill supported overdevelopment. The continued rampant development and absence of adequate infrastructure generates unnecessary and nonproductive expenditures, increased risk and inconvenience.

>> Similarly, our roadways too are ill prepared to address the demands mixed use development of this tract will create. It raises serious questions about the overall quality of life for residents of the area as well as the economic future of Charleston. What will be the impact be of at least 1000 more vehicles traversing this location in addition to those generated by ongoing development of local smaller property parcels. How much longer will our commutation become with the additional traffic and traffic control devices and increased number of accidents that development of this property will generate? And at what point do potential tourists and business start to view Charleston as overly congested and look elsewhere because of the problems associated with overdevelopment?

>> As recognized in the County Comprehensive Plan land is a vital resource. It is a resource which does not regenerate. Undeveloped land serves a very critical role in maintaining the balance in our society and activities and quite frankly our survival as a species. As this is one of the largest undeveloped tracts of land in this area the County should be studying it to determine what the best constructive use for it can and should be for the benefit of the citizens of the County. It should not merely be responding to the economic based desires of the present owner and a potential buyer/developer.

>> It is time to represent the citizens of the community not the special interests.

>> If there is to be any zoning change, I recommend that consistent with the Comprehensive Land Use Plan, that the Commissioners modify it so the Charleston County Park and Recreation Commission may acquire the site and preserve it for passive parkland use by future generations.

>> Thank you for your consideration.

>>

>> Sincerely,

>> Megan Fanning

>> A Hunt Club resident

>>

From: [Pamela Ingram](#)
To: [CCPC](#)
Subject: Campground development
Date: Thursday, October 08, 2020 8:49:12 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

I am writing to express my concern about the development being planned for the campground property at the intersection of 17S/Main Rd./Bees Ferry Rd.

First, this area falls in the Church Creek drainage area. Church Creek is over capacity as it is from overdevelopment in the area. Development must be planned when thinking of the big picture not just one piece of property. I cannot imagine that either study completed on the flooding problems in Charleston would give this plan the green light.

Additionally, traffic is already a nightmare in that area. The 17S/Main Rd intersection in particular is atrocious and is a main bottleneck area for traffic. Traffic there often backs up for miles and also onto Bees Ferry Rd. The Glenn McConnell will also get more traffic from this development, and even though it will be widened, those plans will be obsolete by the time Long Savannah and all the other Bees Ferry Rd development (more shopping centers and apartments) are being built.

By adding yet another grocery store to the area, you are increasing the risk of empty shopping centers in the future. The plan is also going to draw in more traffic as people who live in food deserts drive to our area to shop. As much as I would love to not have the traffic issues in Grand Oaks that the new Harris Teeter contributed to, adding another one so close by is just ridiculous. There is also another Publix planned for across Bees Ferry from Grand Oaks. That will be seven grocery stores (including Wal-Mart) in a small radius.

Furthermore, our schools are overcrowded and adding more people before there are more schools is going to further damage future generations of children.

It is also my understanding that the density of this development is not allowed by the urban growth boundary. This project also does not seem to fall under the zoning regulations for the property. What good does it do to plan and set boundaries if they are just going to be ignored?

Thank you for your attention to this matter. I will not be able to attend the 10/12 meeting due to my work schedule.

Sincerely,
Pamela E. Ingram
1036 Tyron Circle
Charleston, SC 29414

Sent from my iPhone

From: [tgardy1](#)
To: [CCPC](#)
Subject: Oak Plantation Campground Planned Development
Date: Thursday, October 08, 2020 9:27:45 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

As a concerned Charleston County Citizen
I am opposed to the new development to be
built on the Oak Plantation Campground
property. I don't believe that the current highway
17 can properly handle the additional traffic
this development would generate. Ingress and Egress to this property off highway 17
would
be a major problem for an already over burdened
highway. Congestion in this area is already a problem we don't need to exacerbate it.

Respectfully submitted
Anthony J Gardy

Sent from my Verizon, Samsung Galaxy smartphone

From: [Marty Sorrentino](#)
To: [CCPC](#)
Cc: [LaDon Wallis](#)
Subject: 3550 Savannah Highway project
Date: Thursday, October 08, 2020 9:30:44 PM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Dear Sir/Madam

It is with great displeasure that I must voice my opinion regarding the proposed project 3550 Savannah Highway in West Ashley.

It is beyond belief that such a project could be conceived given the horrendous traffic volume at the Main Road/Savannah Highway intersection. I have become accustomed to stupid and poorly planned developments that seek to enrich the builders and investors without considering the effects on traffic and a harmonious balance of both natural beauty and SMART development. West Ashley has become the "poster child" for the "suburbanization" of the Lowcountry. We too shall join the ranks of Mount Pleasant residents who fight traffic and overdevelopment of their once "idyllic" community on a daily basis.

If the planning commission moves forward on this project WITHOUT major roadway improvements I fear West Ashley will become just another "suburban sprawl" of concrete and asphalt.

I beg the Commission to re think this proposal and preserve what little natural beauty remains here in West Ashley. What brought me and many others to the Lowcountry was the "country" feel of the area. A mixture of smart planning and a wise use of open space. I fear this shall one day soon disappear and all we will be left with will be memories of "what once was" a beautiful community.

Marty and Tammy Sorrentino
Grand Oaks Resident
432 Sycamore Shade Street
Charleston SC 29414
516-551-0082
Martysorrentino@gmail.com

From: [Doug White](#)
To: [CCPC](#)
Subject: Planned development of the existing Oak Plantation Campground
Date: Friday, October 09, 2020 8:52:11 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

The proposed planned development at Main Rd-Hwy17-Bees Ferry Rd on land zoned industrial which has a land use element number 11 is way beyond the density allowed and more than the infrastructure can support. The need for utilities, landfills, schools, mass transit, police and first responders will greatly increase. The intersections on Bees Ferry and Main Rds, Main Rd and Hwy17 are congested now and are sites of numerous accidents. Another issue is drainage. Where will all the water go? The developers will make a lot of money and then walk away leaving us with more problems to solve. Our area does not need or can support this project. Doug White. 845 Hunt Club Run, Charleston, SC 29414

From: ramin.farahvashi
To: [CCPC](#)
Subject: 3550 Savannah Hwy Johns Island, SC 29455
Date: Friday, October 09, 2020 11:28:54 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Good day

But bad day for my Neighborhood hunt club which county promise by 2006 will shut down and relocate the land fill ! Now with this development at 3550 Savannah Hwy already traffic and trucks traffic and intersection of main road and Savannah Hwy as bad as can bee !!! ???
Development of this site must be stopped for more study and flooding issues will be the biggest problem for this area keep building over the vet lands and not do anything about drainage under the railroad!

I would love to see your attention to this issue thanks

Ramin Farahvashi
Hunt Club resident

From: [Jordan Mann](#)
To: [CCPC](#)
Subject: Regarding a Planned Development (PD) at TMS 286-00-00-004 and 286-00-00-005 (3550 Savannah Highway)
Date: Friday, October 09, 2020 10:18:25 AM

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Hi,

As a homeowner in the Hunt Club subdivision across from the planned development site we strongly disapprove of this planned development. The development would only further strain our existing resources and infrastructure. The Savannah Hwy/Main intersection is already over its capacity and a constant location of traffic accidents and grid lock. Adding further congestion in the area without infrastructure improvements will only continue the theme of jamming more developments into an infrastructure not built to handle them. This will bring further traffic and flooding issues to our area that are not needed.

I strongly disagree with the planned development and would prefer our county to focus on improving the infrastructure we have before exacerbating the problem further.

Thank You,
Christopher Mann

**ZLDR Text
Amendment
Request:**

ZLDR-08-20-00124

ZLDR-08-20-00124

- Case History**
- Presentation**
- Application**

ZLDR-08-20-00124: Case History

Planning Commission: October 12, 2020
Public Hearing: November 10, 2020
Planning and Public Works Committee: November 19, 2020
First Reading: November 19, 2020
Second Reading: December 8, 2020
Third Reading: December 17, 2020

CASE INFORMATION

Applicant: John Aquino

Owner: Expressway Center LLC

Location: 1920 Dunbar Street

Parcel Identification: 350-05-00-057

Council District: 7 - Moody

Property Size: 1.03 acres

Application: Request to amend the Zoning and Land Development Regulations Ordinance (ZLDR), Table 5.13-1, *DuPont-Wappoo Area Overlay Zoning District Use Table*, to conditionally allow “Beverage or Related Products Manufacturing, including Alcoholic Beverages and Excluding Microbreweries and Brewpubs” and “Food or Related Products Manufacturing” in the Community Commercial District (Du-Wap-O only), subject to conditions, Sec. 6.4.57, *Manufacturing and Production*.

Zoning History: The subject property, TMS 350-05-00-057 was zoned General Commercial (CG) on the original 1970 County Zoning Map, but was subsequently rezoned to Community Commercial (CC) as part of the adoption of the 2001 Zoning and Land Development Regulations Ordinance. The property was included in the Community Commercial District with the adoption of the Dupont-Wappoo Overlay Zoning District in November 2016.

Adjacent Zoning: The subject property contains a multi-tenant commercial building. Adjacent properties to the North, East, and West are also zoned Community Commercial in the DuWap-O and contain boat storage, vehicle sales, a tattoo shop, and a veterinary clinic. Properties to the South are in the City of Charleston, zoned General Business (GB), and are undeveloped.

Municipalities Notified/Response: The City of North Charleston, Town of James Island, Town of Kiawah Island, Berkeley County, City of Charleston, City of Folly Beach, City of Isle of Palms, Colleton County, Town of Awendaw, Town of Hollywood, Town of Lincolnville, Town of McClellanville, Town of Meggett, Town of Mt. Pleasant, Town of Ravenel, Town of Seabrook Island, and the Town of Summerville have all been noticed of this request.

APPROVAL CRITERIA

According to Section §3.3.6 of the *Zoning and Land Development Regulations Ordinance (ZLDR)*, applications for ZLDR Text Amendments may be approved only if County Council determines that the following criteria are met:

A. The proposed amendment corrects an error or inconsistency or meets the challenge of a

changing condition;

Applicant's Response: *"I would submit that the area of Dupont/ Wappoo community commercial overlay is a commercial area in which my proposed usage is very much in line with several already established businesses. Co-packing salts, bloody mary mix, relishes and pickles is similar to the restaurants within the overlay, and I am upheld to the same safety procedures and compliance requirements, the only difference being that I am overseen by the USDA and not DHEC. I take these practices very seriously and refuse to compromise; safety is most important."*

B. The proposed amendment is consistent with the adopted Charleston County Comprehensive Plan and goals as stated in Article 1.5;

Applicant's Response: The applicant's letter of intent states that criteria A, H, I, J, and N are met as the applicant, *"plans on adhering to all rules and regulations as specified, and enhancing if possible,"* and *"on setting up a teaching kitchen, using local chefs, garden center owners, and nutritionists in order to show the elderly and children that you can eat healthy, grow your own herbs and veggies, and maintain a healthier lifestyle. Showing them how easy it is to do these simple things will cut down on medical bills and promote a longer life.* The applicant is *"currently running 5 businesses out of this building, co-packing for most. This amendment may open the doors for others wanting to start their own business and give back to the community by sharing products and ideas in order to enhance the healthy lifestyle project we envisioned.* The applicant is *"already donating time and products to multiple charities, such as Ronald McDonald House, Project Green Heart, MUSC Children's Fund, and other locally specific events,"* and they *"encourage all others to do so via social media and personal connections."* The applicant has *"also connected with all tenants in our block and have a wonderful relationship with all."*

C. The proposed amendment is to further the public welfare in any other regard specified by County Council.

Applicant's Response: *"As far as furthering public welfare and other community services, I pride myself on recycling my jars from local restaurants and bars and reusing them after a thorough cleansing. I have minimal waste; for example, I use the solids from my hot sauce to create a jalapeno pineapple relish, I use the waste from pureeing the tomatoes to send to farmers for compost or feed, and I filter rainwater for washing dishes and floors. I also use biodegradable plastic for my cocktail mixers and give many cases of products to charitable events. In the near future, I want to use my facility as a teaching kitchen to show that healthy eating and living is not as hard as people think, with the main focus on children and the elderly."*

Staff Recommendation – Sec. 3.3.6, Approval Criteria, has been met; therefore, staff recommends approval of the request.

PLANNING COMMISSION MEETING: October 12, 2020

Notifications: 749 notification letters were sent to owners of property located within 300 feet of the boundaries of the subject parcel and individuals on the ZLDR/Comprehensive Plan and St. Andrews Interested Parties Lists on September 25, 2020. Additionally, these requests were noticed in the *Post & Courier* on September 25, 2020.



Charleston County ZLDR Text Amendment Request

Planning Commission: October 12, 2020

Public Hearing: November 10, 2020

Planning and Public Works Committee: November 19, 2020

First Reading: November 19, 2020

Second Reading: December 8, 2020

Third Reading: December 17, 2020

ZREZ-08-20-00124

- West Ashley Area: 1920 Dunbar Street
- Parcel I.D.: 350-05-00-057
- Owner: Expressway Center LLC
- Applicant: John Aquino
- Property Size: 1.03 acres
- Council District: 7 - Moody

Request to amend the Zoning and Land Development Regulations Ordinance (ZLDR), Table 5.13-1, *DuPont-Wappoo Area Overlay Zoning District Use Table*, to conditionally allow “Beverage or Related Products Manufacturing, including Alcoholic Beverages and Excluding Microbreweries and Brewpubs” and “Food or Related Products Manufacturing” in the Community Commercial District (Du-Wap-O only), subject to conditions, Sec. 6.4.57, *Manufacturing and Production*.

Zoning History

- The subject property, TMS 350-05-00-057 was zoned General Commercial (CG) on the original 1970 County Zoning Map, but was subsequently rezoned to Community Commercial (CC) as part of the adoption of the 2001 Zoning and Land Development Regulations Ordinance.
- The property was included in the Community Commercial District with the adoption of the Dupont-Wappoo Overlay Zoning District in November 2016.

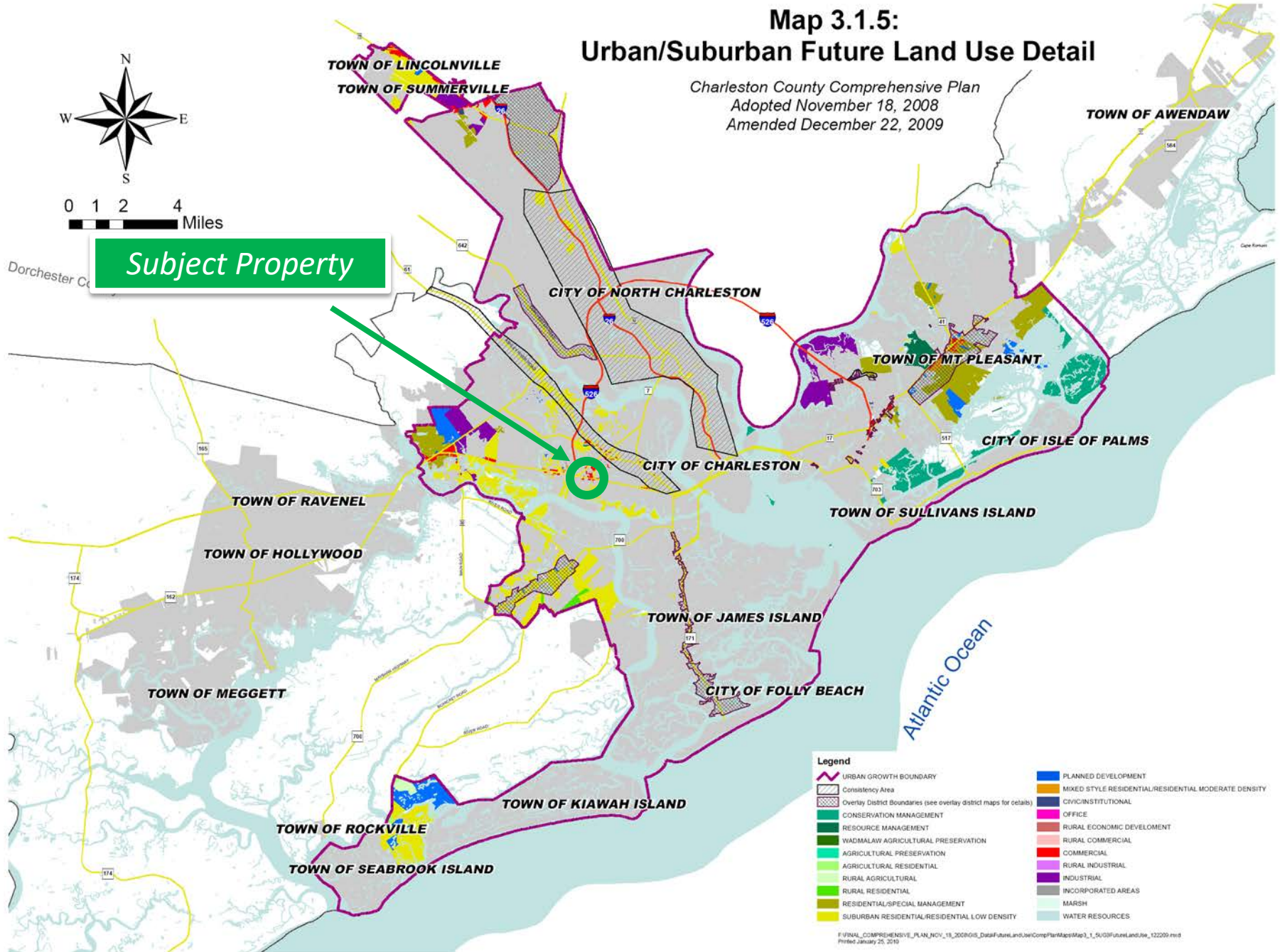
Map 3.1.5: Urban/Suburban Future Land Use Detail

Charleston County Comprehensive Plan
Adopted November 18, 2008
Amended December 22, 2009



0 1 2 4
Miles

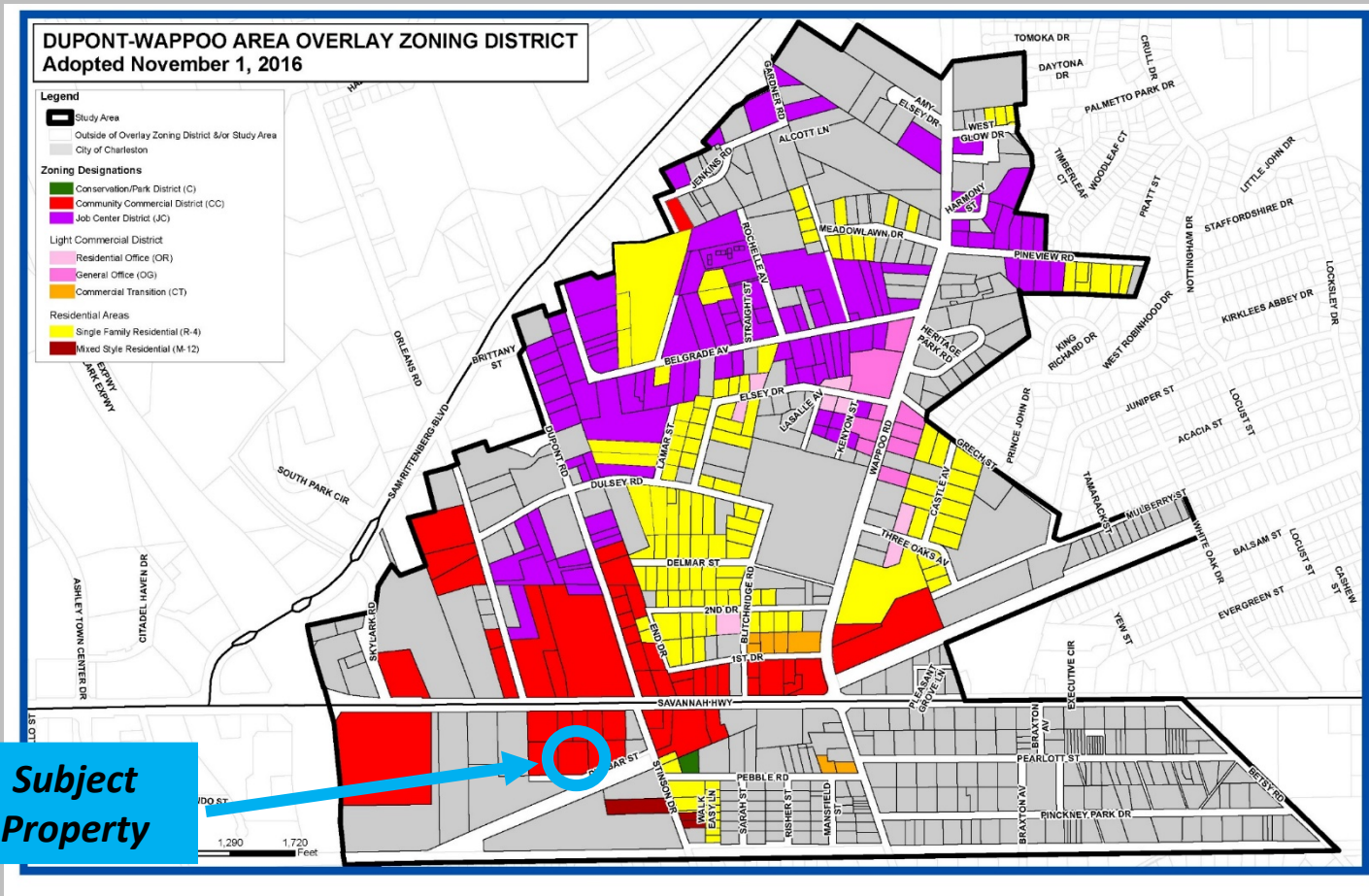
Subject Property



Legend

- URBAN GROWTH BOUNDARY
- Consistency Area
- Overlay District Boundaries (see overlay district maps for details)
- CONSERVATION MANAGEMENT
- RESOURCE MANAGEMENT
- WADMALAW AGRICULTURAL PRESERVATION
- AGRICULTURAL PRESERVATION
- AGRICULTURAL RESIDENTIAL
- RURAL AGRICULTURAL
- RURAL RESIDENTIAL
- RESIDENTIAL/SPECIAL MANAGEMENT
- SUBURBAN RESIDENTIAL/RESIDENTIAL LOW DENSITY
- PLANNED DEVELOPMENT
- MIXED STYLE RESIDENTIAL/RESIDENTIAL MODERATE DENSITY
- CIVIC/INSTITUTIONAL
- OFFICE
- RURAL ECONOMIC DEVELOPMENT
- RURAL COMMERCIAL
- COMMERCIAL
- RURAL INDUSTRIAL
- INDUSTRIAL
- INCORPORATED AREAS
- MARSH
- WATER RESOURCES

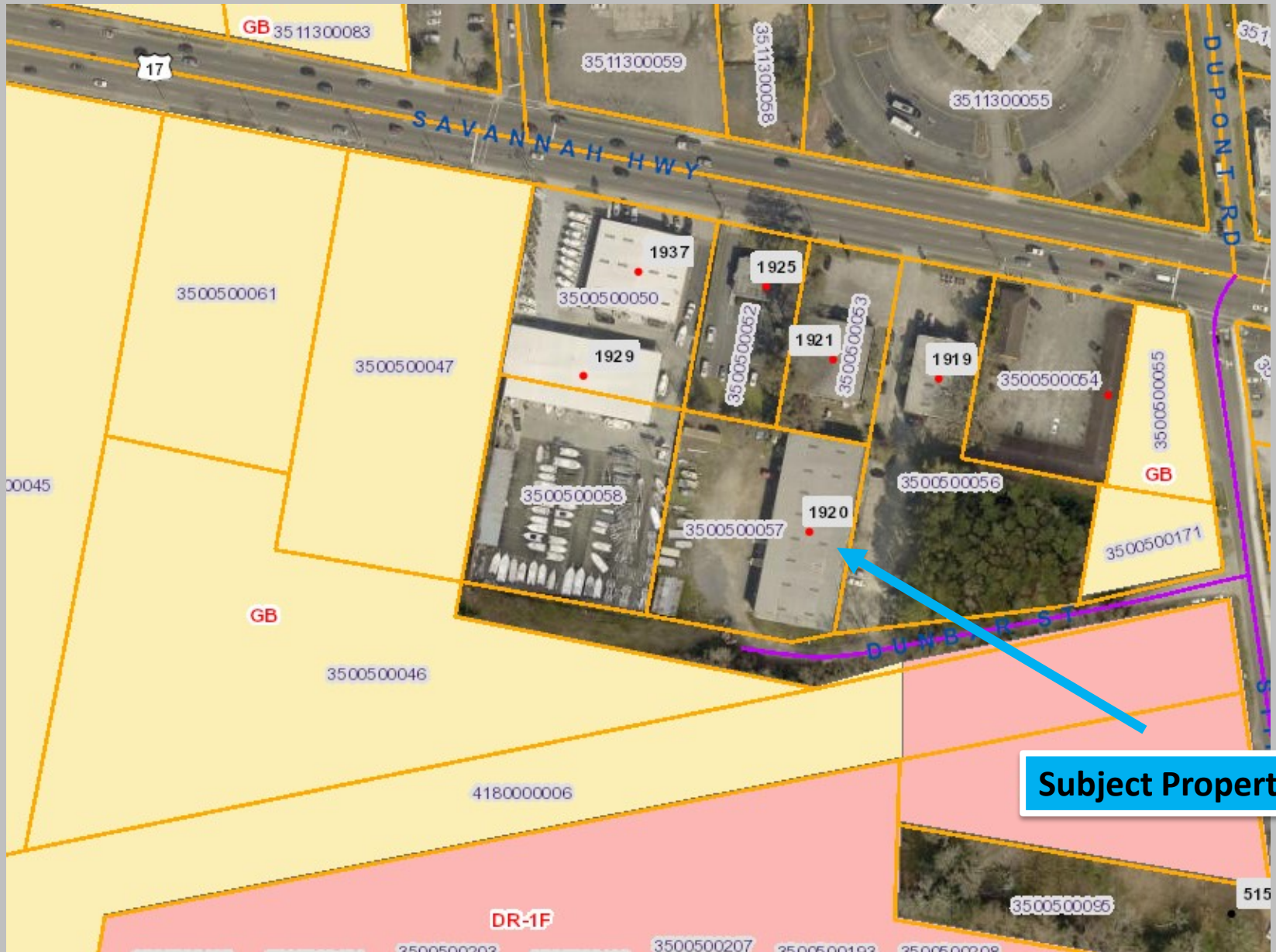
Current Zoning



**Subject
Property**

The subject property contains a multi-tenant commercial building. Adjacent properties to the North, East, and West are also zoned Community Commercial in the DuWap-O and contain boat storage, vehicle sales, a tattoo shop, and a veterinary clinic. Properties to the South are in the City of Charleston, zoned General Business (GB), and are undeveloped.

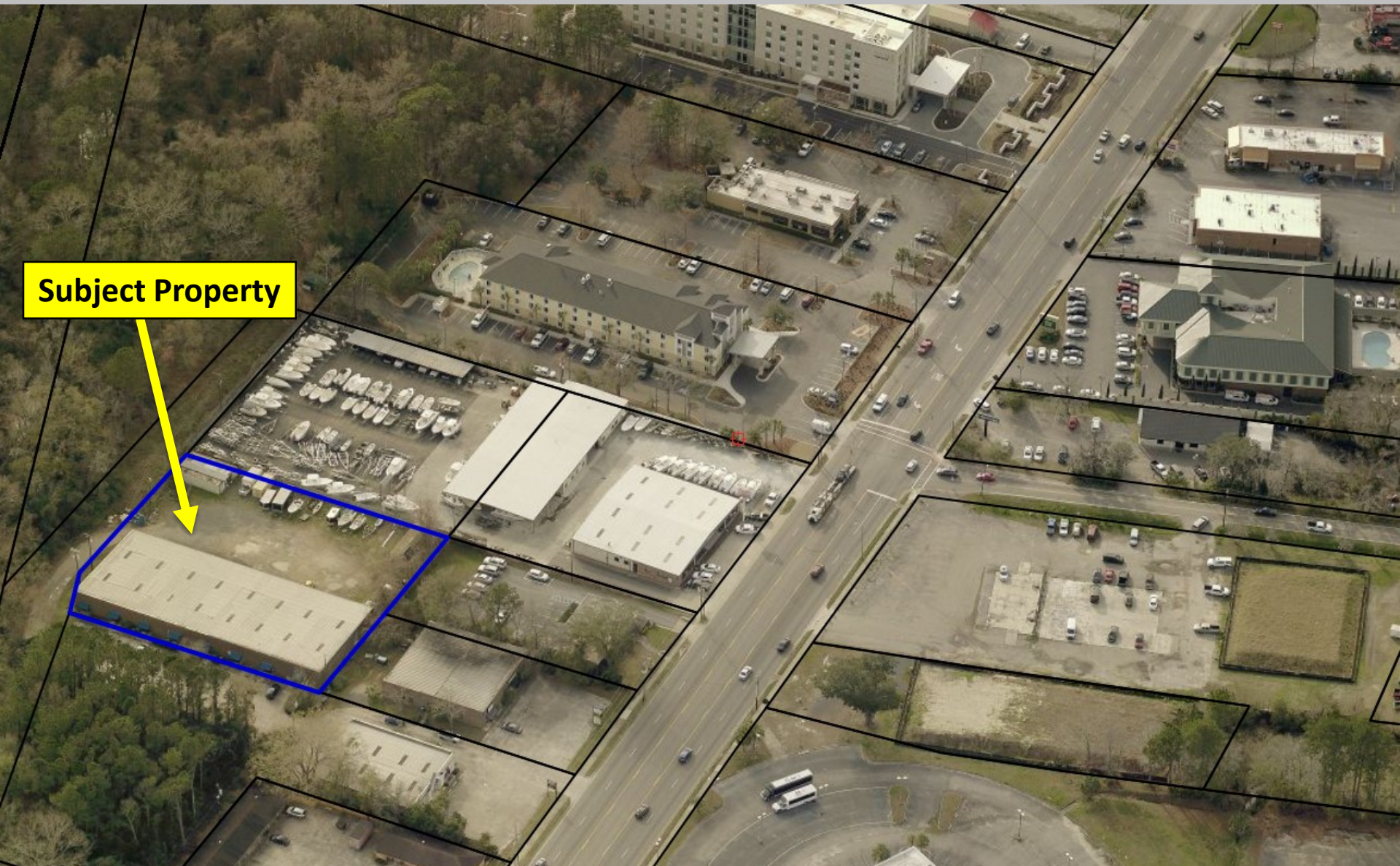
City of Charleston Zoning



Aerial View to the East



Aerial View to the West



Subject Property

Proposed Amendments to ZLDR Text

§5.13.6, Use Regulations, Table 5.13-1, Dupont-Wappoo Area Overlay Zoning District Use Table

Table 5.13-1, DuPont-Wappoo Area Overlay Zoning District Use Table									
<p>"A" indicates uses allowed by right.</p> <p>"C" indicates uses subject to conditions. A cross-reference to the applicable conditions can be found in the column entitled "Conditions."</p> <p>"S" indicates uses allowed only if reviewed and approved in accordance with the Special Exception procedures of this Ordinance, subject to compliance with use-specific conditions. A cross-reference to the applicable conditions can be found in the column entitled "Conditions."</p> <p>Blank cells indicate uses that are not permitted.</p>									
USES	Job Center District	Community Commercial District	Light Commercial District			Residential Areas			Conditions
			OR	OG	CT	R4	M8	M12	
INDUSTRIAL									
MANUFACTURING AND PRODUCTION									
Aircraft Manufacturing, including Related Parts	A								
Beverage or Related Products Manufacturing, including Alcoholic Beverages and Excluding Microbreweries and Brewpubs	A	C							§6.4.57
Clay or Related Products Manufacturing	A	C							§6.4.57
Computers or Electronic Products Manufacturing	A	C							§6.4.57
Electrical Equipment, Appliances, or Components Manufacturing	A	C							§6.4.57
Fabric or Apparel Manufacturing, including Textile Mills	A								
Food or Related Products Manufacturing	A	C							§6.4.57
Furniture, Cabinets, or Related Products Manufacturing	A								
Glass or Related Products Manufacturing	A	C							§6.4.57
Leather Products Manufacturing, including Tanneries	A								
Machinery, Tools, or Construction or Construction Equipment Manufacturing, including Farm Equipment	A	C							§6.4.57
Microbreweries	C	C							§5.13.7.B
Printing Press Production or Lithography	A	C							§6.4.57
Professional or Commercial Equipment or Supplies Manufacturing, including Office, Medical, Restaurant Equipment, or Specialty Items	A	C							§6.4.57
Sign Manufacturing	A	C							§6.4.57
Toy or Artwork Manufacturing	A	C			C				§6.4.57
Wood Products Manufacturing	A	C			C				§6.4.57
Other Miscellaneous Manufacturing and Production	C	C			C				

§6.4.57 Manufacturing and Production

1. The following conditions shall apply to all Zoning Districts subject to conditions (C):
 1. All activities related to the specialized manufacturing use shall be confined to a structure that is entirely enclosed; and.
 2. All specialized manufacturing uses shall comply with the Site Plan Review requirements of this Ordinance.
2. The following additional conditions shall only apply to the CR, CT, and CC Zoning Districts:
 1. A structure or structures used for specialized manufacturing shall have a maximum floor area of 5,000 square feet; otherwise, this use shall fall under the Special Exception provisions of this Ordinance;
 2. Specialized manufacturing shall have no more than ten (10) employees, otherwise this use shall fall under the Special Exception provisions of this Ordinance; and
 3. On-site retail sales are limited to 25% of the gross receipts and 15% of the floor area.

DUPONT-WAPPOO AREA OVERLAY ZONING DISTRICT Adopted November 1, 2016

Legend

- Study Area
- Outside of Overlay Zoning District &/or Study Area
- City of Charleston

Zoning Designations

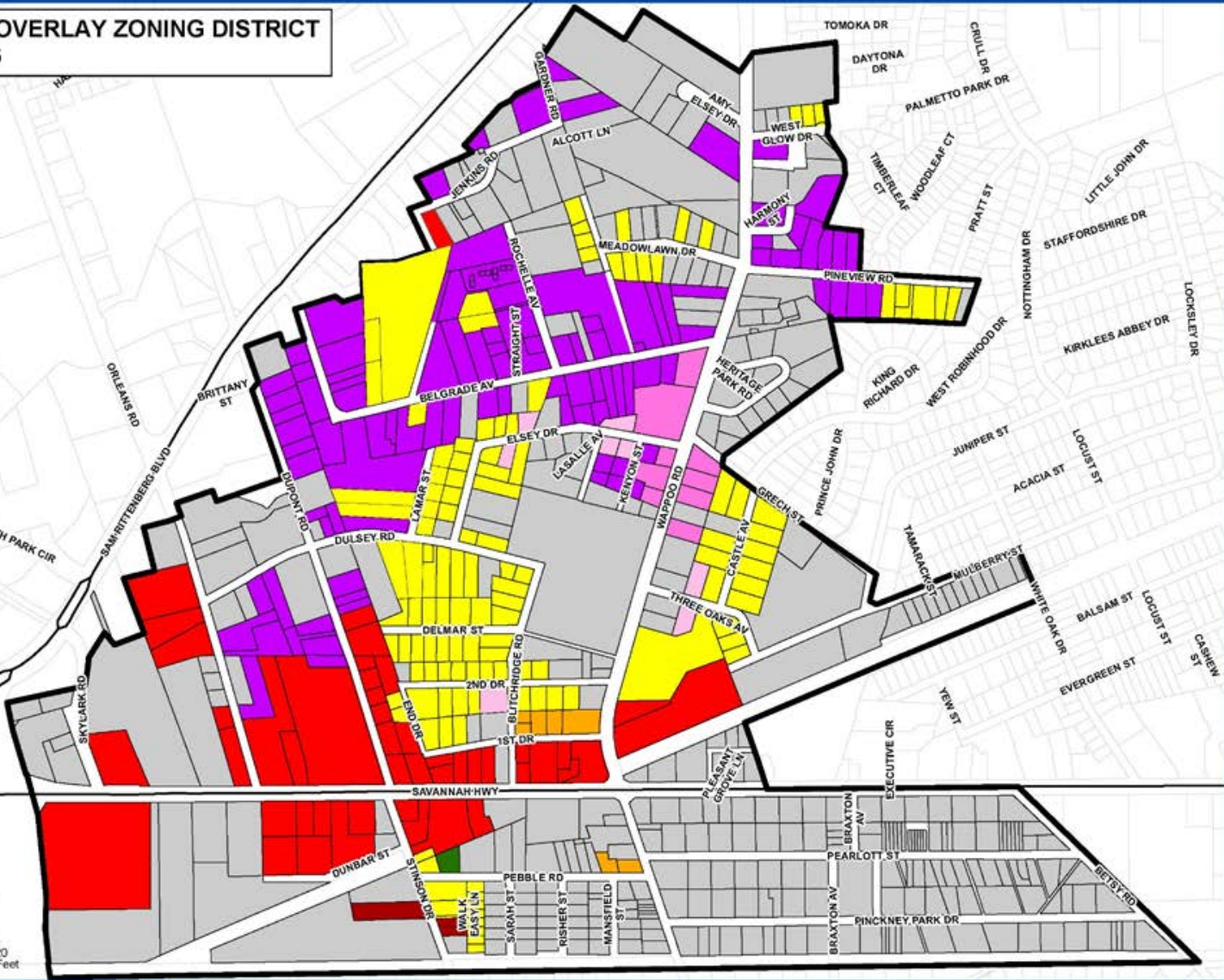
- Conservation/Park District (C)
- Community Commercial District (CC)
- Job Center District (JC)

Light Commercial District

- Residential Office (OR)
- General Office (OG)
- Commercial Transition (CT)

Residential Areas

- Single Family Residential (R-4)
- Mixed Style Residential (M-12)



The proposed amendment would impact all Community Commercial properties in the DuPont-Wappoo Area Overlay Zoning District, shown here in red.

Approval Criteria—Section 3.3.6

According to Section §3.3.6 of the Zoning and Land Development Regulations Ordinance (ZLDR), applications for ZLDR Text Amendments may be approved only if County Council determines that the following criteria are met:

A. The proposed amendment corrects an error or inconsistency or meets the challenge of a changing condition;

Applicant's Response: *"I would submit that the area of Dupont/Wappoo community commercial overlay is a commercial area in which my proposed usage is very much in line with several already established businesses. Co-packing salts, bloody mary mix, relishes and pickles is similar to the restaurants within the overlay, and I am upheld to the same safety procedures and compliance requirements, the only difference being that I am overseen by the USDA and not DHEC. I take these practices very seriously and refuse to compromise; safety is most important."*

Approval Criteria—Section 3.3.6

B. The proposed amendment is consistent with the adopted Charleston County Comprehensive Plan and goals as stated in Article 1.5; and

Applicant's Response: The applicant's letter of intent states that criteria A, H, I, J, and N are met as the applicant, *"plans on adhering to all rules and regulations as specified, and enhancing if possible,"* and *"on setting up a teaching kitchen, using local chefs, garden center owners, and nutritionists in order to show the elderly and children that you can eat healthy, grow your own herbs and veggies, and maintain a healthier lifestyle. Showing them how easy it is to do these simple things will cut down on medical bills and promote a longer life.* The applicant is *"currently running 5 businesses out of this building, co-packing for most. This amendment may open the doors for others wanting to start their own business and give back to the community by sharing products and ideas in order to enhance the healthy lifestyle project we envisioned.* The applicant is *"already donating time and products to multiple charities, such as Ronald McDonald House, Project Green Heart, MUSC Children's Fund, and other locally specific events,"* and they *"encourage all others to do so via social media and personal connections."* The applicant has *"also connected with all tenants in our block and have a wonderful relationship with all."*

Approval Criteria—Section 3.3.6

C. The proposed amendment is to further the public welfare in any other regard specified by County Council.

Applicant's Response: *“As far as furthering public welfare and other community services, I pride myself on recycling my jars from local restaurants and bars and reusing them after a thorough cleansing. I have minimal waste; for example, I use the solids from my hot sauce to create a jalapeno pineapple relish, I use the waste from pureeing the tomatoes to send to farmers for compost or feed, and I filter rainwater for washing dishes and floors. I also use biodegradable plastic for my cocktail mixers and give many cases of products to charitable events. In the near future, I want to use my facility as a teaching kitchen to show that healthy eating and living is not as hard as people think, with the main focus on children and the elderly.”*

Staff Recommendation

Approval

The ZLDR Text Amendment Request meets all of the approval criteria of Sec. 3.3.6.

Notifications

September 25, 2020

- 749 notifications were sent to owners of property located within 300 feet of the boundaries of the subject parcel and individuals on the ZLDR/Comp Plan and St. Andrews Interested Parties Lists
- Request advertised in the *Post & Courier*



Charleston County ZLDR Text Amendment Request

Planning Commission: October 12, 2020

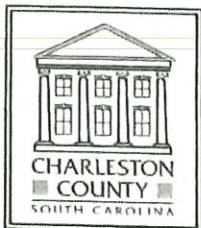
Public Hearing: November 10, 2020

Planning and Public Works Committee: November 19, 2020

First Reading: November 19, 2020

Second Reading: December 8, 2020

Third Reading: December 17, 2020



Charleston County Planning Department
 Lonnie Hamilton III, Public Services
 Building 4045 Bridge View Drive
 North Charleston, SC 29405
 Phone (843) 202-7200 Fax (843) 202-7218

Joel H. Evans, PLA, AICP, Director

ZONING ORDINANCE TEXT AMENDMENT APPLICATION

APPLICATION INFORMATION

Application Number: **ZUPR-08-20-00124**
 Date Submitted: **8/28/20**
 Applicant Name: **John Aquino**
 Address: **1920 B Dunbar St**
 City: **Charleston** State: **SC** ZIP Code: **29407**
 Telephone: **843-779-7155** Fax: E-mail: **john@naturalblondeblondy.com**

TEXT/MAP LOCATION OF REQUESTED AMENDMENT

Section No. and Title: **5.13.6**
 Page:

Please provide further indicators below to locate the subject text (subsection, item, paragraph, figure/exhibit)

REQUESTED AMENDMENT

PROVIDE PRECISE WORDING FOR THE PROPOSED AMENDMENT

(documentation may be attached to the application in lieu of completing this section)

The proposed amendment would read as follows: Amend Sec. 5.13.6, *Use Regulations*, Table 5.13-1, *DuPont-Wappoo Area Overlay Zoning District Use Table* to allow Beverage or ~~Related~~ Products Manufacturing, including Alcoholic Beverages and Excluding Microbreweries and Brewpubs conditionally in the Community Commercial District, subject to Sec. 6.4.57, *Manufacturing and Production*.

~~and~~ **and Food and Related Products Manufacturing**

SIGNATURES

APPLICANT(S)	PLANNING DEPARTMENT OFFICIAL
PRINTED NAME: John Aquino	PRINTED NAME: Emily Pigott
SIGNATURE: <i>[Signature]</i>	SIGNATURE: <i>[Signature]</i>
DATE: 8/28/20	DATE: 8/28/20
PRINTED NAME:	
SIGNATURE:	
DATE:	

FOR OFFICE USE ONLY

Application Number	ZUPR-08-20-00124		
Date Submitted	8/28/20		
Amount Received	\$250.00	Cash	Check Number:
Receipt Number			

Emily Pigott

From: John Aquino <cousineddie2@gmail.com>
Sent: Friday, August 28, 2020 7:32 AM
To: Emily Pigott
Subject: Re: Art. 1.5 ZLDR

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

ARTICLE 1.5 PURPOSE AND INTENT

CRITERION B Sec. 3.3.6:

This Ordinance is intended to protect the health, safety, and general welfare of existing and future residents of Charleston County by:

- A. Implementing the goals, objectives and policies of the Comprehensive Plan;
I plan on adhering to all rules and regulations as specified, and enhancing if possible
- H. Furthering the public welfare in any other regard specified by a local governing body;
I plan on setting up a teaching kitchen, using local chefs, garden center owners, and nutritionists in order to show the elderly and children that you can eat healthy, grow your own herbs and veggies, and maintain a healthier lifestyle. Showing them how easy it is to do these simple things will cut down on medical bills and promote a longer life
- I. Facilitating the creation of a convenient, attractive and harmonious community;
See above
- J. Encouraging the development of economically sound and stable municipalities and counties
I am currently running 5 businesses out of this building, co-packing for most. This rezoning may open the doors for others wanting to start their own business and give back to the community by sharing products and ideas in order to enhance the healthy lifestyle project we envisioned
- N. Fostering growth and development, and preserving our natural and cultural resources, always respecting the rights of the individual, including private property rights.
See all of the above! We are already donating time and products to multiple charities, such as Ronald McDonald House, Project Green Heart, MUSC Children's Fund, and other locally specific events. We encourage all others to do so via social media and personal connections. We have also connected with all tenants in our block and have a wonderful relationship with all

On Mon, Aug 24, 2020 at 10:50 AM Emily Pigott <EPigott@charlestoncounty.org> wrote:

ARTICLE 1.5 PURPOSE AND INTENT

This Ordinance is intended to protect the health, safety, and general welfare of existing and future residents of Charleston County by:

Emily Pigott

From: John Aquino <cousineddie2@gmail.com>
Sent: Friday, August 28, 2020 11:56 AM
To: Emily Pigott
Subject: Re: Art. 1.5 ZLDR

CAUTION: This email originated outside of Charleston County. Do not click links or open attachments from unknown senders or suspicious emails. If you are not sure, please contact IT helpdesk.

Good day all!

In accordance with Approval Criteria, Section 3.3.6, Subject A, regarding Challenge, I would submit that the area of Dupont/ Wappoo community commercial overlay is a commercial area in which my proposed usage is very much inline with several already established businesses. Co-packing salts, bloody mary mix, relishes and pickles is similar to the restaurants within the overlay, and I am upheld to the same safety procedures and compliance requirements, the only difference being that I am overseen by the USDA and not DHEC. I take these practices very seriously and refuse to compromise; safety is most important.

In accordance with Approval Criteria, Section 3.3.6, Subject C, as far as furthering public welfare and other community services, I pride myself on recycling my jars from local restaurants and bars and reusing them after a thorough cleansing. I have minimal waste; for example, I use the solids from my hot sauce to create a jalapeno pineapple relish, I use the waste from pureeing the tomatoes to send to farmers for compost or feed, and I filter rainwater for washing dishes and floors. I also use biodegradable plastic for my cocktail mixers and give many cases of products to charitable events. In the near future, I want to use my facility as a teaching kitchen to show that healthy eating and living is not as hard as people think, with the main focus on children and the elderly.

On Fri, Aug 28, 2020 at 9:45 AM Emily Pigott <EPigott@charlestoncounty.org> wrote:

Hi John,

Sec. 3.3.6 is on Page 9 of the Application Packet; please refer to the first email.

Thanks,

Emily Pigott

Planner I